

## OFFICIALS

Joe Stear, Mayor  
Briana Buban-Vonder Haar, Council President  
Richard Cardoza, Council Member  
Warren Christensen, Council Member  
Greg McPherson, Council Member



## CITY OF KUNA

Kuna City Hall Council Chamber, 751 W 4th Street, Kuna, Idaho 83634

### City Council Meeting AGENDA Tuesday, March 5, 2019

#### 6:00 P.M. REGULAR CITY COUNCIL

**1. Call to Order and Roll Call**

**2. Invocation:** Dean Herring, South Valley Baptist Church

**3. Pledge of Allegiance:** Mayor Stear

**4. Consent Agenda:** ALL OF THE LISTED CONSENT AGENDA ITEMS ARE ACTION ITEMS

*All items listed under the Consent Agenda are considered to be routine and are acted on with one motion by the City Council. There will be no separate discussion on these items unless the Mayor, Council Member, or City Staff requests an item to be removed from the Consent Agenda for discussion. Items removed from the Consent Agenda will be placed on the Regular Agenda under Business or as instructed by the City Council.*

**A. City Council Meeting Minutes:**

**I. Regular City Council Minutes, February 19, 2019**

**B. Accounts Payable Dated February 28, 2019 in the Amount of \$540,641.75**

**C. Resolutions**

**1. Consideration to approve Resolution No. R15-2019**

A RESOLUTION OF THE CITY COUNCIL FOR KUNA, IDAHO APPROVING A PERFORMANCE BOND BY DB DEVELOPMENT, LLC, FOR THE DESERTHAWK SUBDIVISION NO. 4 FOR UNCOMPLETED WORK INCLUDING STREET LIGHTING, FENCING AND LANDSCAPING PURSUANT TO THE TERMS OF THIS RESOLUTION.

**2. Consideration to approve Resolution No. R16-2019**

A RESOLUTION OF THE CITY COUNCIL FOR KUNA, IDAHO APPROVING A PERFORMANCE BOND BY DB DEVELOPMENT, LLC, FOR THE CAZADOR SUBDIVISION NO. 1 FOR UNCOMPLETED WORK

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INCLUDING STREET LIGHTING, FENCING AND LANDSCAPING PURSUANT TO THE TERMS OF THIS RESOLUTION.

3. Consideration to approve Resolution No. R17-2019

A RESOLUTION OF THE CITY OF KUNA, IDAHO APPROVING THE PARK USE AGREEMENT BY AND BETWEEN THE CITY OF KUNA, IDAHO AND THE KUNA POLICE ACTIVITIES LEAGUE (KPAL) AT ARBOR RIDGE PARK, WINCHESTER PARK, FARM ESTATES PARK AND PURSUANT TO THE TERMS OF THE AGREEMENT; AND AUTHORIZING THE MAYOR TO EXECUTE THE AGREEMENT.

4. Consideration to approve Resolution No. R18-2019

A RESOLUTION OF THE CITY COUNCIL OF KUNA, IDAHO AUTHORIZING THE MAYOR TO EXECUTE THE CONSENT TO USE AGREEMENT WITH THE UNITED STATES DEPARTMENT OF THE INTERIOR BUREAU OF RECLAMATION.

5. Consideration to approve Resolution No. R19-2019

A RESOLUTION OF THE CITY COUNCIL OF KUNA, IDAHO AUTHORIZING THE MAYOR TO EXECUTE ALL AGREEMENTS AND CONTRACTS AND THE CLERK TO ATTEST TO THE MAYOR'S SIGNATURE ON ALL SAID DOCUMENTS RELATED TO THE PURCHASE OF THE REAL PROPERTY WITH ADA COUNTY PARCEL NUMBERS S1326121090 AND S1326121015.

6. Consideration to approve Resolution No. R20-2019

A RESOLUTION OF THE CITY COUNCIL FOR KUNA, IDAHO APPROVING THE PROJECT PRIORITY LIST TITLED "CITY OF KUNA IDAHO'S TRANSPORTATION PRIORITY REQUESTS - 2019" AS THE OFFICIAL TRANSPORTATION PROJECT PRIORITY LIST FOR THE CITY OF KUNA, IDAHO FOR THE ADA COUNTY HIGHWAY DISTRICT; AUTHORIZING THE CITY CLERK TO TRANSMIT THE DOCUMENT TO ACHD; AND HEREBY REPEALING ALL PREVIOUS TRANSPORTATION PROJECT PRIORITY LISTS.

**D. Final Plats**

1. Consideration to approve 18-20-FP (Final Plat) for Ashton Estates Subdivision No. 2

**5. Community Reports or Requests:**

A. Update on Changes for the Kuna Clean Up – Chad Gordon, J & M Sanitation

**6. Public Hearings:** (6:00 p.m. or as soon thereafter as matters may be heard.)

- A.** Public Hearing and consideration to approve 18-06-AN (Annexation) & 18-08-S (Preliminary Plat) – Jace Hellman, Planner II **ACTION ITEM**

On behalf of Trilogy Development, Inc., Jane Suggs, with WHPacific requests to annex two parcels comprising of approximately 60.85 acres on East Hubbard Road and North Locust Grove Road into Kuna City Limits with an R-8 zoning designation; and to subdivide the 60.85-acres into 298 total lots, consisting of 253 single family lots, and 45 common lots. The subject sites are adjacent to the southwest corner of East Hubbard Road and North Locust Grove Road, Kuna, ID 83634, within Section 18, Township 2 North, Range 1 East; (APN's S1418121126 & S1418123400).

- *Open Public Hearing*
- *Receive evidence*

Potential Motions:

- *Consideration to close evidence presentation and proceed to deliberation*
- *Consideration to either:*

*Option 1: Approve a Decision and Order and Close the Public Hearing and direct staff to prepare Findings of Fact, Conclusions of Law and Order of Decision for approval by the Council on the next Council Meeting Consent Agenda.*

*Option 2: Continue the Public Hearing to a time and date certain, direct staff to prepare a draft Findings of Fact, Conclusions of Law and Order of Decision for consideration by the Council.*

- B.** Public Hearing for Resolution No. R14-2019 and Consideration to approve Ordinance No. 2019-07 – Chris Engels, City Clerk **ACTION ITEM**

- *Open Public Hearing*
- *Receive evidence*

Potential Motions:

- *Consideration to close evidence presentation and proceed to deliberation*

- I.** Consideration to approve Ordinance No. 2019-07

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF KUNA:

- REPEALING SECTION 16 OF CHAPTER 3 OF TITLE 7 KUNA CITY CODE; AND
- AMENDING CHAPTER 3 OF TITLE 7 KUNA CITY CODE BY THE ADDITION OF NEW SECTION 16 MAKING IT UNLAWFUL TO SELL, DISTRIBUTE, POSSESS OR CONSUME ANY TYPE OF ALCOHOLIC BEVERAGE ON KUNA CITY PARK AND CITY PUBLIC PROPERTY

EXCEPT FOR BEER AND/OR WINE UNDER A PARK BEER AND/OR WINE ALCOHOLIC BEVERAGES SPECIAL USE PERMIT ISSUED BY THE CITY; AND PROVIDING PERMIT TERMS AND CONDITIONS; AND

- DIRECTING THE CITY CLERK; AND
- PROVIDING AN EFFECTIVE DATE.

*Consideration to waive three readings*

*Consideration to approve ordinance*

*Consideration to approve summary publication*

2. Consideration to approve Resolution No. R14-2019

A RESOLUTION OF THE CITY OF KUNA, IDAHO, ESTABLISHING FEES FOR BEER/WINE PERMIT ALLOWING FOR INDIVIDUAL USE AT CITY PARKS AND PROVIDING AN EFFECTIVE DATE.

#### **7. Business Items:**

- A. Fair Housing Month 2019 Proclamation – Mayor Stear
- B. Consideration to approve Resolution No. R21-2019 – Nancy Stauffer, Human Resources Director **ACTION ITEM**

A RESOLUTION OF THE CITY OF KUNA, IDAHO, APPROVING AND ADOPTING AN AMENDMENT TO THE CITY OF KUNA PERSONNEL POLICY MANUAL SECTION 12.3.5 REGARDING APPROVAL OF EMPLOYEE VACATION CAP EXTENSION AND PROVIDING AN EFFECTIVE DATE.

#### **8. Ordinances:**

- A. Second Reading of Ordinance No. 2019-05 **ACTION ITEM**

AN ORDINANCE OF THE CITY COUNCIL OF KUNA, IDAHO AMENDING TITLE 1, CHAPTER 6, SECTION 8 ENTITLED PUBLIC HEARING RULES OF PROCEDURE OF THE KUNA CITY CODE PROVIDING FOR:

- CLARIFICATION ON PUBLIC HEARING RULES OF PROCEDURE;
- ESTABLISHING TESTIMONY TIME LIMITS;
- REGULATING THE PRESENTATION OF DOCUMENTS AND OTHER FORMS OF PHYSICAL EVIDENCE;
- AMENDING THE RECOGNIZED PUBLIC HEARING MOTIONS;

- CLARIFYING CONFLICT OF INTEREST PROCEDURES; AND
- PROVIDING FOR AN EFFECTIVE DATE AND PUBLICATION.

*Consideration to waive third reading*  
*Consideration to approve ordinance*  
*Consideration to approve summary publication*

#### **9. Mayor/Council Announcements:**

#### **10. Executive Session:**

- A. Adjourn to Executive Session pursuant to:

Idaho Code Section 74-206 (1)(d)

(d) To consider records that are exempt from disclosure as provided in Chapter 1, Title 74, Idaho Code **ACTION ITEM**

#### **11. Business Continued**

- A. Personnel Matter Nos. 2019-1 and 2019-2: Consider a motion to approve recommended extension of time for personal to use vacation time – Nancy Stauffer, Human Resources Director **ACTION ITEM**

#### **12. Adjournment:**

**OFFICIALS**

Joe Stear, Mayor  
 Briana Buban-Vonder Haar, Council President  
 Richard Cardoza, Council Member  
 Warren Christensen, Council Member  
 Greg McPherson, Council Member

**CITY OF KUNA**

**Kuna City Hall Council Chamber, 751 W 4th Street, Kuna, Idaho 83634**

**City Council Meeting  
 MINUTES  
 Tuesday, February 19, 2019**

**6:00 P.M. REGULAR CITY COUNCIL****1. Call to Order and Roll Call****COUNCIL MEMBERS PRESENT:**

Mayor Joe Stear  
 Council President Briana Buban-Vonder Haar  
 Council Member Richard Cardoza  
 Council Member Warren Christensen - Absent  
 Council Member Greg McPherson

**CITY STAFF PRESENT:**

Bob Bachman, Public Works Director  
 Jared Empey, City Treasurer  
 Chris Engels, City Clerk  
 Wendy Howell, Planning & Zoning Director  
 Bobby Withrow, Parks Director  
 Lisa Holland, Economic Development Director  
 Troy Behunin, Planner III  
 Sam Weiger, Planner I

**2. Invocation:** Chris Bent, Calvary Chapel**3. Pledge of Allegiance:** Mayor Stear

**4. Consent Agenda:** ALL OF THE LISTED CONSENT AGENDA ITEMS ARE ACTION ITEMS  
 (Timestamp 00:01:23)

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**A. City Council Meeting Minutes:****1. Regular City Council Minutes, February 5, 2019**

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**B.** Accounts Payable Dated February 14, 2019 in the Amount of \$544,731.42

**C.** Resolutions

**1.** Consideration to approve Resolution No. R09-2019

A RESOLUTION OF THE CITY COUNCIL OF KUNA, IDAHO APPROVING THE PARK USE AGREEMENT BY AND BETWEEN THE CITY OF KUNA, IDAHO AND THE KUNA YOUTH SOFTBALL AND BASEBALL ASSOCIATION, INC., PURSUANT TO THE TERMS OF THE AGREEMENT; AND AUTHORIZING THE MAYOR TO EXECUTE THE AGREEMENT.

**2.** Consideration to approve Resolution No. R10-2019

A RESOLUTION OF THE CITY COUNCIL OF KUNA, IDAHO APPROVING THE DEED OF PUBLIC UTILITY EASEMENT AND ACCEPTANCE FROM RUSSELL AND KAREN HUNEMILLER.

**3.** Consideration to approve Resolution No. R11-2019

A RESOLUTION OF THE CITY COUNCIL OF KUNA, IDAHO APPROVING THE DEED OF PUBLIC UTILITY EASEMENT AND ACCEPTANCE FROM SANDSTONE FARMS, LLC.

~~**4.** Consideration to approve Resolution No. R12-2019~~

~~A RESOLUTION OF THE CITY COUNCIL OF KUNA, IDAHO APPROVING THE DEED OF PUBLIC IRRIGATION UTILITY EASEMENT AND GRANT CONVEYANCE OF MUNICIPAL IRRIGATION SYSTEM UTILITY PIPELINES AND GRANTEE ACCEPTANCE FROM SDN, LLC.~~

**5.** Consideration to approve Resolution No. R13-2019

CITY SEWER INFRASTRUCTURE IMPROVEMENT AND LATECOMER REIMBURSEMENT POLICY - 2019.

**6.** Consideration to approve Resolution No. R08-2019A

A RESOLUTION OF THE CITY OF KUNA, IDAHO AMENDING R08-2019 TO CORRECT THE STEP AND GRADE CHART COLUMN A AND INCLUDE ALL DEPARTMENTS IN THE AMENDED LONGEVITY POLICY OF THE CITY OF KUNA, IDAHO AS ATTACHED HERETO; AND DECLARING THE EFFECTIVE DATE.

Mayor Stear noted they needed to remove Item 4C4, Resolution No. R12-2019 from the agenda. It was already approved at the last Council meeting.

**Council President Buban-Vonder Haar moved to remove Item 4C4 from the agenda. Seconded by Council Member McPherson. Motion carried 3-0-1. Council Member Christensen absent.**

**Council President Buban-Vonder Haar moved to approve the consent agenda with item 4C4 removed. Seconded by Council Member McPherson. Approved by the following roll call vote:**

**Voting Aye: Council Members Cardoza, Buban-Vonder Haar, and McPherson**

**Voting No: None**

**Absent: Council Member Christensen**

**Motion carried 3-0-1.**

#### **5. Community Reports or Requests:**

*None*

#### **6. Public Hearings: (6:00 p.m. or as soon thereafter as matters may be heard.)**

- A.** Continuation of Public Hearing to receive further information as it relates to the overview of pedestrian and vehicle traffic with testimony from ACHD, additional testimony from staff, and general testimony and consideration to approve 18-01-S (Preliminary Plat) for Whisper Meadows Subdivision – Troy Behunin, Planner III

#### **ACTION ITEM**

*(Timestamp 00:02:49)*

Jeff Bower, on behalf of the applicant, requests approval to subdivide approximately 73.50 acres into 310 buildable lots, 38 common lots. The subject site is located *near* the northwest corner of Linder and Columbia Roads, Meridian, Idaho, ID 83642, within Section 2, Township 2 North, Range 1 West, B.M. (APN#'s R1393850100, R1693860010, R1693860290 and R1693860280).

Mayor Stear stated the time had run out for going to ACHD to ask for a variance but they graciously accepted and placed Kuna on the agenda for the following day at noon. He would go to the meeting to try to get the variance from ACHD. He was not sure they would because it would require a variance from the City which wouldn't be a big deal, as well the variance from ACHD.

Planner III Troy Behunin thanked Mayor for the information on the ACHD meeting. He clarified for the record the City was asking for a revisit on decision for a waiver request therefore public notice was not required. In between the last meeting and this meeting, on Friday afternoon, Staff received an email letter from the residents of Chisum Valley. Unfortunately, it did not make it to their inbox in time for the packet. He passed out a copy of the letter to Council and noted the applicant was also mailed the letter that afternoon and received a copy. He read the letter into the record. The letter reviewed the Whisper Meadows Subdivision case history and requested Council conditionally approve the Whisper Meadows development on

Whisper Meadows successfully obtaining a waiver of policy from ACHD waiving stub-street connection between the 2 subdivisions.

Mr. Behunin stated staff added to their report the power point that was shown at the last meeting as well as answers from Gary Inselman, an ACHD representative, regarding questions and another item he couldn't remember right then. Mr. Behunin stood for questions.

Council President Buban-Vonder Haar asked, regarding the ACHD hearing for the next day or past that point, if there was any further action required of the developer/applicant.

Mayor Stear said no but the applicant sent a letter saying they would be happy to make changes if ACHD granted the waiver and they provided a letter of support. Beyond that there was nothing.

Mayor Stear reopened the public hearing.

Jeff Bower, on behalf of the applicant, reviewed the application with a power point presentation that was 99% the same as the previous week. He reviewed his letter requesting conditional approval. He summed it up as requesting Council approve the plat with the condition that they would amend it to reflect whatever ACHD decided. He felt they were in between a rock and a hard place and felt conditional approval was fair and proper. ACHD had jurisdiction and the plat they submitted met both ACHD and Kuna City standards. To put it bluntly, regardless of whether or not ACHD granted the waiver, their plat met the standards and he did not feel they could be denied based on the connectivity issue so they wanted to move forward. He addressed the statements made in the letter that was read into record. He noted the conditions of approval from a prior plat are cast aside because this land had been annexed by the City and rezoned to R-6 so currently the standards they had to meet were for R-6 designation. He appreciated the letter and it reflected everything correctly but the condition they were asking for was slightly different. Instead of approval subject to ACHD they were asking that this not be approved unless ACHD approved the waiver. He believed that was improper and objected because it put their fate into the hands of another body and that was not permissible. He noted they met all the criteria for the plat process, they met the comprehensive plan goals, all the necessary public services were nearby, they were building a bunch of infrastructure and supporting the city CIP, and the evidence submitted by ACHD traffic engineers showed the development they were proposing was safe with the mitigation. He stood for questions.

Mayor Stear asked if it was 2 soccer fields in the plan. It didn't look big enough for 2 soccer fields.

Mr. Bower explained it was 2 soccer fields at approximately 5.5 acres. His understanding was it was designed for 2 soccer fields to go north-south and that was

2 youth 12 and below size fields but it could also function as 1 full size or high school size field if you ran it east-west.

Council Member Cardoza asked how they came up with the 5% usages for trips on McLintock.

Mr. Bower explained those numbers were generated by Kittleson & Associates, their traffic engineers. The engineers told him when they did a study, they went out in town cars and got an understanding for the direction cars moved from that region. Basically, they said on a daily basis which direction cars were headed from this quadrant. What they found and what their modeling showed after they built in their trips was the predominant number of trips went north towards the interstate and then towards Boise based on the traffic counts they did at the intersections.

Council Member Cardoza thought that putting in another subdivision would alter the course of traffic with people wanting to take a shorter route or a less frequented route. He asked if they took that into account with the 5%.

Mr. Bower replied they did. They had done a couple things with this plat to specifically address the neighbors cut through concern. He went back to a slide that showed how they had designed short blocks which created shorter straight-aways and more curves so to get from Linder to Chisum they increased the number of turns to dissuade cut through traffic. Also, the way they phased the project to make the last phase, phase 6, so all of the other connections and road infrastructure would be built out before connecting to Chisum so people would get used to driving a different way and to make sure traffic wasn't directed that way out of need. The collector street would be built and the turning lane would be built on Linder. Those would be the main access points.

Mayor Stear asked if there was parking for the soccer fields somewhere.

Mr. Bower did not believe there was specific dedicated parking but there was street parking.

Council Member Cardoza clarified 150 trips a day was a typical day coming out of the subdivision.

Mr. Bower replied that was correct. He explained the engineer's modeling predicted 150 trips per day total would come down the 2 streets combined.

Council Member Cardoza responded if they had 310 buildable lots with approximately 2 cars that's 620 vehicles within that subdivision but they were saying it would only generate 150 trips.

Mr. Bower explained the total trips per day number, and this was a national standard, they took 9.8 times the number of houses so their total trips per day generated by the

whole subdivision was 2,983 he believed and of that 2,983 they believed that 5%, based on the traffic engineer's modeling, would go through Chisum to reach Columbia. That was about 150 trips per day and that 150 trips per day would be split between the 2 streets.

Parks Director Bobby Withrow asked if Mr. Bower was including all of the open space when discussing the park, or just the park, because on the map he had it showed an acre and a half not 5 acres.

Mr. Bower replied he was referring to total open space as 5 acres not the park that was shown.

Support: None

Neutral:

Richard Durrant, 7592 S Ten Mile Road, Meridian, Idaho 83642, asked if Council Member Cardoza had a chance to review the previous meeting. He didn't want to rehash things if he had already reviewed them.

Council Member Cardoza replied he had read some of Mr. Durrant's letters from the previous meeting so he could highlight.

Mr. Durrant had 2 issues to discuss. He thought the letter from the D R Durrant Trust was in the minutes. He was trying to decide how, historically, as he had farmed this property over the last 20 years and how he had improved and made modifications to drain ditches and things, trying to make sure they did not go off existing drain ditches and working with the developer and trying to get the developer to give him some indication on the final plat on how drain ditches and water to the properties to the north that were previously provided water through gravity irrigation and were currently being irrigated by the pivot irrigation. He was trying to get information and wanted some sort of recourse to deal with it if it didn't meet with those land owners' developments and the drain ditches that currently went on these properties. They were moved onto his property to take care of the pivot irrigation so they didn't have to have pivot towers crossing them. He was also concerned that they made concessions for the lot sizes against Chisum Valley being half acre lots. Along his property he had 15 eighth acre lots so he would have 15 neighbors to deal with instead of his current none. Additionally, he was concerned about Durrant Lane and the right-of-way that was granted. He finally talked with Gary Inselman from ACHD the previous week but he was still concerned that he was being placed with an undue burden to provide more property on his side for that road if it ever continued to go through and the roundabout the developer currently had about 20% and they would be left with about 80% to improve at that point in the future. In regards to the safe guards for his recovery pond, he knew they would be fenced but there would still be the road access to the north so he was trying to make sure there was some protection there and fencing to keep people out of it.

Council Member Cardoza thought his rights to access and the flow of water to his property were guaranteed by the Bureau of Reclamation.

Mr. Durrant replied they were but they had substantially changed since he put the pivot irrigation in. There used to be different ditches going to the different properties there. He tried to get some information. The land owners to the north hadn't received any information on how that water was going to be provided to them at that point.

Mayor Stear clarified his question was about water delivery.

Mr. Durrant replied that was correct.

Council Member Cardoza asked if the Bureau of Reclamation had any drawings from the subdivision as to how they were going to allocate the water to Mr. Durrant's property.

Mr. Durrant had not been able to see any of those. He had asked for some plats but he didn't know if that was going to happen in final plat or where it was going to happen at for the properties to the north and those weren't his personal properties. The ones on his were the drain ditches that historically were on the proposed Whisper Meadows Subdivision and went straight north from Chisum and ended up down in the recovery ponds that were there and then eventually ended up at Mason Creek if they didn't pump back that water.

Council Member Cardoza addressed Durrant Lane stating Mr. Durrant knew that street was there, or proposed to be there, previously.

Mr. Durrant explained he did not receive any information from Ada County regarding the improvements to Durrant Lane. When Chisum Valley went in, there was no right-of-way set aside from Chisum for proposed improvements 20 years ago.

Council Member Cardoza said, in perpetuation going backwards, Mr. Durrant knew Durrant Lane or School Street would be completed someday.

Mr. Durrant had seen it turning into School Street but never envisioned it going across Mason Creek due to the amount of traffic that potentially was there. He could say that he knew something could happen in the future but he didn't feel it was his responsibility to provide 80% of property for that to be done on and along Chisum would be 100% if that went through at that point.

Mayor Stear pointed out a report from Boise Project Board of Control on page 247 of 671 in the packet. It said local irrigation/drainage ditches that cross this property in order to serve neighboring properties must remain unobstructed and protected by appropriate casements. He noted there was more in the report as well.

Council Member Cardoza stated it sounded like his rights were being observed.

Mr. Durrant had a feeling, with another development, that things would change. He wanted the developer to provide information for him and then, trying to come back after the fact, to document this discussion and how everything was taken care of.

Council Member Cardoza could see this time of year being imperative for the developer to keep in contact with Mr. Durrant since they were getting into the planting season and they would need to know where Mr. Durrant's water was coming and going. He would think the engineer would contact him about that.

Mayor Stear noted that had come up before, keeping things open for irrigation and that type of thing. He asked Planner III Troy Behunin if there was some way through the City's process to contact people that required that delivery of water and how that was being taken care of for them to ease their minds. He knew Boise Project said they had to do that but how that ended up happening was sometimes a problem.

Mr. Behunin replied that was always a concern. People always wanted to know that, especially if they were farming something that was being developed. It was a state law that you could not impede someone's delivery or waste ditch, whether it was coming or going. You had to maintain the in and the out. If there was something else staff could do to get them to bring up that water rights would not be impeded during the neighborhood meeting or something else they could do, they absolutely would. Mr. Eck was used to this. He developed farm land and other lands with water.

Public Works Director Bob Bachman added, during the construction process, the City's inspectors made sure those ditches were maintained and water was preserved for the existing property owner. They watched those and frequently had to work on them or have the contractors work on them to maintain that water flow. They watched it very closely.

Mayor Stear wanted to make sure that during this process Mr. Durrant was kept in the loop so he would know there weren't going to be problems they would have to go back and correct or if there was a period of time he would be out of water because something was improperly done by accident.

Mr. Behunin suggested adding something to the staff report that, when it gets to P & Z, they could add another condition that they maintain those water rights that were protected by the state or maybe make it a requirement that it be discussed at the neighborhood meetings from the get go.

Mayor Stear thought communicating the facts with the people effected would be most beneficial. He asked that staff take that into consideration.

Council Member Cardoza asked if Mr. Behunin knew if a developer could change the ditch flow when doing a plat; did they have to maintain the same characteristics or could they move it so it ended up at the same weir at the very end.

Mr. Behunin asked for clarification.

Council Member Cardoza asked if it was going from left to right could they change the flow of the water through the plat to make it go straight to the existing weir or would they have to maintain the original creek or ditch bank through there.

Mr. Behunin replied it was possible to change the path of delivery water and waste but it was a process that had to be done through Boise Project because all of those waters were in an easement that predated most of them. They had to maintain the in and the out but everything in between was flexible. However, there was a process a developer had to follow and it wasn't easy.

Against:

Shannon Luckie, 7714 S McLintock, Meridian, Idaho 83642, appreciated Council's follow up and due diligence with ACHD over the variance. She asked if the meeting the next day at noon was open to public testimony.

Mayor Stear did not think it was open to public testimony. He thought it was just a request they were going to present.

Planning & Zoning Director Wendy Howell explained it was a public meeting, not a public hearing, in front of ACHD commissioners therefore public could be present but they would not take public comment.

Ms. Luckie asked if there was anything Chisum Valley residents could provide that would help with this process.

Mayor Stear thought the letter from the homeowners that was presented at the beginning of the meeting was very beneficial. Basically, the timeline had passed to bring it back to ACHD so it couldn't be a public hearing. They were kind of opening it up as a favor to hear what they had to say.

Ms. Luckie said they appreciated that.

Mayor Stear stated staff worked really hard on making that happen.

Ms. Luckie was concerned the traffic study was done to only include Chisum Way. When this was first proposed Chisum Way was going to be connected and McLintock was only going to be a pedestrian right-of-way. She wanted to make sure the traffic study included both streets. Also, the traffic study numbers seemed very low. It would be approximately 620 cars coming out of Whisper Meadows

Subdivision and she believed a lot of those folks would be looking to bypass the increased traffic that would be on Linder Road due to the Springhill Subdivision located on Linder that had 677 homes and with the new high school there. The School Zone speed limit for that area would only be 20 MPH and if they accessed through Chisum Valley it would be 25 MPH. They would bypass a lot of that traffic through that school zone. She thought Whisper Meadows would be starter homes with a lot of families with small children. All of the elementary schools were to the south so they would be going south. They would not all be going north. Those were just some concerns.

Lauri Allen, 7980 S Chisum Way, Meridian, Idaho 83642, wanted to reiterate about the traffic. They were the first house in and to the right when pulling into Chisum Valley. They were, she guessed, about 60 feet from Columbia Road. Sitting at their house you could watch how many cars came racing through there that, if a child was to run out, couldn't stop. All of their neighbors knew they had children and were very conscious when coming through the subdivision. This new subdivision wouldn't care less where children were and what homes they had.

Ms. Allen stated they also had horses, horse trailers, a pontoon boat, and 4-wheel trailers. They all backed them into their arena. In order to do so, they pulled into their neighbor's drive and backed up. She knew she had stalled traffic while she was trying to back the trailers in. She said you're looking at 60 feet with a truck and trailer backing in and you have 300 and some cars lining up on Columbia waiting for her or her husband to back their trailer into their arena. She said think of the repercussions that could happen with cars building up there.

Ms. Allen thought an interesting thing would be a traffic study on Denali Heights and Arbor Ridge all crossing, and she was guilty of this too, through Applewood Subdivision to get to Silver Trail instead of going to Ten Mile. There were tons of cars racing through that subdivision because they didn't know or care where the kids would be on their bikes because they weren't in that subdivision.

Ms. Allan would also be interested, at the final phase of this subdivision, in whether the bus route would have them go to Linder or if it would be easier for 60 homes at the edge of the subdivision to go down Chisum Way to meet their bus. She appreciated the effort and thought they were putting into traffic. It was a huge concern. Growth was going to happen but safety had to always come first especially for those who were there first.

Stephanie Kennedy, 7638 S McLintock, Meridian, Idaho 83642, respectfully requested again, a new impact study including environmental, traffic, and noise be conducted since McLintock was now mandated to be a main artery for the proposed 310 homes. They were challenging Mr. Eck's reported number of cars going through their subdivision. The Spark Engineering and Highway Assessment Program and Company clearly showed in their equation that 310 homes would equate to approximately 3,315 expected trips per day. That included work, school, sports,

shopping, etc. This was clearly much higher than Mr. Eck stated. They requested a second opinion, perhaps from HDR, specifically Dan Jones, a professional traffic engineer who assessed proposed subdivisions and their impact.

Ms. Kennedy said Mr. Eck stated McLintock was wide enough for 2 cars and had a 4-foot walkway on each side. This was erroneous. After careful measurement they found less than 12 inches per side and there were rocks and a deep drainage ditch on each side. Clearly not enough to safely ride a bike or walk. She said imagine the backed-up traffic as the 2 garbage trucks that came into their subdivision weekly stopped in front of each home or the mail man stopped at each mailbox daily. There was no way to get around these vehicles. It was clearly not wide enough for an emergency lane so backing out of their driveways would be nearly impossible.

Ms. Kennedy quoted a statement from Mr. Eck as saying, "A 20-foot buffer zone would go all around the subdivision", yet the plat showed that to be incorrect. There was none adjacent to their subdivision. He also stated there was an easement. However, the financial responsibility would still be theirs to repair their landscaping, outbuildings, and fences and this could come to thousands of dollars per lot that they would be responsible for. His irresponsible plan jeopardized their subdivision and could put them into litigation and possibly bankrupt their small community if forced to pay for these. She asked for a common area between the 2 subdivisions.

Ms. Kennedy also asked if they could pose some type of irrigation damage trust against Mr. Eck for future repairs for the irrigation line he proposed building on top of. She asked that Council look at their plat, she thought it was number 3 on the program, it showed there was an irregular boundary to the west side. That was to be extended clear across the back of their property lines to protect their pressurized 6-inch irrigation line. Right now, looking at it, and Mr. Eck stated his fence would be directly against theirs, there was no protection for that 6-inch pressurized irrigation line. They wanted to know if there was no grandfather clause after 20 years that would protect them and this pressurized irrigation line. They heard that there was protection for Mr. Durrant's water line and the water that went to the other homes but what about theirs. Basically, what would happen after Mr. Eck generated the revenue and changed how they lived and the very reasons they moved to Chisum Valley was they would have to live with those consequences. They kindly asked that Council protect their neighborhood, investments, safety, and the very reason they moved there and deny Whisper Meadows.

Josh Ricciardi, 7752 S McLintock Street, Meridian, Idaho 83642, stated Chisum Valley had been established for 20 years. They had done ok with those cul-de-sacs. Fire emergency vehicles had been down there and hadn't done any 3-point turns or anything like that. Those safety issues had been avoided. Other subdivisions had sidewalks. Ms. Allen brought up people cutting through subdivisions. The kids stay on the sidewalks. Hopefully their kids would stay on the side of the street of 12 inches. ACHD would do a hardship to Chisum Valley if they approved the connectivity. It was still within Council's court. He asked that they consider that.

Mr. Ricciardi added that the road to the north of Whisper Meadows was a road Mr. Eck would be building. The road to the left would basically take most of Mr. Durrant's property away. He also reiterated concerns regard traffic impact, traffic coming through their subdivision, and access to their irrigation line for maintenance.

Kevin Hall, 7675 S McLintock, Meridian, Idaho 83642, was new to the subdivision. He had moved there the previous August. He had purchased a house on an acre in Boise County 20 years prior and enjoyed it but over the last few years it was annexed and there was a fire station and a day care center in his backyard. He moved out here because it was in the country back in the county. It was private and on a dead-end road without through traffic. He passed some pictures of their road widths out to Council and the City Clerk. He said they talked about the streets based on Ada County double lines, side, 13-foot lane widths. They had less than 12 inches for kids. Both sides of the roads had deep rocks. It was a pit. There was no escape for bicycles or pedestrians. There was no where to go when there were 2 cars passing side by side on the street.

Council Member Cardoza asked if Chisum Valley was in City limits or if it was in the county.

Mayor Stear replied it was in the county.

Jenna Von Der Ehe, 7601 S Chisum Place, Meridian, Idaho 83642, stated the applicant communicated there would be widening and paving on Columbia and Linder. There was no easement or turning lane to turn into their street from Columbia. You could easily, from the 4-way stop at Linder and Columbia to Chisum, get up to 50 MPH. It was great that they were improving Linder but there was no mention of Columbia and Chisum. He said the initial plat that was approved for Chisum, the cul-de-sacs would provide for future connectivity. That was correct but they also said all lots would be at least 1 acre lots. The initial planning was that connectivity would include like size lots, not 310 homes. Also, the applicant represented they were trying to mitigate by designing shorter blocks in their plat but did not focus on the long straight shot from the end of Chisum to Columbia. She was at the end of Chisum on the left-hand side. You could shoot an arrow to Columbia it was that straight. That was great that they were worrying about their neighborhood but she guaranteed that was like a bullet shot straight to Columbia. They brought attention to the new high school and cars heading east but failed to mention cars heading south through their neighborhood that would be attending Silver Trail. She guaranteed they would not go east to Linder down to Columbia to head west when they could cut through their neighborhood. She thanked Council for listening.

Stephanie Shank, 7825 S. Chisum Way, Meridian, Idaho 83642, noted one thing through the traffic study that had been done and was up on the slide they showed. It said half of the cars would be coming south on McLintock and half would be coming south on Chisum but McLintock fed into Chisum. That would be, if you were going with 75 cars on each road, 150 cars right in that little tiny intersection and trying to

get onto Columbia. She wanted to point that out since it wasn't mentioned and, if they hadn't looked at the map, they might not have realized that was a real potential snarl.

Rebuttal:

*(Timestamp 01:07:00)*

Tim Eck, the applicant, 6152 W Half Moon Lane, Eagle, Idaho 83616, reviewed process, engineering, traffic, and safety. When they began the entitlement process, they hired highly educated, licensed individuals to begin the process of designing the preliminary plat culminating in the finished preliminary plat before them that evening. When they began the platting process, they hired engineers to design a plat that was 100% compliant with all applicable statutes, codes, and polices. When they initially laid out a plat they met extensively with the City Planning & Zoning Department and ACHD to ensure the plat design was 100% compliant with all ACHD polices and Kuna City Codes.

Mr. Eck explained once the plat was designed, they submitted to the City Planning & Zoning Department and ACHD where it was again thoroughly reviewed for compliance with all codes and polices. ACHD reviewed the plat because Idaho law provided that ACHD was responsible for the design of all streets within Ada County. This was Idaho Code section 40-1415. This Idaho law specifically charged ACHD as the agency to set standards and review subdivision applications for street width and traffic flow and connectivity.

Mr. Eck said for large projects ACHD required the preparation of a traffic impact study to analyze mitigation. Licensed engineers prepared traffic impact studies. The traffic impact studies started with the study of the impact area to determine the background traffic and existing levels of service. To do this, actual data and traffic counts were collected and then modeled. This created a baseline. The study was required to determine the existing levels of service and the levels of service that would exist at the studied segments and studied intersections at full build out of the subdivision.

Mr. Eck stated upon approval by ACHD of the studied scope of work, existing traffic counts and background traffic data was collected. Background traffic was the additional traffic generated by all other projects previously approved and in various phases of development that would have impact on traffic within the study area.

Mr. Eck said, lastly, the additional traffic resulting from the new subdivision was calculated and added to the road segments and intersections. This exhaustive study, including all recommendations for mitigation, was then submitted to ACHD where it was thoroughly reviewed for code, statute, and policy compliance. If any discrepancies were found, ACHD would return the traffic impact study to the traffic engineer for expanded study area and/or revisions. The results were then used to

determine any and all mitigation required to produce and maintain safe road segments and intersections that would perform at acceptable levels of service.

Mr. Eck explained once the traffic impact study was reviewed by ACHD and found to be accurate, complete, and compliant, ACHD would accept the traffic impact study as complete and proceed with the ACHD staff report. Once the ACHD staff report was issued, ACHD, under their statutory discretion, had approved the preliminary plat subject only to the general and site-specific conditions of approval, and determined that the traffic impact study was complete, accurate, and identified all mitigation required to maintain safe roadway infrastructure. In this case, the ACHD staff report fully supported the project as designed and concluded it met all the districts standards and policies.

Mr. Eck said engineers at Kittleson & Associates, a well-respected national engineering firm, and ACHD's own staff and engineers devoted many hours to studying this project and modeling the impacts. Both concluded the plat design was safe and met all applicable road design safety standards. Anecdotal testimony and opinions should not outweigh conclusions from traffic engineers and experts.

Mr. Eck reviewed, as previously discussed, the roads in Chisum Valley were built to the proper ACHD standards for local streets in a 1-acre subdivision. The streets were not substandard. The traffic engineer concluded that both Chisum and McLintock would continue to operate at Level of Service A at full build out of Whisper Meadows. Local roads like Chisum and McLintock were designed for 2,000 trips per day. In this case, at full build out, they would share only 460 trips per day. In addition, they designed their plat to deter cut through traffic and ensure slow speeds on local roads.

Mr. Eck stated again, they were requesting approval or conditional approval of the preliminary plat application before Council. They did not have a preference on the connections to McLintock or Chisum but they designed the plat to be fully compliant with the City Code and ACHD policies which both required connections. ACHD had required these connections and most importantly the evidence before Council showed the connections were safe. They understood Kuna had been granted the opportunity to speak at the ACHD Commission Hearing the following day where Kuna would request ACHD to reconsider the connections to McLintock and to Chisum. They were prepared to comply with the results of the ACHD hearing pertaining to the connections to McLintock or Chisum. However, they did not believe the ongoing dialogue between the City and ACHD should prevent an approval of the current application.

Mr. Eck followed up on notes stating they would be putting in a road trust \$172,000 for the completion of Durrant Land and a short section of Butterfly for the components that could not be built or could not be opened. They could not build that last segment of Butterfly and open it because it went nowhere. They didn't want it built and they didn't want to maintain it so they made them put the money into a

road trust. Water delivery and waste was all designed with engineering and was looked at extensively by public works. It was state law. He had to get the water from A to B but how he got it there was up to their discretion. He could relocate ditches but they still had to provide the delivery and the waste. They did delay this application to have McLintock looked at extensively. They asked their traffic engineer to look at McLintock again because ACHD wasn't going to allow the disconnection. So, it was looked at and the analysis came back, which was submitted a long time ago, that the general trip generation in that direction wouldn't change, it would just be split between those 2 streets because they did connect in 1 location.

Mr. Eck reviewed the landscape buffers did meet all requirements. There was no landscape buffer required between subdivisions. City Code required it along arterial roads and mid mile connectors and all those landscape buffers would be installed. There would be a pressurized irrigation easement across the common boundary between the backs of their lots and Chisum Valley's lots. There was a requirement for pressurized irrigation easement. If Chisum Valley irrigation was in that easement it was Chisum Valley's, not theirs. They would clean up all the risers that stuck up and remove them all because those risers actually provided irrigation water to his land or it did at one time. They would clean them up and cap them off and if they damaged a pipe while doing that, they would repair that. However, if Chisum Valley had a failure of their system in the future, it was their system. Just like if the City system failed in the future, it was the City's system. Chisum Valley did have an easement that granted them access to get there but they would have to return it to the condition it was in when they went in.

Mr. Eck stated there was a road standard for standard rural roads in subdivisions greater than 1 acre but less than 5 acres that required 30 feet minimum of paving and it said there was no sidewalk required and 4 feet on each side was striped for non-motorized travel. That 30-foot segment included the 4 feet of pavement on each side for non-motorized travel, pedestrians and bicycles. That was the condition of their approval; that their roads were built to 30 foot. Their conditions of approval also required both McLintock and Chisum to be stubbed to his property including the right-of-way that currently went to their property. They were not opposed to not connecting to them. He had drug this plat out 9 months trying to accommodate that but it was at ACHD's discretion. Fortunately, there was a little wiggle room left in the motion they made to deny the waiver which allowed the City to come back and ask. It was a public hearing and if the residents wanted to show up at the hearing to support an additional request for a waiver, they were welcome to be there or to send in emails. He would also be there to support the waiver. He wanted to accommodate them but it was out of his control. It was up to ACHD. He stood for questions.

Ms. Howell clarified it was a public meeting not a public hearing at ACHD.

Mr. Eck stated it might be open to public testimony but they could show up and ACHD would know that they were in support of the waiver request.

Council President Buban-Vonder Haar asked if the City could point them out.

Mayor Stear replied yes.

**Council President Buban-Vonder Haar moved to close the evidence presentation and proceed to deliberation. Seconded by Council Member McPherson. Motion carried 3-0-1. Council Member Christensen was absent.**

Council President Buban-Vonder Haar reviewed Council delayed this at the last Council Meeting so they could receive additional information from ACHD about why they denied the waiver and to see if there was anything they could do from that point. She was satisfied with what they had been able to accomplish and was hopeful that the hearing the next day would have a positive outcome for Chisum. At that point it sounded like the concerns had been dealt with in the sense that they were working to get the single connection instead of the 2. In terms of water and irrigation issues and that sort of thing, the delivery of and access of that service was protected and still be in place. She was certainly sympathetic to additional concerns about the safety of the roads and the adequacy of the space to facilitate vehicle and pedestrian traffic at the same time. She did feel like they were kind of stuck though in that there was a very thorough traffic study completed. ACHD, the entity in charge of regulating the roads, determined that what was presented to them was a safe and appropriate way forward and at that point she did not feel comfortable substituting her judgment, or lay judgement generally, in place of the entity that was tasked specifically with this function. She felt like they had done what they could in terms of going back to ACHD and that they would agree to even hear an additional request for the waiver since that process had already played out. That was where she was standing at that point. She asked what others thought.

Mayor Stear stated one of the reasons ACHD denied that was because they wanted more access for emergency vehicles and that type of thing. It was possible they could make those available for emergency access only, such as a gate that only police, fire, and ambulance could use. It was pointed out earlier there was 1 street that tied into everything. If there was an incident there everything was blocked off so it was legitimate to want a back entrance. If Council approved of that, it could be something he asked of ACHD in lieu of having access to Chisum Valley.

Council President Buban-Vonder Haar thought anything that could reduce access through Chisum Valley was what they were hoping for at that point.

Council Member McPherson agreed. If it could be limited to just emergency barricades and they had access to go around, that would definitely be best. He still did not like the traffic flow through there at all but, again, ACHD was the authority on streets so they were at their mercy and, more so, Chisum Valley was at their mercy, and the developer too. The water issues had somewhat been addressed. He knew they had to maintain those by rights so he was confident that would happen.

He just hoped this worked out. It had a lot of negativity and hoops it had to jump through. Hopefully there was a good conclusion the next day.

Council Member Cardoza stated it seemed like the developer had tried to accommodate the neighbors going with the half acres all except for Mr. Durrant. He found it ironic that they as a City were making a decision that kind of affected people in the county indirectly. It put them in an awkward position being that they were neighbors so to speak. On everything he had read in the past, he hadn't been there for 3 or 4 meetings, it looked like the developer had met every condition of the City. It seemed to be the City's problem and it was a problem the City seemed to manifest with other subdivisions as ACHD. The request from the engineer, from what he read, was that they proceed that night with the understanding that whatever decision was made the next day would be obeyed or conditioned by Tim Eck, if he understood that engineer. It gave them a little latitude if they took a big stick to ACHD and started swinging it hard not paying attention to who they hit, maybe they could get someone to make a decision that would benefit them. He wished, sitting there after some vacancy, that he had better news for them but, based on everything he read, Mr. Eck seemed to have the upper hand and had worked with them as much as he could. The problem seemed to be ACHD. Irrigation could be an issue but if they had the right-of-way, he thought Mr. Eck would honor that. Mr. Durrant's water was guaranteed by the Bureau of Reclamation. He hated to say it but, they were between a rock and a hard place. They wouldn't know what ACHD would do until the next day. He applauded Mayor and Council for trying to resolve this in favor of the people out in the county. He wasn't diminishing them living in the county. It was just that Council had no authority over those in the County but they wanted to be good neighbors to them.

**Council President Buban-Vonder Haar moved to direct staff to prepare Findings of Fact, Conclusions of Law and Order of Decision for approval by the Council on the next Council Meeting Consent Agenda approving 18-01-S with conditions of approval as listed and including the following condition that if, within six (6) months of this approval, the Ada County Highway District approves a waiver of its connectivity policy such that the applicant is not required to provide local street connections to either Chisum Way or McClintock Place, the applicant shall submit a revised preliminary plat to the City's Planning Director reflecting the connections allowed by the Ada County Highway District, and the City's Planning Director shall have the authority to approve such revised preliminary plat as a minor modification. The Ada County Highway District's refusal to grant any connectivity waivers will not affect the validity of this approval, but it is the applicant's obligation to cooperate with the City's efforts to obtain such a waiver. Seconded by Council Member McPherson. Motion carried 3-0-1. Council Member Christensen was absent.**

Mayor Stear stated they would do their best the next day to see what they could come up with and thanked everyone.

Someone from the audience asked if folks could submit email comments or if those wouldn't be considered because it wasn't a public hearing.

Planning & Zoning Director Wendy Howell did not know. She wasn't sure how ACHD ran their meetings.

Council President Buban-Vonder Haar suggested, subject to the Mayor saying it was a bad idea, that people share their comments with Mayor Stear if he was the one that was allowed to talk. That way he could summarize the comments for them.

Mayor Stear added that, since it wasn't a public hearing, talking to them would not create any ex parte. He shared his business cards with his contact information and that he would share any comments received with Planning & Zoning and the City Clerk for the record. Certainly, anything that they saw as beneficial they should share with him and reminded them the meeting was at noon.

Council President Buban-Vonder Haar added, even in the absence of receiving any additional feedback, the Mayor was well apprised of the concerns and interests and the City shared the same interests. Rest assured, they would speak on their behalf.

Mayor Stear reiterated the letter from earlier was pretty awesome.

Council President Buban-Vonder Haar agreed the letter from the Chisum Valley Board of Directors and Virginia Jeppeson was very thorough. She and Mayor Stear thanked everyone for showing up.

- B. Public Hearing and consideration to approve 18-05-AN (Annexation) and 18-06-S (Preliminary Plat) for Redcloud Subdivision – Troy Behunin, Planner III ACTION ITEM**  
(Timestamp 01:30:40)

The applicant, SSM2 Company, request annexation of approximately 8.00 acres into Kuna City with an R-6, residential zone, and to subdivide the property into 40 single family buildable lots and three (3) common lots. The site is located at the southeast corner of Luker and Great Bear Roads, the site address is 1250 S. Luker Rd., Kuna, Idaho; In Section 25, T 2N, R 1W, APN #: S1325438500. – Tom Maile Presenting.

Planner II Troy Behunin reviewed the staff report. He noted the minutes from the neighborhood meeting were not included in the packet. He passed out a brief summary of the minutes. He stood for questions.

Mayor Stear stated at some point they needed to do something different with these. He liked to know what concerns the neighbors had. That was the reason he liked to see the minutes and this didn't really do that for him. He didn't know how to have people change the way they took minutes but, other than date, time, and the list of people there, that was all it said. He did not find it very useful. He didn't know what

they were going to do but they needed to do something different, otherwise there was no point in having it in the packet.

Council Member Cardoza agreed.

Council Member McPherson suggested providing an outline to the developer or whoever to follow whenever they held a neighborhood meeting that listed what Council needed.

Council President Buban-Vonder Haar suggested comments in favor and comments against.

Mr. Behunin stated that would be asking for a vote.

Council President Buban-Vonder Haar clarified she was talking about comments that occurred during the meeting. The minutes already said the vast majority of the neighbors provided positive comments. She asked how that would be different than asking for them to provide the specific positive or negative comments.

Mr. Behunin asked Planning & Zoning Director Wendy Howell and City Attorney Bill Gigray to correct him if he was wrong but he thought the reason for the neighborhood meeting was a noticing procedure. They were not there to garner any kind of support or opposition. They were there to describe what they intended to do through application to the City. Asking if people were in favor or against was asking for a vote which could lead people to believe their vote counted when it came to that proceeding.

Council President Buban-Vonder Haar replied she would wait for Council to weigh in on whether or not what they were asking for would be permissible but, as an alternative, having some sort of a verbatim print out of what was described would at least allow, at the next stage at the public hearing, for people to say if that wasn't what they talked about or if that was what they talked about or point out what was originally proposed versus what was being proposed and which was in response to concerns that were brought up.

Mr. Behunin replied perhaps they could be more descriptive in their request on their applications saying a detailed topic itemization rather than just a general overview be proved.

Council President Buban-Vonder Haar was still skeptical that they wouldn't be permitted to know what concerns were raised at the meeting by citizens.

Mr. Behunin responded they certainly could ask for that but they could not ask people if they were for or against because that was essentially asking for a vote.

Council President Buban-Vonder Haar stated they could change the language. What they were hoping for was to avoid getting a sole and exclusive statement like the majority of the neighbors provided positive comments and getting more into what were the concerns, were they addressed, and what were the positive comments.

Mayor Stear agreed. He added that helped Council with their deliberation. If a project was denied they had to come up with some reasonable efforts the developer could do to satisfy things so Council could approve it. Things like this were beneficial because they would know what the concerns were from the beginning and how they were addressed through the process. He always felt like Council had a disadvantage because it had gone through all these people and they couldn't know anything about it until the night they sat there. He knew that was the process and the law and that was the way that was but when they got those things in the packet it made it easier for them to digest.

Mr. Behunin agreed they needed more detail.

Ms. Howell thought with their application where they asked for the minutes, they could specify they would like detailed minutes including conversations with the neighbors that took place, the presentation if there was one, and the pros and cons.

Council President Buban-Vonder Haar clarified they would like to specifically say positive feedback as well as concerns raised.

Ms. Howell said they could ask for that but she couldn't guarantee everything they put down was 100% accurate.

Mr. Behunin suggested they could guide that by saying topics discussed and concerns or suggestions on a template they could ask them to fill in at the neighborhood meeting.

Ms. Howell added their staff reports were also taking on a different flavor that would provide a more high-level overview of the preapplication meeting and detail out some of the information that was discussed. It was a work in progress so they would take comments and recommendations as they moved that forward.

Mayor Stear opened the public hearing.

Tom Maile, 885 W Rush, Eagle, Idaho 83616, on behalf of the applicant, presented the application and stood for questions.

Council Member Cardoza asked if Mr. Maile was familiar with if that area floods. It had flooded a couple times.

Mr. Maile said he was aware of it flooding once and reviewed the incident. He was advised of the flooding.

Council Member Cardoza also thought there were some waste ditches that ended up there and was aware of it flooding occasionally. He asked Mr. Maile if he was elevating the property and, if he did that, where the flow of water would go.

Mr. Maile replied they would not be interfering with the ingress and egress of drainage ditch. Those would be honored and they would tap into those. He thought they would put in a 3-foot pipe that would accommodate the drainage water in the southwest corner. They would be elevating the property a little bit but there shouldn't be any effect on any surrounding properties because it would all be pressurized irrigation within the subdivision. The only issue was the waste water that Council Member Cardoza was referring to which was the drainage ditch that was the overflow that caused the extensive flooding. They would deal with that by elevating the ground and piping in a culvert that could handle it. He hoped the drainage district had a better maintenance routine with gopher control. That was what he heard was the cause of the breach. Perhaps they learned a lesson in being more proactive in maintaining the ditch banks.

Support: None

Against: None

Neutral:

Mike Losh, 1032 S Threave Avenue, Kuna, Idaho 83634, stated they would be connecting on his street. They were good to work with. They did have their meeting and there weren't many comments because they all realized it was a nice area to live. He did sign as neutral and not opposition because of that. The only thing he had asked for was some open area for kids to play in. The dog park was on the north end of their subdivision and for this subdivision to get to that there weren't any real pathways but it sounded like he had worked on that. He hadn't seen the plat but he understood that he was working on that. He was really open with them and Mr. Losh appreciated that.

Rebuttal: None

Mayor Stear couldn't find the greenspace.

Council President Buban-Vonder Haar pointed it out on page 572 overall and page 21 of 72 of the packet as well as the path that went to it from the cul-de-sac. She noted he had said it was in the bottom southeast corner so adjacent subdivisions for future development could, if they wanted to, put their open space contiguous to it to allow for back and forth between the subs and provide greenspace for them to build off of.

**Council President Buban-Vonder Haar moved to close the evidence presentation and proceed to deliberation. Seconded by Council Member McPherson. Motion carried 3-0-1. Council Member Christensen was absent.**

Council President Buban-Vonder Haar appreciated the flexibility with changing the road per ACHD's suggestion and the inclusion of the open space at the request of Planning & Zoning. It appeared to be an appropriate zoning based on the surrounding properties and what the comp plan indicated, medium density residential, and it appeared it complied with all of the City's requirements as well as the comp plan.

Council Member Cardoza asked what the final count of houses per acre they intended to finish up with was.

Mr. Maile responded 40 houses or building lots for the acreage.

Council Member Cardoza clarified he wanted to know how many homes per acre they would end up with.

Mayor Stear stated 8 acres with 40 lots would be R-5.

Council Member Cardoza said the 3 common lots were throwing him off. He asked if they were going to end up with 4 homes per acre.

Council President Buban-Vonder Haar explained it would be 8 acres with 40 houses regardless of the common lots in terms of calculating the density so Mayor Stear was right; it would be 5.

Mr. Maile added the lot size varied. They had a good blend of different square footages on the lots but it ended up as 40 per 8.

**Council President Buban-Vonder Haar moved to direct staff to prepare Findings of Fact, Conclusions of Law and Order of Decision for approval by Council at the next Council Meeting approving 18-05-AN and 18-06-S for Redcloud Subdivision with the conditions of approval as listed in the packet. Seconded by Council Member McPherson. Motion carried 3-0-1. Council Member Christensen was absent.**

## **7. Business Items:**

- A. Consideration to approve 18-04-LS (Lot Split) for Kevin Allcot with Water Tower Development – Sam Weiger, Planner I **ACTION ITEM**  
(Timestamp 01:56:30)

Kevin Allcot seeks approval of one Lot Split. The subject property is located at 199 South Kay Avenue, Kuna, Idaho 83634.

Planner I Sam Weiger presented the staff report and stood for questions.

Council President Buban-Vonder Haar stated everything appeared to be in order, staff recommended approval, and she did not see any reason to disagree.

**Council President Buban-Vonder Haar moved to approve 18-04-LS. Seconded by Council Member McPherson. Motion carried 3-0-1. Council Member Christensen was absent.**

**B. Request for direction – Chris Engels, City Clerk ACTION ITEM**  
(Timestamp 01:59:22)

The Kuna Chamber of Commerce has declined the annual donation of \$2,500 for Kuna Days Fireworks. As part of a safety strategy for the City required Special Event Plan, bussing before and after the Fireworks display to satellite parking lots was requested to assist in limiting pedestrian/vehicle interaction, ease the parking burden of downtown residential areas and complaints from Union Pacific of illegal parking on railroad property. The Kuna Chamber of Commerce is asking the City to take the planning and financial responsibility for the bussing costs and scheduling.

City Clerk Chris Engels presented the situation to Council and requested direction. She noted the money from Council was allocated for fireworks so in order to change that they would need permission from Council. Also, this was not a service the City usually provided for any of the events that came to Kuna. There were some large ones that were nonprofit and another large for-profit event coming that year. She saw there were some Chamber members present.

Ms. Engels noted, for disclosure, a Chamber member had called and made a complaint to Mayor Stear about the bussing request and said that the Chief had said he wasn't concerned about safety at the fireworks. That didn't seem right since they had had multiple conversations. They all met, her, the Chief, Mayor, and Parks Director Bobby Withrow, and the Chief explained when the question was posed to him, he thought it was about the fireworks themselves which would have been the fire department who handled the fall out zone and that kind of thing.

Ms. Engels also stated, there was that question about closing Main Street. They had met with Karri Keller and a board member, she thought it was the representative from ICCU, on January 3<sup>rd</sup>. They discussed potentially, to assist with the pedestrian and vehicle interaction, closing down Main Street but they were clear that would be a City responsibility and would have to speak with Council about it to try and again limit the number of pedestrian/vehicle incidents. She thought that, in transparency, the Chamber said that she was requiring that but that was not the case. She showed Mayor the emails and he was able to get that resolved. She stood for questions and believed the Chamber members may be able to field questions as well if Council had any.

Mayor Stear asked if any of the Chamber members wanted to come up to talk about any of that.

Greg Decker, Kuna Chamber of Commerce Member, 5403 S Palatino Avenue, Meridian 83642, gave a little background on some of the challenges they had from a planning perspective and things they had implemented over the years and objectives they had as far as moving forward. The Kuna Days Planning was taken over by the Kuna Chamber of Commerce about 6 years prior from Pat Jones who previously ran it successfully for many years. Each year the Chamber had managed Kuna Days there had been changes and improvements implemented as they have seen an increase in participation from vendors and the community.

Mr. Decker explained previously Kuna Days was managed solely by volunteers. There were no formal road closures and no added security which put extra stress on local law enforcement. After Kuna days 2017, they again used strictly volunteers for the event and had a number of issues with volunteers not showing up or events being understaffed. In those cases, the Chamber and the committee scrambled to fill the staffing void. This caused a number of issues and diminished the overall community experience. Last year, in an effort to alleviate these issues, the chamber and the committee implemented the policy that required all applicants to be solely responsible for their area of participation.

Mr. Decker stated while the Chamber and committee volunteered hundreds of hours in planning and execution of Kuna Days, they simply lacked the human resources to provide additional staffing. For example, the Lion's Club managed the Car Show, the Boy Scouts managed the Pancake Breakfast, and the American Legion managed the Beer Garden. They provided no assistance to them for that process. Because of their limited human resources and in an effort to better coordinate the events, improve communication, and responsibilities they implemented the following changes. They hired a third party to manage kids' carnival. They managed that process from start to finish. They hired extra security. They hired a staffing company to provide traffic flaggers at the crosswalks and to provide park clean up. They added a formal road closure plan in accordance with ACHD requirements, added extra porta-potties, improved lighting and provided additional tents, and enhanced the audio and entertainment. Implementing these changes increased their expenses by \$10,000.

Mr. Decker reviewed they completed a debrief last year after the event and invited all interested parties to participate. They reviewed all concerns and implemented any changes needed to better improve the overall experience for all participants at this year's event. Feedback received from citizens to the Chamber was very positive. They received no direct feedback to the contrary from the City or otherwise. It was only later brought to their attention that there were safety concerns regarding pedestrian and auto traffic at the conclusion of the Saturday night fireworks show.

Mr. Decker stated this year the City notified the Chamber there would be required bus transportation provided during the hours of 8:00 pm and 11:00 pm on Saturday from satellite locations to help alleviate this congestion as part of obtaining their Special Event Permit. Without having the transportation in place, they would not be

issued the SEP. With their goals to provide a safe event, the Chamber and committee offered the City the following proposal. They would provide the following street closures; 2nd Street would be closed from Linder to Ave C for the entire duration of Kuna Days, which was 2 blocks more than what they had in the past, Main Street would be closed Saturday from 10:00 am to Midnight. In addition, Avenues A through D would be closed between Main and 2<sup>nd</sup> during this time. To provide continuity and clear communication the Chamber and committee would be solely responsible for managing this process and would incur all expenses associated with the closures. They would work with ACHD and their vendor to insure this was a smooth and safe process.

Mr. Decker further stated the Chamber and Committee would also pay the City's park fees that have been quoted and approved by the Parks and Rec Department dated January 3, 2019 and totaling \$854.00 for the entire weekend. They did not wish for any additional discounts. The Chamber and Committee would take care of all fireworks costs. They would not seek a donation from the City Council to support the cost of the fireworks show; which in the past had averaged about \$2,500.

Mr. Decker requested, with this in mind, that the City take full responsibility of planning, implementation, and coordination of the requested bussing program. As mentioned previously the Chamber did not have the human resources for this program. They hoped the Council would utilize cost savings outlined and reallocate those dollars to defray any costs associated with the City provided bus transportation. The City would provide detailed information of the bussing plan on the Chamber's website, social media, and any printed material the Chamber distributed to the public. He stood for questions.

Mayor Stear clarified Mr. Decker was saying this was the first they had heard about bussing for Kuna Days. He was confused because he thought it had been under discussion for several years.

Mr. Decker replied he was not aware that it was talked about for several years. He understood it was brought up in a meeting in early December. Prior to that he had no knowledge of any bussing requirement or request.

Mayor Stear thought that was interesting because he had been talking about it with VRT for 3 years at least.

Council President Buban-Vonder Haar asked if that was specifically for fireworks or just generally.

Mayor Stear explained the problem the police always had was when the fireworks were over and they had all these people crammed in the park and in the parking lot and then cars were trying to leave with pedestrians out. They had been talking about alleviating that with bussing.

Council President Buban-Vonder Haar asked if Valley Regional Transit had suggested they would like to be involved.

Mayor Stear replied no, that was when he had started with them but because of the federal funding they received they had to have a plan done years in advance. The City actually got bids from Caldwell Bussing. He was just surprised they hadn't heard of it before when it had been talked about for years and the police chief was pretty set on it. He didn't understand why it was all of the sudden something sudden.

Ms. Engels stated previous conversations in previous years had been with Pat Jones and Mary Ann Sugai, the former executive director and they were discussed with a previous chamber president. The discussions had been going for a number of years. Even with different chiefs. It went clear back to when Mayor Nelson was in office.

Mayor Stear guessed it was neither here nor there. What was before them was whether they wanted to try to staff for the bussing.

Council President Buban-Vonder Haar asked for clarification on what the Chamber wanted; either a \$2,500 fireworks donation or the City covering the \$500 or \$600 in bussing costs and scheduling that or were they totally uninterested in the fireworks money and the only involvement they would accept was the City paying and scheduling the bussing.

Mr. Decker explained it was not that they didn't want to accept. Their issue was that it would be required to provide bussing. The challenge they had was limited resources and they utilized the resources they had to take care of the management and responsibility and the planning. They just did not have the resources available to implement any other programs. That was their concern. What they were offering was to take on the responsibility. They believed the road closure would really help a lot of that situation. An issue they had the previous year was the number of cars parked on 2<sup>nd</sup> Street. They had utilized the parking lot for participant and vendor parking. That would be extremely limited to only vendor parking that year. They would close the streets in the hope that they would get rid of a lot of that car and pedestrian interaction.

Mr. Decker agreed that safety was the utmost concern and the congestion was an issue. It would be great if they could spread out the participants more throughout the greenbelt to other viewing areas, unfortunately people were people. They were happy to assist the City in trying to get the participants to spread out and not be so congregated. They looked at the possibility, like the City did, at trying to move the fireworks across the railroad tracks on the other side of the street there but there were limitations because of buildings and concerns from the fireworks company. Unfortunately, that put them back in the park which took away a significant area of the greenbelt.

Mr. Decker said there were definitely challenges. They would love to work with the City, police department, and security professionals to develop a plan to see what they could do to try to alleviate some of the congestion. They were exceptionally hopeful that closing those streets for the duration would make a significant impact. Their problem was the human resource issue. What they were offering was to take on the financial responsibility for the fireworks and road closures that would free up dollars for the City to be able to provide the appropriate bussing plan and the human resources as well as the vehicle resources to implement that.

Council President Buban-Vonder Haar asked if a potential solution would be for Council to clarify their fireworks donation could be used for anything related to getting the fireworks to happen which would include hiring someone to coordinate a shuttle service or were they just not interested in that.

Mr. Decker replied they had no problem with that and believed that was just semantics. All they were trying to do was shift some costs and did not want to incur an additional burden on the City and their resources. Their biggest challenge was getting bodies to volunteer and what they were trying to do was provide a first-class experience for the community. They realized there were challenges. The community was growing and it was getting more and more popular and they were getting more influxes of people so they had to come up with solutions. The only effective way to do that was to communicate clearly on how they could work together as a team to accomplish that.

Council President Buban-Vonder Haar had some concerns about the efficacy with which the City could carry out this bussing task given the Chamber was the puppet master of all the moving pieces in terms of what would be where and that sort of thing for the purposes of trying to figure out a good bussing schedule. She felt they might be better equipped to handle that than the City.

Council President Buban-Vonder Haar was also concerned about whether they were setting a precedent that the City would need to pay for transportation for other events that would happen downtown. Given the estimates that were in the email string about what potential costs might be for the bussing schedule, it was around \$75 per hour with a minimum of 3 hours. Her rough math was with 2 buses with a 4-hour time frame they would be looking at maybe \$600. Even if it was double that, her preference would be, but she could be persuaded, to say please use the \$2,500 the City allocated for fireworks for anything associated with getting the fireworks to happen which would include safety plans and all that sort of thing and in theory that would mean they would be able to potentially put \$1,200 towards the bussing and \$1,200 towards paying someone to figure out the bussing. She would love to hear others comments and if they had feedback.

Mr. Decker stated, as far as the estimate, he thought it was closer to \$1,000 to \$1,200 to provide that service but he wasn't a transportation expert. He didn't know if 2 buses would be sufficient or what anyone was thinking about as far as a specific plan

and time frames. They hadn't seen any proposed plans so he didn't know where they were talking about for the satellite locations and what it would take to do that. They had not explored the possibility of hiring someone to manage and coordinate that. It might be a possibility and they could look into it. It was not an answer he could give them that night. As for the allocation of dollars, he realized it was just semantics. They were just trying to work out a plan that was amenable and effective for everybody. At the end of the day the most important thing was to provide an outstanding experience for Kuna Days.

Mayor Stear said every year it was. It was getting better all the time.

Council President Buban-Vonder Haar wanted to be sure that, no matter the outcome, everybody knew they appreciated it and she understood the amount of work that went into this. She did not feel like a lot of people necessarily appreciated how time consuming it was for such a small group of people and it was always the same people volunteering. She didn't want anything she said to be taken as a lack of support for Kuna Days and the fireworks show. They were so grateful for all the work they did. She shared their interest in making sure this worked out for everybody in a way that wouldn't be over burdensome for anyone. She was on board with figuring out something that would work for everybody; whether it was that night or in the near future.

Council Member Cardoza asked if they were talking about just 3 hours for the bussing or all day long. If they looked at 8 hours at \$75 per bus that was approximately \$1,200. He asked where they would park and if the school district would allow them to use the school parking lots.

Mayor Stear replied those were the logistics that needed to be worked out but he thought the \$1,000 to \$1,200 estimate was probably where they would be at. They would probably have to come in before the fireworks for people who wanted to come in and wander around. It was definitely going to be longer than a couple of hours to make that happen. He couldn't imagine the school not being supportive of allowing people to park cars there so they could be picked up but that was a conversation that they had to have with them.

Council Member Cardoza wondered who would accept the responsibility if there was an accident on school property. He asked if the City would have to provide a waiver and if the City's insurance would cover a facility like that.

Council President Buban-Vonder Haar said they could ask counsel but she was not aware that the City would have any responsibility given that it was school district property and a private event.

Council Member Cardoza was wondering if the school district would let them use it without some kind of a waiver.

City Attorney Bill Gigray thought the best way to manage this would be through whatever City department would run this. This was clearly within their recreation and cultural authority, because it was an activity, for the City to be a co-sponsor. They could check with ICRMP on their coverage because, it was his understanding with ICRMP, if it was a City sponsored event they were covered. The thing there would be a logistic of what the City's participation would be in this project, what their right of control would be, and what was their insurance coverage if they had it. He suggested referring this back to the departments with the authority to explore it and a budget range. Then they could come back with a proposal on how they would want to administer it with the other participants. He felt, if they were involved in this, they should clearly define their involvement from the beginning knowing they had coverage for it and everyone would know where they stood.

Ms. Engels stated the previous year she and Parks Director Bobby Withrow had put together a satellite parking plan and indicated the distance to the park. They had communicated with the school district about satellite parking and they had no issues. They were fine with them listing the school parking lots and the distances. The school district was also covered by ICRMP. They suggested at the January 3<sup>rd</sup> meeting that potentially some of the businesses with large parking lots such as Ridley's or Albertsons be included in making parking available to the Chamber and then maybe they could be listed as sponsors.

Council Member Cardoza said if Avenue A and 2<sup>nd</sup> Street were closed, the parking lot where the ball fields were wouldn't have access and then if Avenue D was closed anybody coming from the west going east on Avalon would have to make a sharp turn and go to E to get on 4<sup>th</sup> Street. He was not sure they would want to close D.

Mr. Decker replied they were open to options on that and could discuss whatever challenges. This was what they were proposing in order to utilize Main Street and maybe add some additional events and things like that which would help alleviate some of the pressure in the park. He thought they had the wild card of the weather. They were going to use asphalt and paving which was an issue. As for the parking lot off 2<sup>nd</sup> Street, they were going to use that for vendor parking only. That would keep the traffic in and out and allow them to control that process much more stringently than in the past.

Council Member Cardoza stated he had complaints in the past about Avalon being closed off and the RV dump being inaccessible. He knew it was dangerous, some of those 5<sup>th</sup> wheels and trailers getting in there, but they had the right to go in there and dump. He would prefer to not see them go in there to dump but didn't know how stop them.

Mr. Decker said he hoped there would be enough knowledge to know that 1 weekend a year that might not be available and that they would be amenable as part of the community to work with them on that. Also, with the other issues associated

with dumping, the participants and vendors there and everything else, it would probably not be in everyone's best interest to do that.

Council Member Cardoza said he would be the first to say it was very dangerous.

Mr. Decker added they certainly realized that not everyone would be happy. They did their best but they weren't perfect. He also noted when talking about the hours for the buses, the clock started when they left their yard and stopped when they returned, so while they were looking at 3 hours of service here, there would also be trip charges to and from and that would factor into the cost. However, he thought the Mayor was probably right that \$1,000 to \$1,200 was the most reasonable and that's if 2 buses would handle that.

Mayor Stear said that was the thing. They would have to try it out for a year and see how things worked. It was kind of like the first year was when people first started getting used to something and they would participate more heavily the next year. If they could just whittle down the number of cars on that end it would pacify the issues the police had and would have.

Mayor Stear stated an issue with the City hiring out was they would have to put out an RFP, Request for Proposal. They had a lot of restrictions for hiring people for doing certain things and it would probably be less restrictive if that was to come from the Chamber. His preference would be to find some way to assist them in hiring somebody and, like Council President Buban-Vonder Haar said, maybe they could open up the availability of the funds for whatever they were needed for.

Council Member McPherson said that was the way he was leaning. His only question was on the satellite parking lots and who they would talk to but that had already been answered. He noted Avenue D was only closed from Main Street south, not north at all. If cars came into town from the west, they would still turn north on Avenue D.

Council Member Cardoza thought they were closing Avenue D.

Council Member McPherson replied they were from Main Street to 2<sup>nd</sup> so not north at all. He read that wrong the first time too. He also agreed with the Mayor that it would be better if it ran through the Chamber and that they just clarify how those funds could be spent.

Council President Buban-Vonder Haar suggested they proceed with the idea that the Chamber be empowered to spend the \$2,500 donation from the City for anything related to the fireworks show including bussing to observe the fireworks show and that they see if any of the shuttle companies would be willing, for an extra fee, to be the organizer of that shuttle service. She wanted them to see if, with that additional freedom, it would become a tenable solution that they would be able to facilitate

bussing with that money. They could come back to Council if they were running into trouble to see if there was any additional stuff they could try to do to fix those issues.

Mr. Decker said to do that they would need to see what the preliminary plan looked like. He didn't know if they had a map or plan or anything along those lines.

Ms. Engels didn't know what plan he was referring too. The intent was for them to develop the plan that worked for their event. The City didn't have an outstanding plan and had not designated satellite parking. That was for the Chamber. She was confident in saying the chief felt 1 or 2 buses, depending on how far spread out the satellite parking was, would be sufficient. He didn't expect any more than that. As far as satellite parking and where they wanted them to drop off and pick up, that was up to the Chamber to determine in their plan as it worked best for their event.

Council President Buban-Vonder Haar suggested, if Mr. Withrow had already spoken with someone from the school district the previous year about using school parking lots for pick up spots, he facilitate that contact. That could be a jumping off point.

Mr. Decker asked which schools had been identified to use.

Council President Buban-Vonder Haar was hoping Mr. Withrow could chat with them about that.

Mayor Stear suggested using the police chief to help with the plan because he would want to alleviate his concerns with that. He also asked which schools were contacted the previous year.

Mr. Withrow responded the high school and the middle school potentially but mainly the middle school because it was closer.

Ms. Engels also talked with Kim Bekkedahl about the elementary schools and she believed Dave Reinhart was on the Chamber as well and could be a good place to start.

The Chamber members present said Mr. Reinhart was not on the Chamber.

Karri Keller, with the Chamber of Commerce, stated they did want to work together. They did speak about bussing on January 3<sup>rd</sup> and that they would end their live entertainment at the park at 8:00 pm. The City would start their entertainment at 8:00 pm to try start moving people down and the bussing would help with that. That was when they discussed maybe participating together since it was not just their event that was happening between 8:00 and 10:00. They definitely noticed the congestion and they wanted to spread out on the greenbelt. The fireworks show was in a great location and could be seen from City Hall or wherever. That was something to consider. They were trying to help the City do their event as well as help with the

congestion. There was no contention between the City and the Chamber. She thanked Council for working with them.

Darius Elison, 11381 Gila Drive, Kuna, Idaho 83634, thought communication was the issue. He thought bussing, the way it was explained, was needed at that point but the reason they were considering disassociating from the City was lack of communication. When he called Mayor Stear, Mayor told him something different that he was saying that night.

Mayor Stear asked what he told him that was different.

Mr. Elison continued speaking, saying that the police chief was also not being consistent across the board. That was the reason they were there that night; because this could obviously be taken care of in a meeting where they all sat down together. There was a meeting where the police chief, Ms. Engels, and Mayor Stear were there and they spoke about the community and Kuna Days but the people putting on Kuna Days were not invited to the meeting. That was where the issue lied.

Mayor Stear asked again what he told him that was different that night.

Mr. Elison said when they talked, they talked about the bussing first coming up that year but now he was saying it had been discussed for a long time.

Mayor Stear stated he never said it first came up that year.

Mr. Elison continued, saying that Mayor Stear also made it sound like the bussing was required for both days for the duration of their event.

Mayor Stear responded there was no way he got that from anything he said.

Mr. Elison said it was only later on that he found out it was actually not specifically even during the times of their event but only during the times of the fireworks. He asked if they had the meeting or did that not happen either.

Ms. Engels stated the meeting she was referring to earlier when the police chief, Mayor Stear, herself, the Economic Development Director, and the Parks Director met was to discuss the email they received and Ms. Keller's questions.

Mr. Elison said it was a meeting about Kuna Days with out the other party that was putting it together.

Mayor Stear and Ms. Engels both stated it was a meeting to talk with the chief and respond to the email with questions about bussing.

Mr. Elison said it sounded like the meeting had a lot to do with another party that should have been there.

Mayor Stear asked if this was where they wanted to go because he said nothing Mr. Elison was saying he said.

Council President Buban-Vonder Haar moved forward asking if they were good with Mr. Withrow facilitating contact with the school district and that would be their jumping off point with the Chamber looping back to let them know if they were having success with figuring out bussing or if the City needed to get back involved to change things.

Mr. Decker replied a good path forward would be for them to talk with Mr. Withrow to find out what some of the preliminary ideas were and to talk with the police chief to find out what his suggestions were. Then they would see what would be appropriate and once they came up with a plan they could go out and see what it would cost them to implement that and what resources were available to maintain or manage that process. Then they could come back to Council to give them feedback as far as what they found and if there was an amenable solution. Again, at the end of the day their objective was a first-class event for this community and to be able to do that in a team work environment not in opposition of one another. That was their position and they were happy to work with the City and looked forward to it.

Council thanked the Chamber.

## 8. *Ordinances:*

### A. Consideration to approve Ordinance No. 2019-05 ACTION ITEM (Timestamp 02:41:09)

AN ORDINANCE OF THE CITY COUNCIL OF KUNA, IDAHO AMENDING TITLE 1, CHAPTER 6, SECTION 8 ENTITLED PUBLIC HEARING RULES OF PROCEDURE OF THE KUNA CITY CODE PROVIDING FOR:

- CLARIFICATION ON PUBLIC HEARING RULES OF PROCEDURE;
- ESTABLISHING TESTIMONY TIME LIMITS;
- REGULATING THE PRESENTATION OF DOCUMENTS AND OTHER FORMS OF PHYSICAL EVIDENCE;
- AMENDING THE RECOGNIZED PUBLIC HEARING MOTIONS;
- CLARIFYING CONFLICT OF INTEREST PROCEEDURES; AND
- PROVIDING FOR AN EFFECTIVE DATE AND PUBLICATION.

*Consideration to waive three readings*  
*Consideration to approve ordinance*

*Consideration to approve summary publication*

City Clerk Chris Engels explained the purpose of the ordinance. She noted, if Council were to approve this, on page 5 of 9 of 8A it read “when they are not compliance” and it should read “when they are not in compliance”. She wanted to be able to make that change as well as any other changes they might recommend. She stood for questions.

Council President Buban-Vonder Haar asked, other than having the rules in place and having more formality and consistency, if there was any reason they should approve that night versus doing 3 readings.

Ms. Engels was fine with 3 readings. It was just formalization of what they had been doing.

Council Member McPherson was fine with fast tracking it.

Council Member Cardoza said it was limiting the public but it was whatever the Council wanted.

Council President Buban-Vonder Haar was fine either way.

Council discussed fast tracking it or doing 3 readings and decided on 3 readings.

**B. Consideration to approve Ordinance No. 2019-06 ACTION ITEM**  
(Timestamp 02:44:33)

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF KUNA:

- MAKING CERTAIN FINDINGS; and
- ENLARGING THE BOUNDARIES OF THE KUNA MUNICIPAL IRRIGATION SYSTEM BY THE INCLUSION OF ADA COUNTY ASSESSOR’S SILVER TRAIL SUBDIVISION NO. 3; AND
- DECLARING WATER RIGHTS APPURTENANT THERETO ARE POOLED FOR DELIVERY PURPOSES; and
- DIRECTING THE CITY CLERK TO RECORD THIS ORDINANCE AS PROVIDED BY LAW; and
- DIRECTING THE CITY ENGINEER TO PROVIDE NOTICE OF THIS ORDINANCE TO THE BOISE-KUNA IRRIGATION DISTRICT, THE OWNERS AND UPDATE THE IRRIGATION SYSTEM MAP; and
- PROVIDING AN EFFECTIVE DATE.

*Consideration to waive three readings*  
*Consideration to approve ordinance*

**Council President Buban-Vonder Haar moved to waive three readings of Ordinance No. 2019-06. Seconded by Council Member McPherson. Motion carried 3-0-1. Council Member Christensen was absent.**

**Council President Buban-Vonder Haar moved to approve Ordinance No. 2019-06. Seconded by Council Member McPherson. Approved by the following roll call vote:**

**Voting Aye: Council Members Cardoza, Buban-Vonder Haar, and McPherson**

**Voting No: None**

**Absent: Council Member Christensen**

**Motion carried 3-0-1.**

### **9. Mayor/Council Announcements:**

*(Timestamp 02:46:10)*

Council President Buban-Vonder Haar asked for an update on the splash pad.

Parks Director Bobby Withrow stated they were getting closer. They were in the final stages and just buttoning up a few things. In the spring they would bolt everything up and start it.

Council President Buban-Vonder Haar asked if there was a target date for unveiling it. She assumed there would be some sort of big event like a ribbon cutting.

Mr. Withrow replied they would do a big event but they didn't have a date yet. He would work on that and get back to them at the next Council Meeting.

### **10. Executive Session:**

#### **A. Adjourn to Executive Session pursuant to:**

*(Timestamp 02:47:05)*

Idaho Code Section 74-206 (1)(c)

(c) To receive information regarding the acquisition of an interest in real property which is not owned by the City of Kuna. **ACTION ITEM**

**Council President Buban-Vonder Haar moved to adjourn to Executive Session pursuant to Idaho Code Section 74-206(1)(c) to receive information regarding the acquisition of an interest in real property which is not owned by the City of Kuna. Seconded by Council Member McPherson. Approved by the following roll call vote:**

**Voting Aye: Council Members Cardoza, Buban-Vonder Haar, and McPherson**

**Voting No: None**

**Absent: Council Member Christensen**

**Motion carried 3-0-1.**

Mr. Withrow would serve as special clerk to take the minutes of the Executive Session.

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**Executive Session City Council Regular Meeting of February 19, 2019** – The City Council, upon a unanimous roll call vote, convened into the Executive Session, pursuant to Idaho Code Section 74-206 (1)(c) to communicate with Parks Director Bobby Withrow in order to receive information regarding the acquisition of an interest in real property which was not owned by the City of Kuna. The executive session was convened at 8:58 p.m. In attendance, were Mayor Joe Stear, Council President Briana Buban-Vonder Haar, Council Member Richard Cardoza, Council Member Greg McPherson, and Parks Director Bobby Withrow and Council Member Warren Christensen was absent. Mr. Withrow was appointed as special clerk to take the minutes of this executive session. Mr. Withrow presented Council with a packet with information about land acquisition and Council discussed the possible purchase. At 9:04 p.m. Council President Briana Buban-Vonder Haar moved and Council Member Greg McPherson seconded to come out of executive session and to reconvene into open session which motion passed unanimously.

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**Open session resumed** and Mayor Stear reported that information was received and no action was taken by the City Council.

### ***11. Business Continued***

- A. Consideration to approve Land Acquisition – Bobby Withrow, Parks Director  
**ACTION ITEM**

Council agreed to the purchase price of the land acquisition.

**Council President Buban-Vonder Haar moved to authorize Parks Director Bobby Withrow to move forward with the land acquisition and site-specific memorial. Seconded by Council Member McPherson. Motion carried 3-0-1. Council Member Cardoza was absent.**

### ***12. Adjournment: 9:06 P.M.***

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Joe L. Stear, Mayor

ATTEST:

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Chris Engels, City Clerk

*Minutes prepared by Ariana Welker, Deputy City Clerk*  
*Date Approved: CCM 03.05.2019*



CITY OF KUNA

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SIGN-UP SHEET

February 19, 2019 – City Council, Public Hearing

Case Name: Preliminary Plat, Whisper Meadows; Request by Bailey Engineers.

Case Type: Applicant requests approval to subdivide approximately 73.50 acres (previously zoned R-6 and four lots within Chisum Valley Subdivision No. 1 and 2) into 310 single home lots, an additional 38 common lots, following the Comprehensive Plan of MDR and the Council approved conditions of approval from the recent rezone. This site is located at the NWC Linder and Columbia Roads. APN No's; R1393850100, R1693860010, R1693860290, R1693860280.

Case No.: 18-01-S (Prelim Plat) - A Pre Plat to Subdivide Approx. 73.50 acres.

Please print your name below if you would like to present oral testimony or written exhibits about this item to the Commission/Council.

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Kuna City Council  
Kuna City Hall  
751 W 4<sup>th</sup> Street  
Kuna, Idaho 83634

To Whom It May Concern:

Following the February 5, 2019, city council meeting, the Chisum Valley HOA's Board of Directors met to discuss and organize its thoughts relating the stub-street connectivity issues that came up at the meeting. We have outlined those thoughts here and respectfully request the council consider them prior to making a decision relating to the Whisper Meadows subdivision.

**Background.**

- ACHD Policy 7207.2.4 requires stub streets to be extended to provide circulation between adjoining properties. However, the regulations provide that ACHD reserves the power to waive any adopted policy on a "case-by-case basis." (See Policy 1002)
- Whisper Meadows requested that ACHD waive its policy of requiring both stub streets in Chisum Valley to be extended. Chisum Valley supports that request. ACHD staff recommended approval of the waiver. In its report, Staff stated:

Staff recommends approval of the applicant's proposal to not extend McLintock Place into the site, as it is not desired by the City of Kuna and a pedestrian connection will be provided to provide connectivity between the two developments. The street is not necessary to serve the site, and the turnaround is fully improved. (07/17/2018 ACHD Staff Report at 8(d)).
- The commission considered Whisper Meadow's waiver request at its September 26, 2018, meeting. Of note, no one from Chisum Valley was provided notice of the meeting and no representative from Chisum Valley was in attendance.
- The commission voted to deny the waiver request. It provided no basis for this decision.
- At the February 5, 2019, city council meeting, ACHD representative David Corcoran testified as follows regarding ACHD stub street extension regulations and the possible bases for the denial. Mr. Corcoran's testimony begins at approximately 1:06 in the recorded meeting minutes.

- That ACHD regulations are flexible. The regulations provide that “[t]he street design in a proposed development shall cause no undue hardship to adjoining property.” (See Policy 1207.2.4.1.)
- That, under the regulations, factors ACHD should consider when assessing stub street development include property size and configuration of the proposed subdivision and adjacent parcels, the needs of emergency service providers, and the location or existing stub streets. (See Policy 1207.2.4.1.)
- That ACHD has a “broad policy” that when there’s a stub street, it gets extended. (02/05 Meeting Minutes at 1:09)
- That whether a stub street is extended is not based on the particular subdivision but on a “blanket sort of policy.” (02/05 Meeting Minutes at 1:09).

#### Reasons for not connecting the subdivisions.

- As noted above, the commission provided no basis for its refusal to grant the waiver. Further, Mr. Corcoran testified that ACHD has a blanket policy of requiring connectivity without case-by-case consideration of the circumstances. While no good reason has been provided for requiring the connection, the connection would create undue hardship for Chisum Valley and there are good reasons for granting the waiver.
  - SAFETY. Chisum Valley Subdivision is a rural subdivision. There are no sidewalks and no barriers protecting pedestrians from street traffic. Further, there are no traffic signs or painted lines on the road. The increased traffic resulting from the connection puts Chisum Valley’s pedestrians, bike riders, and horse riders in danger.
  - PROPERTY SIZE AND CONFIGURATION.
    - As Mr. Corcoran testified, ACHD should consider property size and configuration when assessing stub-street connectivity.
    - Whisper Meadows is a 310-home subdivision. Chisum Valley consists of just over 30 homes.
    - When Chisum Valley was developed it was 160.24 acres in size and encompassed both present-day Chisum Valley and the proposed site for Whisper Meadows. The area proposed for Whisper Meadows was to be maintained as open-space lots for at least 15 years after Chisum Valley was developed.
    - Initially, one of the conditions of approval for Chisum Valley was that all building lots were to maintain a minimum of one hundred feet of street frontage. (Exhibit A, page 17.)
    - On January 24, 1995, the engineer designing Chisum Valley sent Ada County Development Services a letter in which he requested that the language of the conditional approval be changed. According to the developer, the 100’ street frontage requirement was “generally good, especially for the future development of deed restructured agricultural property that lacks appropriate street access. However, it is . . . limiting to [creative open space uses such as pathways].” (Exhibit A, page 9.)

The developer proposed modifying the language to state: "All residential building lots and deed restricted agricultural open-space lots shall maintain a minimum of one hundred feet of street frontage." (Exhibit A, page 9.)

- The language was amended in accordance with the developer's letter and approval of Chisum Valley was conditioned on all future developments having at least one-hundred feet of street frontage.
- Obviously, at the time it was approved, it was contemplated that the entire 160.24-acre lot would be developed into larger residential lots similar to those already developed in Chisum Valley. It would be reasonable for Chisum Valley to connect to such similar subdivisions, but Chisum Valley was not developed nor is it designed to connect to a dense 310-home subdivision of much smaller lots.
- **NEED.** There is no need for both stub streets to connect to Whisper Meadows. As has been noted several times, this is likely to actually decrease safety instead of increasing safety by making Chisum Valley more accessible to emergency vehicles. Further, McLintock Way is not a through street. It simply connects back to Chisum Way. Accordingly, there is absolutely no need for both McLintock and Chisum Way to be extended into the subdivision.
- **LAND-OWNER DESIRES.** Neither the developer nor the Chisum Valley landowners want the subdivisions to be connected.

Requested action.

Chisum Valley respectfully requests that city council condition approval of Whisper Meadows development on Whisper Meadows successfully obtaining a waiver of policy from ACHD waiving stub-street connection between the two subdivisions. The connections serve no benefit to the subdivisions but are detrimental to Chisum Valley and create undue hardship for the reasons described herein.

Thank you for your time and consideration.

Sincerely,



Virginia Rennae Jeppesen (Feb 15, 2019)

Virginia Jeppesen, on behalf of the  
Chisum Valley Board of Directors



**TO:** BOARD OF ADA COUNTY COMMISSIONERS

**DATE:** March 24, 1999

**STAFF:** Daren Fluke, AICP, Planner III

**FILE NO:** 94-66-PDR-NF - Final Plat (Chisum Valley Subdivision #2)

**OWNER/  
APPLICANT:** Gerald & Evelyn Butler / Chisum Valley Subdivision  
6223 N. Discovery Way, Suite 100  
Boise, Idaho 83713

**AGENT:** Briggs Engineering

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**SUMMARY OF THE RECORD**

The applicant is seeking approval of a final plat for the second phase of Chisum Valley Subdivision. Phase one included Lot 1, Block 1 of the approved subdivision. Phase II contains the balance of the approved lots including 32 buildable lots and 6 open space lots. The property is located on the northwest corner of Columbia Road and Linder Road; Southwest Quarter, Section 2, Township 2 North, Range 1 West. The current zoning on the property is RR (Rural Residential).

The application is comprised of:

1. Final plat of Chisum Valley Subdivision #2.
2. All other information contained in Files # 94-66-PDR-NF.

**PROCEDURAL ITEMS**

1. On January 26, 1995 the Board approved the preliminary plat and adopted Findings of Fact and Conclusions of Law for File # 94-66-PDR-NF.
2. On February 14, 1996 the Board approved a time extension for File # 94-66-PDR-NF.
3. On April 9, 1997 the Board approved the final plat for Phase I of Chisum Valley Subdivision consisting of one common lot.
4. On February 11, 1998 the Board approved a time extension for Phase II of File # 94-66-PDR-NF.
5. On February 11, 1999 Development Services received File # 94-66-PDR-NF-final plat and scheduled it for review by the Commission on February 25, 1999.
6. On February 11, 1999, the Planning and Zoning Commission voted to recommend approval of File #94-66-PDR-NF - Final Plat to the Board. Based upon the recommendation of the Commission, staff scheduled the application for review and approval by the Board on March 24, 1999.

### APPLICABLE LAW

1. Section 50-1308 of the Idaho Code states that if a subdivision is not within the corporate limits of a city, the plat shall be submitted, accepted and approved by the Board of Commissioners of the county in which the tract is located in and if the county has established a planning commission, then all plats must be submitted to the Commission.
2. Section 8-10-4-3 of the Ada County Code establishes the procedures for approval of a final plat.

### STAFF ANALYSIS

The applicant is seeking approval of a final plat for File # 94-66-PDR-NF (Chisum Valley Subdivision). The Board approved Phase I on April 9, 1998 with the conditions of approval requiring approval of Phase II within one year. On February 11, 1999 the Board approved a time extension for submittal of Phase II. Staff has reviewed the Conditions of Approval in Exhibit A and finds that the required sign-offs have been submitted by the applicant and the final plat conforms substantially to the approved preliminary plat.

### FINDINGS OF FACT

If any of these Findings of Fact are deemed to be Conclusion of Law, they are incorporated into the Conclusions of Law section.

1. Based upon the above noted procedural items and the evidence and testimony in the record the Board finds that this final plat is not within the corporate limits of a city and as such, is under the jurisdiction of Ada County and has been reviewed by the Commission in accordance with the provisions of Section 50-1308 of the Idaho Code.
2. The Board finds that the final plat conforms substantially to the approved preliminary plat in that it contains 38 lots configured in substantially the same manner as the preliminary plat. In addition, based upon the above noted procedural items, the Board finds that the final plat was submitted and acted on by the Board in accordance with the provisions of Section 8-10-4-3 of the Ada County Code.

### RECOMMENDATION

Based upon information contained in the record, the Commission's review of the final plat as presented, and the Conditions of Approval adopted in the preliminary plat approval, the Commission concludes that this proposal meets the tests of Section 8-10-4-3 of the Ada County Code pertaining to the approval of final plats and Section 50-1308 of the Idaho Code pertaining to the processing of final plats and as such the Commission recommends that the Board approve the final plat and direct the Chairman to sign on their behalf.

### CONCLUSIONS OF LAW

If any of these Conclusions of Law are deemed to be Findings of Fact, they are incorporated into the Findings of Fact section.

1. The Board concludes that this final plat review complies with the provisions of Section 50-1308 of the Idaho Code.
2. The Board concludes that this final plat complies with Section 8-10-4-3 of the Ada County Code.

### ORDER

Based upon the Findings of Fact and Conclusions of Law reviewed above, the Board hereby approves the final plat for Chisum Valley Subdivision #2, as submitted by Briggs Engineering on behalf of Gerald & Evelyn Butler / Chisum Valley Development, and as documented in File # 94-66-PDR-NF, and authorizes the Chairman to sign on their behalf.

### ATTACHMENTS

Exhibit A - Conditions of Approval  
Vicinity map  
Final Plat of Chisum Valley Subdivision #2

## EXHIBIT B

CONDITIONS OF APPROVAL (1-19-95)  
FILE NO. 94-66-PDR/NF (DUKE'S COUNTRY ESTATES)

1. ~~ok~~ All public rights of way shall be dedicated and constructed to standards of the Ada County Highway District. Any work within the Ada County Highway District rights of way requires a permit. For information regarding the requirements to obtain a permit, contact the Ada County Highway District Development Services at 345-7662. Your application file number (94-66-PDR/NF) will be required.
2. ~~ok~~ Prior to submission of the final plat, a surface runoff drainage plan shall be submitted and approved by the Ada County Engineer.
3. ~~ok~~ Compliance with Section 31-3805 of the Idaho Code pertaining to irrigation water is required.
4. ~~ok~~ Installation of fire protection facilities as specifically required by the Kuna Fire District. Prior to approval of the final plat, written approval from the Kuna Fire Protection District is required.
5. ~~ok~~ All new utilities shall be installed underground. Installation of service facilities are to comply with the requirements of the public utility providing the services. All utility easements shall comply with the utility recommendations and Section 8-10A-6A of the Ada County Code (... easements for the utilities... of a width of a minimum of twelve feet (12')...). All utility easements shall be shown on the final plat.
6. ~~ok~~ The preliminary and final plats shall be in conformance with Section 8-10-4 and Sections 8-10A-1 through 8-10-A-7 of the Ada County Code.
7. ~~ok~~ Building setbacks and dimensional standards in this subdivision shall be in compliance with the applicable zoning regulations of Ada County. This statement shall appear on the face of the final plat.
8. ~~ok~~ All site improvements are prohibited prior to approval of the preliminary plat, approval of the surface runoff drainage plan by the Ada County Engineer, and the approval of the appropriate public agency having jurisdiction over the facility being constructed.
9. ~~ok~~ Prior to approval by the Board of County Commissioners, the final plat shall contain the following certificates and endorsements: signature of the owner(s); certificate of the plat engineer; certificate of the County Engineer; endorsement

of the Central District Health Department; and, the endorsement of the Ada County Highway District.

10.  The following statement shall appear on the face of the final plat: "Any re-subdivision of this plat shall comply with the applicable regulations in effect at the time of the re-subdivision."
11.  Irrigation/drainage waters shall not be impeded by any construction on site.
12.  All easements and public rights of way shall be shown on the final plat.
13.  All street and subdivision names shall be reviewed by the Street Name Committee and approved by the Ada County Engineer prior to submitting the final plat for approval (§§ 2-1-4B2 & 8-10-5-1, Ada County Code).
14.  Any internal irrigation and drainage must be approved by the applicable irrigation district and/or by the Ada County Engineer prior to submitting the final plat for approval.
15.  All irrigation ditches, laterals, and/or canals intersecting, crossing, or lying adjacent and contiguous to this development shall be covered or fenced with appropriate materials (in accordance with Ada County Code §8-10A-7). Natural waterways are exempted from this requirement. Any covering or fencing shall have the prior approval from the applicable irrigation district and/or the Ada County Development Services Department. Compliance with this covering/fencing requirement shall be verified by Development Services staff prior to issuance of the first building permit in the development; or, building permits may be issued if the Department of Development Services accepts a Bond in the amount of 120% of the higher of two bids to complete this and/or other incomplete conditions of approval. Contact Development Services or additional information regarding the posting of Bonds.
16.  All building lots within this development shall be at least one acre (43,560 square feet) net in size. All building lots and deed restricted agricultural open-space lots shall maintain a minimum of one hundred feet (100') of street frontage.
17. The following statement shall appear on the face of any final platting:  
 (Legal description and common description of property) is a deed restricted lot and may only be used for open space as defined in the non-farm development section of the Ada County Code, Section 8-4B-7 and in the planned development provisions of the Ada County Code found in Title 8, Chapter 9. The deed restricted lot must also only be used in the manner specified in the conditions of approval issued by Ada County's Department of Development Services. This lot must be used in the

aforestated manner for a period of not less than fifteen (15) years from the recording date of \_\_\_\_\_ (fill in the appropriate date).

18.  The open space lots within this development are approved for the uses listed on the face of the preliminary development plan. Any changes in the open space plan, use, or function shall require review by Development Services and approval by the Board of Ada County Commissioners.
19.  The final plat must be approved by the Board within one (1) year of the Board's approval of the preliminary plat, unless a time extension is granted as per Ada County Code Section 8-9-3C7.
20.  No building permits shall be issued until the final plat is recorded.
21.  The development plan requires the installation of individual sanitary sewer systems and individual domestic water systems; these systems shall be approved by the Central District Health Department. Final approval and/or permits from the Central District Health Department are required and must be delivered to Development Services prior to the issuance of any building permits on individual lots.
22.  Prior to the submittal of the final plat for approval by the Board, written approval from the following agencies shall be submitted to Development Services. These approvals may be either on agency letterhead referring to the approved plan or they may be written/stamped on the approved plan.
  - a.  Idaho Dept. of Water Resources
  - b.  Kuna Fire District
  - c.  Boise-Kuna Irrigation District
23.  The final plat of any phase shall be in conformance with Section 8-4B-7E of the Ada County Code.
24. The final plat must be recorded with the County Recorder's Office within one (1) year of the Board's approval of the final plat as per Ada County Code Section 8-9-3D5(A).
25. These notes shall appear on the face of the final plat:
  - This development recognizes Section 22-4503, Idaho Code, Right to Farm, which states, "No agricultural operation or an appurtenance to it shall be or become a nuisance, private or public, by any changed conditions in or about the surrounding nonagricultural activities after the same has been in operation for more than one (1) year, when the operation was not a nuisance at the time the operation began; provided, that the provisions of this section shall not apply



## ADA COUNTY DEVELOPMENT SERVICES

650 Main, Boise, Idaho 83702

BUILDING DIVISION — Phone: (208) 364-2277

PLANNING & ZONING — Phone (208) 364-2277

November 22, 1994

Colleen Kisler  
3331 Grey  
Nyssa OR 97913

RE: 94-66-PDR-NF DUKE'S COUNTY ESTATES

Dear Ms. Kisler:

This is to notify you that your application has been accepted and is scheduled to be heard by the Ada County Planning and Zoning Commission on December 8, 1994. This meeting will be held in the Les Bois Room of the Boise City Hall, 150 N. Capitol Blvd., and is scheduled to begin at 7:00 p.m. You or your designated representative must be present, or no action may be taken.

A copy of the Staff report will be sent to you prior to the meeting. Please contact me if you have any questions or comments regarding this application, the staff report, or any conditions which may be attached to the staff report.

In accordance with State law, a "Notice of Public Hearing" must be posted on the site. A staff member from Development Services will come to the site with the notice prior to the public hearing. **DO NOT** remove the notice once the notice is posted. After the public hearing, Development Services staff will retrieve the notice.

Sincerely,



Scott Cook  
PLANNER II

cc: Briggs Engineering, Van Elg

SC/dd



January 24, 1995

Scott Cook  
Ada County Development Services  
650 Main Street  
Boise, Idaho 83702

Re: Duke's Country Estates (Chisum Valley Subdivision)

Dear Scott,

We sincerely appreciated your help and your open-minded approach with this development! In addition, we thank you for accepting these proposed modifications to the Conditions of Approval. While we believe that the conditions of approval are generally acceptable, the unique nature of this development should be considered.

As discussed in the public hearing, we suggest the following changes to the Conditions of Approval:

Conditions #15 and #27:

These fencing/covering conditions are addressed in §8-10A-7 and §8-4B-7E5. The fencing requirement may be waived and fencing or buffering is allowed as a means of separating agricultural open-space areas. However, rather than waive the requirement, the owner would simply like the option of allowing each individual lot owner the freedom of constructing a fence, buffer, or covering that suits their individual styles--prior to the issuance of residential building permits. This is not an arduous or entirely new concept/condition and we will duly note this requirement on the face of the plat and in the homeowner's covenants. We suggest that a note also be placed on the front desk zoning map--as a reminder. We suggest that conditions 15 and 27 be combined to read as follows:

15. Prior to the issuance of each individual residential building permit, applicable fencing, buffering or covering shall be installed to separate adjacent uncovered ditches, canals, waterways, and agricultural

940809  
Chisum Valley Subdivision  
Formerly Duke's Country Estates

1111 S. Orchard, Suite 600 • Boise, Idaho 83705 • (208) 344-9700 Fax# (208) 345-2950

**BRIGGS ENGINEERING, INC.** 1111 S. Orchard, Suite 600 • Boise, Idaho 83705 • (208) 345-2881 FAX # (208) 345-2950

open-space areas. Such installation shall be in conformance with applicable Ada County Codes (i.e. §8-10-A-7 and §8-4B-7E5) and shall be reviewed and approved by Development Services staff prior to installation.

Condition #16 and #19:

This condition addresses the 100' minimum street frontage. The requirement is generally good, especially for the future development of deed restricted agricultural property that lacks appropriate street access. However, it is rather difficult and limiting when it comes to being creative with alternate open-space uses (i.e. pocket parks and pathways). Please consider the following combination and modification:

16. All residential building lots within this development shall be at least one acre (43,560 sf) in size. All residential building lots and deed restricted agricultural open-space lots shall maintain a minimum of one hundred feet (100') of street frontage.

Condition #28:

The plans for the open-space areas are quite clear and additional Board review is really not required. The development of the baseball fields will be handled under a separate review by the Planning and Zoning Commission. We suggest that the condition be amended as follows:

26. Parking requirements created by non-agricultural open space areas shall be accommodated on site with no parking in public rights-of-way. All parking/access areas shall be paved and striped. Substantial wheel restraints shall be installed around landscaped areas. Review and approval by Development Services staff is required prior to installation.

Condition #29:

This condition addresses the proposed ball field area by requiring a conditional use permit. We agree with the condition; however, we would like the following provisions added:

27. ...In addition, the following provisions shall apply:
  - A. No artificial lighting shall be allowed at the proposed ball fields without written approval of

940809

Chisum Valley Subdivision  
Formerly Duke's Country Estates

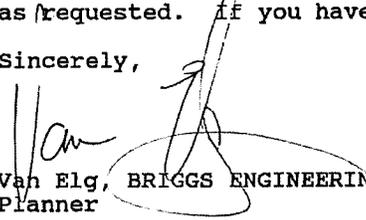
**BRIGGS ENGINEERING, INC.** 1111 S. Orchard, Suite 600 • Boise, Idaho 83705 • (208) 345-2881 FAX # (208) 345-2950

the adjacent landowners, the homeowner's association, and the Planning and Zoning Commission.

- B. The developer and/or the Homeowner's association shall be held harmless from any and all actives performed on this open-space site.
- C. The open-space area shall be appropriately maintained so that it does not become a nuisance or blight to the development. Functional completion of the recreational area must occur by January, 1997 or the issue must be revisited by the Board of County Commissioners at the first available public hearing in February, 1997. If bona fide plans to develop the site are presented at the hearing, then a completion deadline of December, 1998 may be allowed. If plans are not presented or if the site is not completed as scheduled (in general accordance with the plans presented at the February, 1997 hearing) then the property shall be deeded back to the grantor (Kesler) as deed restricted open-space agricultural property. The property shall then become eligible for development consideration with the expiration of the 15 year deed restriction placed on the other large agricultural lots in the development.

Scott, thank you for taking the time to discuss this application. I will attend the Tuesday morning meeting with the Commissioners, as requested. If you have any questions, please give me a call.

Sincerely,

  
Van Elg, BRIGGS ENGINEERING, Inc.  
Planner

940809  
Chisum Valley Subdivision  
Formerly Duke's Country Estates

Staff  
Copy

**CONDITIONS OF APPROVAL (1-19-95)  
FILE NO. 94-66-PDR/NF (DUKE'S COUNTRY ESTATES)**

**ATTACHMENT A**

1. All public rights of way shall be dedicated and constructed to standards of the Ada County Highway District. Any work within the Ada County Highway District rights of way requires a permit. For information regarding the requirements to obtain a permit, contact the Ada County Highway District Development Services at 345-7662. Your application file number (94-66-PDR/NF) will be required.
2. Prior to submission of the final plat, a surface runoff drainage plan shall be submitted and approved by the Ada County Engineer.
3. Compliance with Section 31-3805 of the Idaho Code pertaining to irrigation water is required.
4. Installation of fire protection facilities as specifically required by the Kuna Fire District. Prior to approval of the final plat, written approval from the Kuna Fire Protection District is required.
5. All new utilities shall be installed underground. Installation of service facilities are to comply with the requirements of the public utility providing the services. All utility easements shall comply with the utility recommendations and Section 8-10A-6A of the Ada County Code (... easements for the utilities... of a width of a minimum of twelve feet (12')...). All utility easements shall be shown on the final plat.
6. The preliminary and final plats shall be in conformance with Section 8-10-4 and Sections 8-10A-1 through 8-10-A-7 of the Ada County Code.
7. Building setbacks and dimensional standards in this subdivision shall be in compliance with the applicable zoning regulations of Ada County. This statement shall appear on the face of the final plat.
8. All site improvements are prohibited prior to approval of the preliminary plat, approval of the surface runoff drainage plan by the Ada County Engineer, and the approval of the appropriate public agency having jurisdiction over the facility being constructed.

9. Prior to approval by the Board of County Commissioners, the final plat shall contain the following certificates and endorsements: signature of the owner(s); certificate of the plat engineer; certificate of the County Engineer; endorsement of the Central District Health Department; and, the endorsement of the Ada County Highway District.
10. The following statement shall appear on the face of the final plat: "Any re-subdivision of this plat shall comply with the applicable regulations in effect at the time of the re-subdivision."
11. Irrigation/drainage waters shall not be impeded by any construction on site.
12. All easements and public rights of way shall be shown on the final plat.
13. All street and subdivision names shall be reviewed by the Street Name Committee and approved by the Ada County Engineer prior to submitting the final plat for approval (§§ 2-1-4B2 & 8-10-5-1, Ada County Code).
14. Any internal irrigation and drainage must be approved by the applicable irrigation district and/or by the Ada County Engineer prior to submitting the final plat for approval.
15. All irrigation ditches, laterals, and/or canals intersecting, crossing, or lying adjacent and contiguous to this development shall be covered or fenced with appropriate materials (in accordance with Ada County Code §8-10A-7). Natural waterways are exempted from this requirement. Any covering or fencing shall have the prior approval from the applicable irrigation district and/or the Ada County Development Services Department. Compliance with this covering/fencing requirement shall be verified by Development Services staff prior to issuance of the first building permit in the development; or, building permits may be issued if the Department of Development Services accepts a Bond in the amount of 120% of the higher of two bids to complete this and/or other incomplete conditions of approval. Contact Development Services or additional information regarding the posting of Bonds.
16. All building lots within this development shall be at least one acre (43,560 square feet) net in size. All building lots and deed restricted agricultural open-space lots shall maintain a minimum of one hundred feet (100') of street frontage.
17. The following statement shall appear on the face of any final platting:  

(Legal description and common description of property) is a deed restricted lot and may only be used for open space as defined in the non-

farm development section of the Ada County Code, Section 8-4B-7 and in the planned development provisions of the Ada County Code found in Title 8, Chapter 9. The deed restricted lot must also only be used in the manner specified in the conditions of approval issued by Ada County's Department of Development Services. This lot must be used in the aforesaid manner for a period of not less than fifteen (15) years from the recording date of \_\_\_\_\_ (fill in the appropriate date).

18. The open space lots within this development are approved for the uses listed on the face of the preliminary development plan. Any changes in the open space plan, use, or function shall require review by Development Services and approval by the Board of Ada County Commissioners.

→ REMOVED FORMER CONDITION #11, COMBINED WITH #16

19. The final plat must be approved by the Board within one (1) year of the Board's approval of the preliminary plat, unless a time extension is granted as per Ada County Code Section 8-9-3C7.
20. No building permits shall be issued until the final plat is recorded.
21. The development plan requires the installation of individual sanitary sewer systems and individual domestic water systems; these systems shall be approved by the Central District Health Department. Final approval and/or permits from the Central District Health Department are required and must be delivered to Development Services prior to the issuance of any building permits on individual lots.
22. Prior to the submittal of the final plat for approval by the Board, written approval from the following agencies shall be submitted to Development Services. These approvals may be either on agency letterhead referring to the approved plan or they may be written/stamped on the approved plan.
- a. Idaho Dept. of Water Resources
  - b. Kuna Fire District
  - c. Boise-Kuna Irrigation District
23. The final plat of any phase shall be in conformance with Section 8-4B-7E of the Ada County Code.
24. The final plat must be recorded with the County Recorder's Office within one (1) year of the Board's approval of the final plat as per Ada County Code Section 8-9-3D5(A).

- 25. These notes shall appear on the face of the final plat:

This development recognizes Section 22-4503, Idaho Code, Right to Farm, which states, "No agricultural operation or an appurtenance to it shall be or become a nuisance, private or public, by any changed conditions in or about the surrounding nonagricultural activities after the same has been in operation for more than one (1) year, when the operation was not a nuisance at the time the operation began; provided, that the provisions of this section shall not apply whenever a nuisance results from the improper or negligent operation of any agricultural operation or an appurtenance to it."

"Direct vehicular access onto Columbia Road from Lot 1, Block 2, and Lots 23 and 24, Block 1 is prohibited."

- 26. Fencing/buffering shall be installed to separate all building lots from the open space/agricultural lots as per Section 8-4B-7E5 of the Ada County Code. Appropriate fencing/buffering for each lot in the applicable phase of the development must be reviewed, completed, and approved prior to the issuance of any permits in the phase. Fencing/buffering shall also be installed to separate all building lots from all recreational open space lots.

- 27. Parking requirements created by non-agricultural open space areas shall be accommodated on site with no parking in public rights-of-way. All parking areas and driveways shall be paved, striped and provided with substantial wheel restraints around landscaped areas. Prior to installation of improvements for the proposed activity area (Lot 8, Block 1 as indicated on the preliminary plat) detailed plans shall be submitted and approved by Development Services staff.

- 28. Prior to the issuance of any permits for the youth league facilities (Lot 25, Block 1 as indicated on the preliminary plat) a conditional use permit shall be approved by the Ada County Planning and Zoning Commission. ~~No artificial lighting shall be permitted on the site unless otherwise approved by the Planning and Zoning Commission.~~

- 29. The youth league facilities (Lot 25, Block 1 as indicated on the preliminary plat) shall be appropriately maintained so that it does not become a nuisance or blight to the development. Functional completion of the recreational area must occur by January, 1997 or the issue must be revisited by the Board of County Commissioners at the first available public hearing in February, 1997. If bona fide plans to develop the site are presented at the hearing, then a completion deadline of December, 1998 will be allowed. If plans are not presented or if the site is not completed as scheduled (in general accordance with the plans presented at the February, 1997 hearing) then the property shall be deeded

*changed from "MAY"*

# 2018-2-5 Correspondence to Kuna City Council

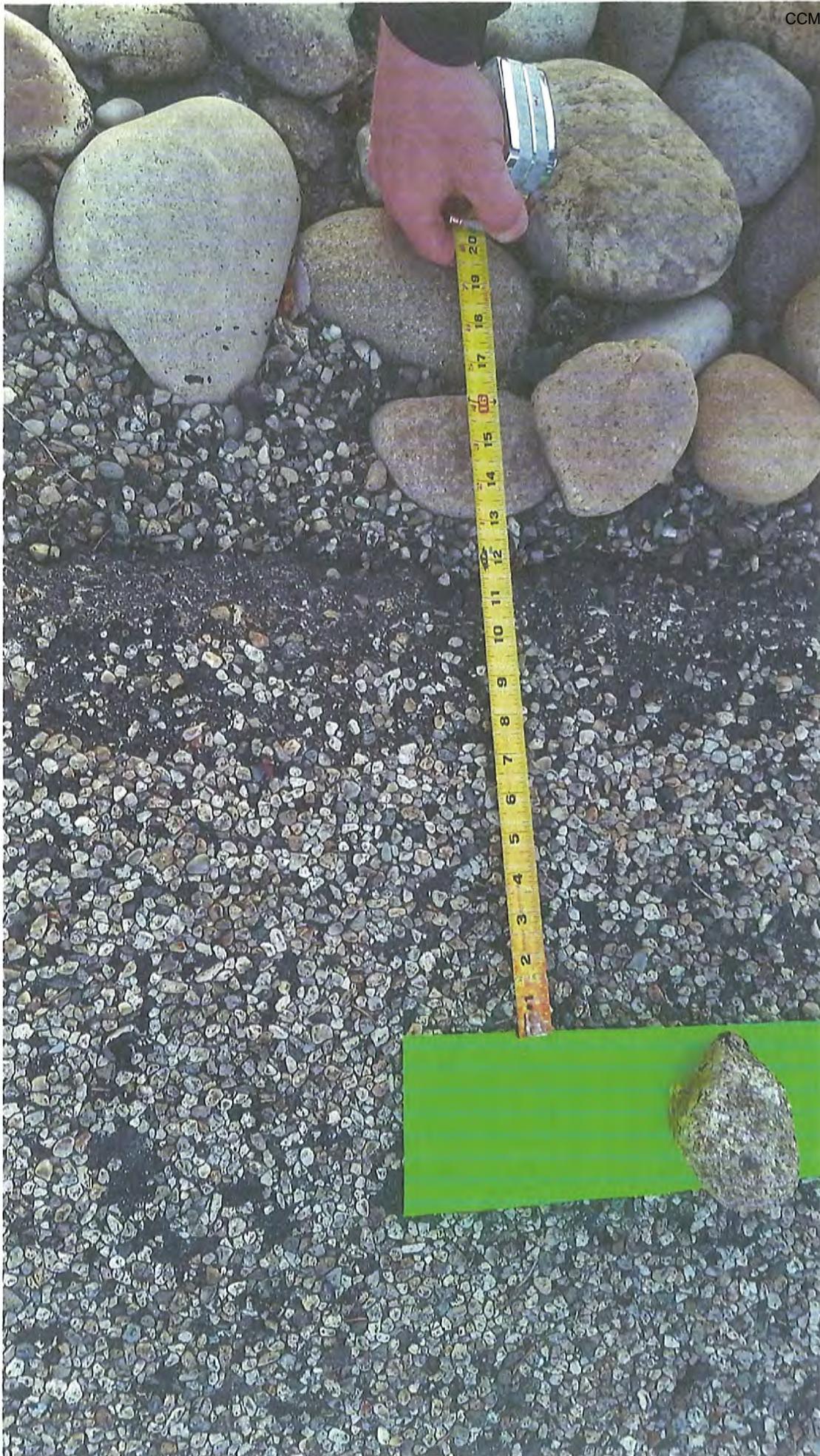
Final Audit Report

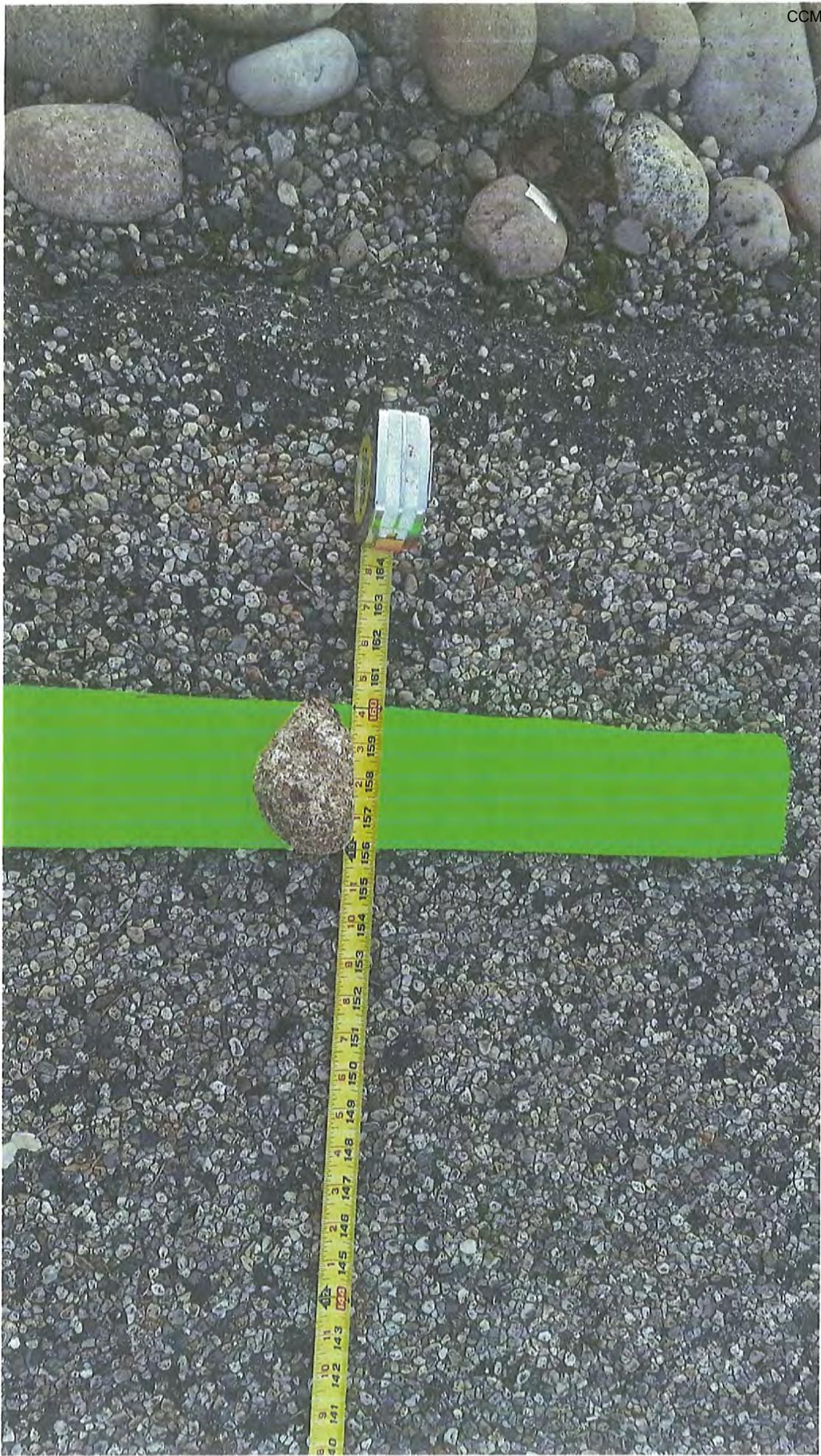
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Created:	2019-02-15
By:	Jaren Wieland (jaren.wieland@mwsrose.com)
Status:	Signed
Transaction ID:	CBJCHBCAABAAkdsKAUKSL4DdWFrzu6lynCuTiDZapsPB

## "2018-2-5 Correspondence to Kuna City Council" History

-  Document created by Jaren Wieland (jaren.wieland@mwsrose.com)  
2019-02-15 - 10:35:08 PM GMT- IP address: 209.210.179.148
-  Document emailed to Virginia Rennae Jeppesen (virginia.jeppesen@yahoo.com) for signature  
2019-02-15 - 10:35:39 PM GMT
-  Document viewed by Virginia Rennae Jeppesen (virginia.jeppesen@yahoo.com)  
2019-02-15 - 10:44:58 PM GMT- IP address: 69.147.90.254
-  Document e-signed by Virginia Rennae Jeppesen (virginia.jeppesen@yahoo.com)  
Signature Date: 2019-02-15 - 10:45:43 PM GMT - Time Source: server- IP address: 174.208.7.21
-  Signed document emailed to Virginia Rennae Jeppesen (virginia.jeppesen@yahoo.com) and Jaren Wieland (jaren.wieland@mwsrose.com)  
2019-02-15 - 10:45:43 PM GMT







CITY OF KUNA

751 W. 4th Street • Kuna, Idaho • 83634 • Phone (208) 922-5274  
Fax: (208) 922-5989 • www.Kunacity.Id.gov

SIGN-UP SHEET

February 19, 2019 – City Council, Public Hearing

Case Name: Redcloud Subdivision – Annex & Preliminary Plat; SSM2, Company

Case Type: Applicant requests to annex approximately 8.00 acres into Kuna City with an R-6, residential zone, and to subdivide the property into 41 single family residential lots and three (3) common lots and have reserved the name Redcloud Subdivision. A Design Review application for four common areas and buffer landscaping accompanies this application. The site is located at the southeast corner of Luker and Great Bear Roads, The site address is 1250 S. Luker Rd., Kuna, Idaho; In Section 25, T 2N, R 1W, APN #: S1325438500.

Case No.: 18-05-AN (Annex) and 18-06-S (Preliminary Plat).

Please print your name below if you would like to present oral testimony or written exhibits about this item to the Commission/Council.

IN FAVOR	NEUTRAL	IN OPPOSITION
<input checked="" type="checkbox"/> <u>Testify</u> <input type="checkbox"/> <u>Not Testify</u> <u>Tom MATE</u> Print Name <u>885 W. Rush</u> Print Address <u>Edge Id 83634</u> City                      State, Zip	<input checked="" type="checkbox"/> <u>Testify</u> <input type="checkbox"/> <u>Not Testify</u> <u>MIKE LOSH</u> Print Name <u>1037 SO THREAVE</u> Print Address <u>KUNA Id 83634</u> City                      State, Zip	<input type="checkbox"/> <u>Testify</u> <input type="checkbox"/> <u>Not Testify</u> <del><u>Kayleen Jones</u></del> Print Name <del><u>1747 N SPIKE AVE</u></del> Print Address <del><u>Kuna</u></del> City                      State, Zip
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MINUTES OF NEIGHBORHOOD MEETING HELD AT 1250 S. LUKER ROAD, KUNA, IDAHO ON  
JULY 26, 2018 7:00 O'CLOCK P.M.

THOMAS MAILE COMMENCED THE MEETING IN A TIMELY MANNER, AND OVERSAW THE SIGN-IN SHEET FOR THE PARTICIPANTS

THEREAFTER, THOMAS MAILE ENGAGED IN PRESENTATION, DESCRIBING THE NATURE OF THE DEVELOPMENT AND SHARED A SITE PLAN WITH THE AUDIENCE (SAME SITE PLAN INITIALLY PROVIDED TO THE CITY OF KUNA.

A TOTAL OF 7 NEIGHBORS (COUPLES AND INDIVIDUALS) HEARD THE PRESENTATION AND PARTICIPATED IN DISCUSSIONS CONCERNING THE PROPOSED DEVELOPMENT.

THE VAST MAJORITY OF THE NEIGHBORS PROVIDED POSITIVE COMMENTS AND INPUT SURROUNDING THE DEVELOPMENT GOALS. DEVELOPER EXPLAINED THE BASIC DESIGN AND SQUARE FOOTAGE OF THE PROPOSED 40 HOME SITE IMPROVEMENTS.

AT 7:35 P.M. MEETING WAS ADJOURNED.

Thomas Maile  
Secretary-treasurer

*Red Cloud*



City of Kuna

## Payment Approval Report - City Council Approval

Page: 2

Report dates: 2/15/2019-2/28/2019

Feb 28, 2019 09:46AM

Vendor #	Vendor Name	Invoice Number	PO #	Description	Invoice Date	Net Invoice Amount	Amount Paid	GL Account and Title	GL Activity #	GL Period	Date Paid	Voided
				CREEK PARK	02/11/2019	160.00	.00	01-6212_RENT- EQUIPMENT	1004	2/19		
	Total 114-8019318:					160.00	.00					
1463	A COMPANY, INC.	114-8019320		ADA WHEELCHAIR ACCESSIBLE RESTROOM RENTAL #ADA412, WEEKLY SERVICE, 1/7-2/3/19 - NICHOLSON PARK	02/11/2019	202.36	.00	01-6212_RENT- EQUIPMENT	1004	2/19		
	Total 114-8019320:					202.36	.00					
1463	A COMPANY, INC.	114-8019321		STANDARD RESTROOM RENTAL #ADA397, BI-WEEKLY SERVICE, 1/7-2/3/19 - WINCHESTER PARK	02/11/2019	160.00	.00	01-6212_RENT- EQUIPMENT	1004	2/19		
	Total 114-8019321:					160.00	.00					
	Total A COMPANY, INC.:					1,366.73	.00					
<b>A.M.E. ELECTRIC, INC.</b>												
1210	A.M.E. ELECTRIC, INC.	190110		HOURLY METERS, CEDAR WELL, FEB.'19	02/11/2019	434.06	.00	25-6160 MISCELLANEOUS EXPENSES	0	2/19		
	Total 190110:					434.06	.00					
1210	A.M.E. ELECTRIC, INC.	190111		OVERLOAD RELAYS, HUBBARD LIFT STATION, T.FLEMING, FEB.'19	02/20/2019	674.15	.00	21-6150_M & R - SYSTEM	0	2/19		
	Total 190111:					674.15	.00					
1210	A.M.E. ELECTRIC, INC.	190112		HOURLY METERS, GFI RECEPTACLE, AND CIRCUIT BREAKER, SEGO PRAIRIE, FEB.'19	02/11/2019	834.08	.00	25-6160 MISCELLANEOUS EXPENSES	0	2/19		

City of Kuna

Payment Approval Report - City Council Approval

Page: 3

Report dates: 2/15/2019-2/28/2019

Feb 28, 2019 09:46AM

Vendor #	Vendor Name	Invoice Number	PO #	Description	Invoice Date	Net Invoice Amount	Amount Paid	GL Account and Title	GL Activity #	GL Period	Date Paid	Voided
Total 190112:						834.08	.00					
Total A.M.E. ELECTRIC, INC.:						1,942.29	.00					
<b>ADA COUNTY HIGHWAY DISTRICT (RENT)</b>												
1037	ADA COUNTY HIGHWAY DISTRICT (RENT)	15354		<u>SHOP RENT FOR MARCH 2019 - PARKS</u>	02/14/2019	148.50	.00	01-6211 RENT-BUILDINGS & LAND	1004	2/19		
1037	ADA COUNTY HIGHWAY DISTRICT (RENT)	15354		<u>SHOP RENT FOR MARCH 2019 - WATER</u>	02/14/2019	126.00	.00	20-6211 RENT-BUILDINGS & LAND	0	2/19		
1037	ADA COUNTY HIGHWAY DISTRICT (RENT)	15354		<u>SHOP RENT FOR MARCH 2019 - SEWER</u>	02/14/2019	121.50	.00	21-6211 RENT - BUILDINGS & LAND	0	2/19		
1037	ADA COUNTY HIGHWAY DISTRICT (RENT)	15354		<u>SHOP RENT FOR MARCH 2019 - P.I</u>	02/14/2019	54.00	.00	25-6211 RENT - BUILDINGS & LAND	0	2/19		
Total 15354:						450.00	.00					
Total ADA COUNTY HIGHWAY DISTRICT (RENT):						450.00	.00					
<b>ADA COUNTY PROSECUTING ATTORNE</b>												
176	ADA COUNTY PROSECUTING ATTORNE	02202019ACP		<u>PROSECUTORIAL SERVICES FOR FEBRUARY 2019</u>	02/20/2019	4,630.83	.00	01-6203 PROSECUTORIAL SERVICES	0	2/19		
Total 02202019ACPA:						4,630.83	.00					
Total ADA COUNTY PROSECUTING ATTORNE:						4,630.83	.00					
<b>AGNEW BECK CONSULTING, INC.</b>												
1883	AGNEW BECK CONSULTING, INC.	7751		<u>KUNA COMP PLAN, 12/1/18-1/31/19 - P &amp; Z</u>	02/13/2019	5,052.22	.00	01-6202 PROFESSIONAL SERVICES	1003	2/19		
Total 7751:						5,052.22	.00					
Total AGNEW BECK CONSULTING, INC.:						5,052.22	.00					



City of Kuna

## Payment Approval Report - City Council Approval

Page: 5

Report dates: 2/15/2019-2/28/2019

Feb 28, 2019 09:46AM

Vendor #	Vendor Name	Invoice Number	PO #	Description	Invoice Date	Net Invoice Amount	Amount Paid	GL Account and Title	GL Activity #	GL Period	Date Paid	Voided
				HIGHLIGHTERS, 1 BOX LAMINATING POUCHES, CITY HALL, FEB.'19	02/13/2019	94.68	.00	01-6165 OFFICE SUPPLIES	0	2/19		
1795	BUYWYZ LLC	133641	8068	2 CASES COPY PAPER, 1 PK CORRECTION TAPE, 1 DOZ BLUE GEL PENS, FEB.'19 - P & Z	02/13/2019	100.28	.00	01-6165 OFFICE SUPPLIES	1003	2/19		
1795	BUYWYZ LLC	133641	8068	2 PKS INDEX TABS, 1 BOX FILE FOLDERS, D.CROSSLEY, FEB.'19 - WATER	02/13/2019	11.63	.00	20-6165 OFFICE SUPPLIES	0	2/19		
1795	BUYWYZ LLC	133641	8068	2 PKS INDEX TABS, 1 BOX FILE FOLDERS, D.CROSSLEY, FEB.'19 - SEWER	02/13/2019	11.63	.00	21-6165 OFFICE SUPPLIES	0	2/19		
1795	BUYWYZ LLC	133641	8068	2 PKS INDEX TABS, 1 BOX FILE FOLDERS, D.CROSSLEY, FEB.'19 - P.I	02/13/2019	4.44	.00	25-6165 OFFICE SUPPLIES	0	2/19		
Total 133641:						222.66	.00					
1795	BUYWYZ LLC	133955	8089	2 PKS SUCTION CUPS, 1 BOTTLE HAND SANITIZER, 1 PK LYSOL WIPES, FOR CITY HALL, FEB.'19	02/20/2019	53.68	.00	01-6165 OFFICE SUPPLIES	0	2/19		
1795	BUYWYZ LLC	133955	8089	1 CASE COPY PAPER FOR THE TREATMENT PLANT, FEB.'19 - WATER	02/20/2019	15.96	.00	20-6165 OFFICE SUPPLIES	0	2/19		
1795	BUYWYZ LLC	133955	8089	1 CASE COPY PAPER FOR THE TREATMENT PLANT, FEB.'19 - SEWER	02/20/2019	15.96	.00	21-6165 OFFICE SUPPLIES	0	2/19		
1795	BUYWYZ LLC	133955	8089	1 CASE COPY PAPER FOR THE TREATMENT PLANT, FEB.'19 - P.I	02/20/2019	6.07	.00	25-6165 OFFICE SUPPLIES	0	2/19		
1795	BUYWYZ LLC	133955	8089	1 DOZ BLUE GEL PENS, J.COULTER, FEB.'19	02/20/2019	19.92	.00	01-6165 OFFICE SUPPLIES	1005	2/19		
Total 133955:						111.59	.00					



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				02/06-03/07/19 - P.I	02/22/2019	4.84	4.84	25-6255 TELEPHONE EXPENSE	0	2/19	02/22/2019	
Total 20892291790207030619:						48.48	48.48					
Total CENTURYLINK:						48.48	48.48					
<b>CORE &amp; MAIN LP</b>												
63	CORE & MAIN LP	K017661	8047	2 VALVE BOX TOOLS FOR WELLS, J. OSBORN, FEB. '19 - WATER	02/08/2019	412.96	.00	20-6175 SMALL TOOLS	0	2/19		
63	CORE & MAIN LP	K017661	8047	2 VALVE BOX TOOLS FOR WELLS, J. OSBORN, FEB. '19 - P.I.	02/08/2019	103.24	.00	25-6175 SMALL TOOLS	0	2/19		
Total K017661:						516.20	.00					
63	CORE & MAIN LP	K125371	8077	2 COURT STOP INJECTORS, BUTLER WELL, D. CROSSLEY, FEB. '19	02/20/2019	714.00	.00	20-6150 M & R - SYSTEM	0	2/19		
Total K125371:						714.00	.00					
63	CORE & MAIN LP	K145892	8077	86 METERS, D. CROSSLEY, FEB. '19	02/20/2019	23,644.84	.00	20-6020 CAPITAL IMPROVEMENTS	1089	2/19		
Total K145892:						23,644.84	.00					
Total CORE & MAIN LP:						24,875.04	.00					
<b>ED STAUB &amp; SONS PETROLEUM, INC</b>												
1731	ED STAUB & SONS PETROLEUM, INC	103787		REFILL PROPANE AT SHOP, FEB.'19 - ADMIN	02/11/2019	152.26	.00	01-6150 MAINTENANCE & REPAIRS - SYSTEM	0	2/19		
1731	ED STAUB & SONS PETROLEUM, INC	103787		REFILL PROPANE AT SHOP, FEB.'19 - WATER	02/11/2019	60.90	.00	20-6150 M & R - SYSTEM	0	2/19		

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1731	ED STAUB & SONS PETROLEUM, INC	103787		<u>REFILL PROPANE AT SHOP, FEB.'19 - SEWER</u>	02/11/2019	60.90	.00	<u>21-6150 M &amp; R - SYSTEM</u>	0	2/19		
1731	ED STAUB & SONS PETROLEUM, INC	103787		<u>REFILL PROPANE AT SHOP, FEB.'19 - P.I</u>	02/11/2019	30.45	.00	<u>25-6150 MAINT. &amp; REPAIRS - SYSTEM (PI)</u>	0	2/19		
Total 103787:						304.51	.00					
Total ED STAUB & SONS PETROLEUM, INC:						304.51	.00					
<b>FERGUSON ENTERPRISES INC</b>												
219	FERGUSON ENTERPRISES INC	0703724		<u>LAGOON VALVE REPAIR, LABOR AND ITEMS RENTAL, T. FLEMING, FEB. '19</u>	02/13/2019	1,949.34	.00	<u>21-6150 M &amp; R - SYSTEM</u>	0	2/19		
Total 0703724:						1,949.34	.00					
219	FERGUSON ENTERPRISES INC	0705097	8078	<u>WEST WELL REFAB AT FARM, PARTS &amp; SUPPLIES, T. FLEMING, FEB. '19</u>	02/14/2019	16,646.39	.00	<u>21-6020 CAPITAL IMPROVEMENTS</u>	1157	2/19		
Total 0705097:						16,646.39	.00					
219	FERGUSON ENTERPRISES INC	0705097-1	8078	<u>WEST WELL REFAB AT FARM, PARTS &amp; SUPPLIES, T. FLEMING, FEB. '19</u>	02/15/2019	277.78	.00	<u>21-6020 CAPITAL IMPROVEMENTS</u>	1157	2/19		
Total 0705097-1:						277.78	.00					
219	FERGUSON ENTERPRISES INC	0705920	7931	<u>2019 FERGUSON CEU ROADSHOW FOR T. FLEMING IN SEWER, D. CROSSLEY, JAN. '19 - SEWER</u>	02/11/2019	60.00	.00	<u>21-6265 TRAINING &amp; SCHOOLING EXPENSE</u>	0	2/19		
219	FERGUSON ENTERPRISES INC	0705920	7931	<u>2019 FERGUSON CEU ROADSHOW FOR D. CROSSLEY, M. DAVILA, J. COX, AND C. DEYOUNG FOR WATER, D. CROSSLEY, JAN. '19 - WATER</u>	02/11/2019	192.00	.00	<u>20-6265 TRAINING &amp; SCHOOLING EXPENSE</u>	0	2/19		

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219	FERGUSON ENTERPRISES INC	0705920	7931	2019 FERGUSON CEU ROADSHOW FOR D. CROSSLEY, M. DAVILA, J. COX, AND C. DEYOUNG FOR WATER, D. CROSSLEY, JAN. '19 - P.I.	02/11/2019	48.00	.00	25-6265 TRAINING & SCHOOLING EXPENSE	0	2/19		
Total 0705920:						300.00	.00					
219	FERGUSON ENTERPRISES INC	0706217	8080	FITTINGS FOR WEST WELL REFAB AT THE FARM, T. FLEMING, FEB. '19	02/19/2019	1,948.61	.00	21-6020 CAPITAL IMPROVEMENTS	1157	2/19		
Total 0706217:						1,948.61	.00					
219	FERGUSON ENTERPRISES INC	0706407		PVC BLUE PIPE, STOCK, FEB. '19	02/20/2019	699.68	.00	21-6020 CAPITAL IMPROVEMENTS	1157	2/19		
Total 0706407:						699.68	.00					
Total FERGUSON ENTERPRISES INC:						21,821.80	.00					
<b>GRANITE EXCAVATION INC</b>												
1907	GRANITE EXCAVATION INC	217070-000-10		PROFESSIONAL SERVICES 1/3- 25/19 - ORCHARD REGIONAL LIFT STATION	02/04/2019	216,567.49	.00	21-6020 CAPITAL IMPROVEMENTS	1104	2/19		
Total 217070-000-10312519:						216,567.49	.00					
Total GRANITE EXCAVATION INC:						216,567.49	.00					
<b>IDAHO POWER CO</b>												
38	IDAHO POWER CO	02252019I		ELECTRIC SERVICE FOR FEBRUARY 2019 - ADMIN	02/25/2019	457.37	.00	01-6290 UTILITIES	0	2/19		
38	IDAHO POWER CO	02252019I		ELECTRIC SERVICE FOR FEBRUARY 2019 - SENIOR CENTER	02/25/2019	277.20	.00	01-6290 UTILITIES	1001	2/19		
38	IDAHO POWER CO	02252019I		ELECTRIC SERVICE FOR FEBRUARY 2019 - STREET LIGHTS	02/25/2019	6,002.43	.00	01-6290 UTILITIES	1002	2/19		



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				A.WELKER, FEB.'19	02/10/2019	96.00	.00	01-6142 MAINT. & REPAIR - EQUIPMENT	0	2/19		
Total 112512:						96.00	.00					
1595	INTEGRINET SOLUTIONS, INC.	112679		PRO-ACTION SERVICE AND MAINTENANCE FOR SERVER AND NETWORK MAINTENANCE AND ADMINISTRATION AS WELL AS ONSITE SERVICES FOR MARCH 2019 - ADMIN	02/15/2019	305.04	.00	01-6142 MAINT. & REPAIR - EQUIPMENT	0	2/19		
1595	INTEGRINET SOLUTIONS, INC.	112679		PRO-ACTION SERVICE AND MAINTENANCE FOR SERVER AND NETWORK MAINTENANCE AND ADMINISTRATION AS WELL AS ONSITE SERVICE FOR MARCH 2019 - P & Z	02/15/2019	108.94	.00	01-6142 MAINT. & REPAIR - EQUIPMENT	1003	2/19		
1595	INTEGRINET SOLUTIONS, INC.	112679		PRO-ACTION SERVICE AND MAINTENANCE FOR SERVER AND NETWORK MAINTENANCE AND ADMINISTRATION AS WELL AS ONSITE SERVICE FOR MARCH 2019 - WATER	02/15/2019	283.25	.00	20-6142 MAINT. & REPAIRS- EQUIPMENT	0	2/19		
1595	INTEGRINET SOLUTIONS, INC.	112679		PRO-ACTION SERVICE AND MAINTENANCE FOR SERVER AND NETWORK MAINTENANCE AND ADMINISTRATION AS WELL AS ONSITE SERVICE FOR MARCH 2019 - SEWER	02/15/2019	283.25	.00	21-6142 MAINT. & REPAIRS - EQUIPMENT	0	2/19		
1595	INTEGRINET SOLUTIONS, INC.	112679		PRO-ACTION SERVICE AND MAINTENANCE FOR SERVER AND NETWORK MAINTENANCE AND ADMINISTRATION AS WELL AS ONSITE SERVICE FOR MARCH 2019 - P.I	02/15/2019	108.94	.00	25-6142 MAINT. & REPAIRS - EQUIPMENT	0	2/19		
Total 112679:						1,089.42	.00					
Total INTEGRINET SOLUTIONS, INC.:						1,185.42	.00					

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<b>INTEGRITY INSPECTION SOLUTIONS INC</b>												
1981	INTEGRITY INSPECTION SOLUTIONS INC	FY19M2-036	8035	4 BOTTLES OF DYE TO TEST PUMPS AT NEW ORCHARD LIFT STATION, R. DAVILA, JAN. '19	02/05/2019	160.00	.00	21-6150 M & R - SYSTEM	0	2/19		
Total FY19M2-036:						160.00	.00					
Total INTEGRITY INSPECTION SOLUTIONS INC:						160.00	.00					
<b>INTERMOUNTAIN GAS CO</b>												
37	INTERMOUNTAIN GAS CO	482135196011		NATURAL GAS CONSUMPTION AT THE SENIOR CENTER, 01/12-02/08/19	02/22/2019	397.02	397.02	01-6290 UTILITIES	1001	2/19	02/22/2019	
Total 4821351960112020819:						397.02	397.02					
37	INTERMOUNTAIN GAS CO	482327707011		NATURAL GAS CONSUMPTION AT THE ORCHARD PARK'S OFFICE - 1/12-2/8/19	02/22/2019	36.39	36.39	01-6290 UTILITIES	1004	2/19	02/22/2019	
Total 4823277070112020819:						36.39	36.39					
37	INTERMOUNTAIN GAS CO	482634665011		NATURAL GAS CONSUMPTION AT CITY HALL, 01/12-02/08/19 - ADMIN	02/22/2019	95.93	95.93	01-6290 UTILITIES	0	2/19	02/22/2019	
37	INTERMOUNTAIN GAS CO	482634665011		NATURAL GAS CONSUMPTION AT CITY HALL, 01/12-02/08/19 - P & Z	02/22/2019	34.26	34.26	01-6290 UTILITIES	1003	2/19	02/22/2019	
37	INTERMOUNTAIN GAS CO	482634665011		NATURAL GAS CONSUMPTION AT CITY HALL, 01/12-02/08/19 - WATER	02/22/2019	89.08	89.08	20-6290 UTILITIES EXPENSE	0	2/19	02/22/2019	
37	INTERMOUNTAIN GAS CO	482634665011		NATURAL GAS CONSUMPTION AT CITY HALL, 01/12-02/08/19 - SEWER	02/22/2019	89.08	89.08	21-6290 UTILITIES EXPENSE	0	2/19	02/22/2019	
37	INTERMOUNTAIN GAS CO	482634665011		NATURAL GAS CONSUMPTION AT CITY HALL, 01/12-02/08/19 - P.I	02/22/2019	34.26	34.26	25-6290 UTILITIES EXPENSE	0	2/19	02/22/2019	

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Total 4826346650112020819:						342.61	342.61					
Total INTERMOUNTAIN GAS CO:						776.02	776.02					
<b>J &amp; M SANITATION, INC.</b>												
230	J & M SANITATION, INC.	02082019-021		<u>SANITATION RECEIPT TRANSFER, 02/08/2019- 02/14/2019</u>	02/15/2019	61,289.21	61,289.21	26-7000 SOLID WASTE SERVICE FEES	0	2/19	02/15/2019	
230	J & M SANITATION, INC.	02082019-021		<u>SANITATION RECEIPT TRANSFER LESS FRANCHISE FEES, 02/08/2019-02/14/2019</u>	02/15/2019	-6,055.37	-6,055.37	01-4170 FRANCHISE FEES	0	2/19	02/15/2019	
Total 02082019-02142019:						55,233.84	55,233.84					
230	J & M SANITATION, INC.	02152019-022		<u>SANITATION RECEIPT TRANSFER, 02/15/2019- 02/21/2019</u>	02/22/2019	47,075.13	47,075.13	26-7000 SOLID WASTE SERVICE FEES	0	2/19	02/22/2019	
230	J & M SANITATION, INC.	02152019-022		<u>SANITATION RECEIPT TRANSFER LESS FRANCHISE FEES, 02/15/2019-02/21/2019</u>	02/22/2019	-4,651.02	-4,651.02	01-4170 FRANCHISE FEES	0	2/19	02/22/2019	
Total 02152019-02212019:						42,424.11	42,424.11					
Total J & M SANITATION, INC.:						97,657.95	97,657.95					
<b>JACK HENRY &amp; ASSOCIATES, INC.</b>												
1328	JACK HENRY & ASSOCIATES, INC.	3052527		<u>BANK FEES, FEB.19 - ADMIN</u>	02/25/2019	33.84	.00	01-6505 BANK FEES	0	2/19		
1328	JACK HENRY & ASSOCIATES, INC.	3052527		<u>BANK FEES, FEB.'19 - P &amp; Z</u>	02/25/2019	12.08	.00	01-6505 BANK FEES	1003	2/19		
1328	JACK HENRY & ASSOCIATES, INC.	3052527		<u>BANK FEES, FEB.'19 - WATER</u>	02/25/2019	31.42	.00	20-6505 BANK FEES	0	2/19		
1328	JACK HENRY & ASSOCIATES, INC.	3052527		<u>BANK FEES, FEB.'19 - SEWER</u>	02/25/2019	31.42	.00	21-6505 BANK FEES	0	2/19		
1328	JACK HENRY & ASSOCIATES, INC.	3052527		<u>BANK FEES, FEB.'19 - P.I</u>	02/25/2019	12.08	.00	25-6505 BANK FEES	0	2/19		

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Total 3052527:						120.84	.00					
Total JACK HENRY & ASSOCIATES, INC.:						120.84	.00					
<b>KELLER ASSOCIATES, INC.</b>												
429	KELLER ASSOCIATES, INC.	214112-007-00		<u>PROFESSIONAL SERVICES 1/1-31/19 - KUNA LAGOONS 2 &amp; 5 SEEPAGE TEST</u>	02/11/2019	1,380.00	.00	<u>21-6020 CAPITAL IMPROVEMENTS</u>	1183	2/19		
Total 214112-007-0000001:						1,380.00	.00					
429	KELLER ASSOCIATES, INC.	217070-000-00		<u>PROFESSIONAL SERVICES 1/1-31/19 - KUNA ORCHARD REGIONAL LIFT STATION PROJECT</u>	02/06/2019	7,167.50	.00	<u>21-6020 CAPITAL IMPROVEMENTS</u>	1104	2/19		
Total 217070-000-0000019:						7,167.50	.00					
429	KELLER ASSOCIATES, INC.	217086-029-00		<u>PROFESSIONAL SERVICES 1/1-31/19 - KUNA 2018 USER RATE/CONNECTION FEE STUDY - WATER</u>	02/06/2019	3,135.00	.00	<u>20-6045 CONTINGENCY</u>	1060	2/19		
429	KELLER ASSOCIATES, INC.	217086-029-00		<u>PROFESSIONAL SERVICES 1/1-31/19 - KUNA 2018 USER RATE/CONNECTION FEE STUDY - SEWER</u>	02/06/2019	3,135.00	.00	<u>21-6045 CONTINGENCY</u>	1059	2/19		
Total 217086-029-0000006:						6,270.00	.00					
Total KELLER ASSOCIATES, INC.:						14,817.50	.00					
<b>KENDALL FORD OF MERIDIAN LLC</b>												
1616	KENDALL FORD OF MERIDIAN LLC	R1CS100980		<u>WATER PUMP FOR SEWER TRUCK #23. FEB.'19</u>	02/07/2019	1,326.11	.00	<u>21-6305 VEHICLE MAINTENANCE &amp; REPAIRS</u>	0	2/19		
Total R1CS100980:						1,326.11	.00					

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Total KENDALL FORD OF MERIDIAN LLC:						1,326.11	.00					
<b>KUNA CHAMBER OF COMMERCE</b>												
314	KUNA CHAMBER OF COMMERCE	02202019		<u>CHAMBER DUES PASS- THROUGH/MARTIN CONSTRUCTION LLC, FEB. '19</u>	02/20/2019	60.00	60.00	01-2075 <u>UNEARNED REVENUE</u>	4000	2/19	02/22/2019	
Total 02202019:						60.00	60.00					
Total KUNA CHAMBER OF COMMERCE:						60.00	60.00					
<b>KUNA JT. SCHOOL DISTRICT NO. 3</b>												
199	KUNA JT. SCHOOL DISTRICT NO. 3	733		<u>FIBER OPTIC LEASE FOR JANUARY &amp; FEBRUARY 2019 - ADMIN</u>	02/27/2019	168.00	.00	01-6255 <u>TELEPHONE</u>	0	2/19		
199	KUNA JT. SCHOOL DISTRICT NO. 3	733		<u>FIBER OPTIC LEASE FOR JANUARY &amp; FEBRUARY 2019 - P &amp; Z</u>	02/27/2019	60.00	.00	01-6255 <u>TELEPHONE</u>	1003	2/19		
199	KUNA JT. SCHOOL DISTRICT NO. 3	733		<u>FIBER OPTIC LEASE FOR JANUARY &amp; FEBRUARY 2019 - WATER</u>	02/27/2019	156.00	.00	20-6255 <u>TELEPHONE EXPENSE</u>	0	2/19		
199	KUNA JT. SCHOOL DISTRICT NO. 3	733		<u>FIBER OPTIC LEASE FOR JANUARY &amp; FEBRUARY 2019 - SEWER</u>	02/27/2019	156.00	.00	21-6255 <u>TELEPHONE EXPENSE</u>	0	2/19		
199	KUNA JT. SCHOOL DISTRICT NO. 3	733		<u>FIBER OPTIC LEASE FOR JANUARY &amp; FEBRUARY 2019 - P.I</u>	02/27/2019	60.00	.00	25-6255 <u>TELEPHONE EXPENSE</u>	0	2/19		
Total 733:						600.00	.00					
Total KUNA JT. SCHOOL DISTRICT NO. 3:						600.00	.00					
<b>KUNA LUMBER</b>												
499	KUNA LUMBER	A105036	8066	<u>THERMOMETER FOR LAGOON OFFICE. C. MCDANIEL, FEB. '19</u>	02/13/2019	2.24	.00	21-6140 <u>MAINT &amp; REPAIR BUILDING</u>	0	2/19		
Total A105036:						2.24	.00					

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499	KUNA LUMBER	A105120		<u>RETURNED HEATER FROM INVOICE# B122924, FEB. '19</u>	02/19/2019	-40.49	.00	<u>21-6140 MAINT. &amp; REPAIR BUILDING</u>	0	2/19		
499	KUNA LUMBER	A105120		<u>HEATER FOR LAGOON OFFICE, FEB. '19</u>	02/19/2019	25.49	.00	<u>21-6140 MAINT. &amp; REPAIR BUILDING</u>	0	2/19		
Total A105120:						-15.00	.00					
499	KUNA LUMBER	B122679	8030	<u>TAPE FOR STOCKING TRUCK, AND PAINT TRAY FOR WELLS, J. OSBORN, FEB. '19</u>	02/04/2019	7.85	.00	<u>20-6140 MAINT. &amp; REPAIR BUILDING</u>	0	2/19		
Total B122679:						7.85	.00					
499	KUNA LUMBER	B122686	8032	<u>DUST MASKS FOR PAINTING WELLS, J. OSBORN, FEB. '19</u>	02/04/2019	1.35	.00	<u>20-6140 MAINT. &amp; REPAIR BUILDING</u>	0	2/19		
Total B122686:						1.35	.00					
499	KUNA LUMBER	B122924	8066	<u>HEATER FOR THE LAGOON OFFICE, C. MCDANIEL, FEB. '19</u>	02/13/2019	40.49	.00	<u>21-6140 MAINT. &amp; REPAIR BUILDING</u>	0	2/19		
Total B122924:						40.49	.00					
499	KUNA LUMBER	B122925	8067	<u>PAINT FOR GREENBELT BATHROOMS AND GRAFFITI, S. JONES, FEB. '19</u>	02/13/2019	138.42	.00	<u>01-6140 MAINT. &amp; REPAIR BUILDING</u>	1004	2/19		
Total B122925:						138.42	.00					
499	KUNA LUMBER	B123109	8087	<u>7/8 INCH WASHERS FOR LAGOON SEWER WELL, J. WEBB, FEB '19</u>	02/20/2019	23.59	.00	<u>21-6020 CAPITAL IMPROVEMENTS</u>	1157	2/19		
Total B123109:						23.59	.00					
Total KUNA LUMBER:						198.94	.00					

Vendor #	Vendor Name	Invoice Number	PO #	Description	Invoice Date	Net Invoice Amount	Amount Paid	GL Account and Title	GL Activity #	GL Period	Date Paid	Voided
<b>LOCAHAN LLC</b>												
1619	LOCAHAN LLC	AR713581		MONTHLY COPY CARE INCLUDING PARTS, LABOR, AND TONER FOR COPIERS AT CITY HALL, MODEL #MPC4504EX, SERIAL # C737M540155 & C737M540938, 2/3-3/2/19 - ADMIN	02/07/2019	102.03	.00	01-6142 MAINT. & REPAIR - EQUIPMENT	0	2/19		
1619	LOCAHAN LLC	AR713581		MONTHLY COPY CARE INCLUDING PARTS, LABOR, AND TONER FOR COPIERS AT CITY HALL, MODEL #MPC4504EX, SERIAL #C737M540155 & C737M540938, 2/3-3/2/19 - P & Z	02/07/2019	36.44	.00	01-6142 MAINT. & REPAIR - EQUIPMENT	1003	2/19		
1619	LOCAHAN LLC	AR713581		MONTHLY COPY CARE INCLUDING PARTS, LABOR, AND TONER FOR COPIERS AT CITY HALL, MODEL #MPC4504EX, SERIAL # C737M540155 & C737M540938, 2/3-3/2/19 - WATER	02/07/2019	94.74	.00	20-6142 MAINT. & REPAIRS - EQUIPMENT	0	2/19		
1619	LOCAHAN LLC	AR713581		MONTHLY COPY CARE INCLUDING PARTS, LABOR, AND TONER FOR COPIERS AT CITY HALL, MODEL #MPC4504EX, SERIAL #C737M540155 & C737M540938, 2/3-3/2/19 - SEWER	02/07/2019	94.74	.00	21-6142 MAINT. & REPAIRS - EQUIPMENT	0	2/19		
1619	LOCAHAN LLC	AR713581		MONTHLY COPY CARE INCLUDING PARTS, LABOR, AND TONER FOR COPIERS AT CITY HALL, MODEL # MPC4504EX, SERIAL #C737M540155 & C737M540938, 2/3-3/2/19 - P.]	02/07/2019	36.45	.00	25-6142 MAINT. & REPAIRS - EQUIPMENT	0	2/19		
Total AR713581:						364.40	.00					
Total LOCAHAN LLC:						364.40	.00					
<b>METROQUIP, INC.</b>												
196	METROQUIP, INC.	101028	8091	PUSH CAMERA, FOR SEWER LINES, T. FLEMING, FEB. '19	02/21/2019	9,808.00	.00	21-6045 CONTINGENCY	1185	2/19		

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Total 101028:						9,808.00	.00					
Total METROQUIP, INC.:						9,808.00	.00					
<b>MISCELLANEOUS VENDORS 2</b>												
1849	MISCELLANEOUS VENDORS 2	01152019DA		<u>REIMBURSEMENT FOR CDL TESTS, D.ABBOTT, JAN.'19 - PARKS</u>	01/15/2019	119.74	.00	<u>01-6265 TRAINING &amp; SCHOOLING</u>	1004	2/19		
Total 01152019DA:						119.74	.00					
1849	MISCELLANEOUS VENDORS 2	02222019SDN		<u>RELEASE OF CASH BOND FOR ASHTON ESTATES, FEB. '19</u>	02/22/2019	54,372.00	54,372.00	<u>30-2075 UNEARNED REVENUE</u>	0	2/19	02/22/2019	
Total 02222019SDN:						54,372.00	54,372.00					
1849	MISCELLANEOUS VENDORS 2	2119SW-01B		<u>EARNEST MONEY 501 W SHORTLINE ST, FEB. '19</u>	02/06/2019	5,000.00	5,000.00	<u>50-6045 CONTINGENCY</u>	0	2/19	02/20/2019	
Total 2119SW-01B:						5,000.00	5,000.00					
1849	MISCELLANEOUS VENDORS 2	2119SW-02A		<u>EARNEST MONEY TBD SOUTH ASPEN WILLOW AVE, PARKS, FEB. '19</u>	02/06/2019	5,000.00	5,000.00	<u>50-6045 CONTINGENCY</u>	0	2/19	02/20/2019	
Total 2119SW-02A:						5,000.00	5,000.00					
Total MISCELLANEOUS VENDORS 2:						64,491.74	64,372.00					
<b>PARTS, INC.</b>												
470	PARTS, INC.	183541	8026	<u>AIR &amp; OIL FILTERS FOR PARKS GATOR, S. HOWELL, JAN. '19</u>	02/01/2019	53.16	.00	<u>01-6142 MAINT. &amp; REPAIR - EQUIPMENT</u>	1004	2/19		
Total 183541:						53.16	.00					

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470	PARTS, INC.	184149	8059	<u>ANTI FREEZE FOR FLEET SHOP SUPPLY, S. HOWELL, FEB. '19 - ADMIN</u>	02/12/2019	12.49	.00	01-6150 <u>MAINTENANCE &amp; REPAIRS - SYSTEM</u>	0	2/19		
470	PARTS, INC.	184149	8059	<u>ANTI FREEZE FOR FLEET SHOP SUPPLY, S. HOWELL, FEB. '19 - WATER</u>	02/12/2019	5.00	.00	20-6150 <u>M &amp; R - SYSTEM</u>	0	2/19		
470	PARTS, INC.	184149	8059	<u>ANTI FREEZE FOR FLEET SHOP SUPPLY, S. HOWELL, FEB. '19 - SEWER</u>	02/12/2019	5.00	.00	21-6150 <u>M &amp; R - SYSTEM</u>	0	2/19		
470	PARTS, INC.	184149	8059	<u>ANTI FREEZE FOR FLEET SHOP SUPPLY, S. HOWELL, FEB. '19 - P.I.</u>	02/12/2019	2.49	.00	25-6150 <u>MAINT. &amp; REPAIRS - SYSTEM (PI)</u>	0	2/19		
Total 184149:						24.98	.00					
470	PARTS, INC.	184295	8069	<u>TAILLIGHT LENS FOR TRUCK #35, SEWER, S. HOWELL, FEB '19</u>	02/14/2019	198.33	.00	21-6305 <u>VEHICLE MAINTENANCE &amp; REPAIRS</u>	0	2/19		
Total 184295:						198.33	.00					
470	PARTS, INC.	184312	8070	<u>VAC TRAILIER TUNE UP, B.BURR, FEB '19 - WATER</u>	02/14/2019	30.21	.00	20-6142 <u>MAINT. &amp; REPAIRS- EQUIPMENT</u>	0	2/19		
470	PARTS, INC.	184312	8070	<u>VAC TRAILIER TUNE UP, B.BURR, FEB '19 - P.I.</u>	02/14/2019	7.55	.00	25-6142 <u>MAINT. &amp; REPAIRS - EQUIPMENT</u>	0	2/19		
Total 184312:						37.76	.00					
Total PARTS, INC.:						314.23	.00					
<b>PEAK ALARM COMPANY, INC</b>												
1021	PEAK ALARM COMPANY, INC	934471		<u>ALARM MONITORING FOR WELLS (SEGO PRAIRIE, SNOWHAWK, DANSKIN, BUTLER, BEST BATH, EL CAJON, &amp; CEDAR), 3/1-3/31/19 - WATER</u>	02/15/2019	218.28	.00	20-6140 <u>MAINT. &amp; REPAIR BUILDING</u>	0	2/19		

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1021	PEAK ALARM COMPANY, INC	934471		<u>ALARM MONITORING FOR WELLS (SEGO PRAIRIE, SNOWHAWK, DANSKIN, BUTLER, BEST BATH, EL CAJON, &amp; CEDAR), 3/1-31/19 - P.I</u>	02/15/2019	54.57	.00	<u>25-6140 MAINT &amp; REPAIR BUILDING</u>	0	2/19		
Total 934471:						272.85	.00					
1021	PEAK ALARM COMPANY, INC	934516		<u>ALARM MONITORING FOR THE TREATMENT PLANT, 3/1-5/31/19</u>	02/15/2019	92.13	.00	<u>21-6140 MAINT &amp; REPAIR BUILDING</u>	0	2/19		
Total 934516:						92.13	.00					
Total PEAK ALARM COMPANY, INC:						364.98	.00					
<b>PRECISION PUMPING SYSTEMS</b>												
952	PRECISION PUMPING SYSTEMS	20905		<u>PANEL UPGRADE AND INSTALLATION, CRIMSON POINT, C.DEYOUNG, JAN.'19</u>	01/31/2019	11,693.00	.00	<u>25-6166 PP&amp;E PURCHASES - OPERATIONS</u>	1057	2/19		
Total 20905:						11,693.00	.00					
Total PRECISION PUMPING SYSTEMS:						11,693.00	.00					
<b>REXEL USA, INC.</b>												
1613	REXEL USA, INC.	U359755		<u>BATTERY AND CHARGER, B.BACHMAN, FEB.'19</u>	02/11/2019	121.55	.00	<u>20-6175 SMALL TOOLS</u>	0	2/19		
1613	REXEL USA, INC.	U359755		<u>BATTERY AND CHARGER, B.BACHMAN, FEB.'19</u>	02/11/2019	121.56	.00	<u>21-6175 SMALL TOOLS</u>	0	2/19		
1613	REXEL USA, INC.	U359755		<u>BATTERY AND CHARGER, B.BACHMAN, FEB.'19</u>	02/11/2019	46.31	.00	<u>25-6175 SMALL TOOLS</u>	0	2/19		
Total U359755:						289.42	.00					
Total REXEL USA, INC.:						289.42	.00					

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<b>RODDA PAINT CO.</b>												
1723	RODDA PAINT CO.	75053151	8027	<u>1-GALLON OF PAINT, AND 2 BOXES OF AEROSOL PAINT, FOR WELL HOUSES, M. DAVILA, JAN. '19</u>	02/04/2019	184.02	.00	<u>20-6140 MAINT. &amp; REPAIR BUILDING</u>	0	2/19		
Total 75053151:						184.02	.00					
Total RODDA PAINT CO.:						184.02	.00					
<b>TOP GEAR INC</b>												
1982	TOP GEAR INC	55283	8038	<u>KIDS BIKE HELMETS, FOR BIKE SAFETY RODEO, A. WELKER, JAN. '19</u>	02/05/2019	200.00	.00	<u>01-6155 MEETINGS/COMMI TTEES</u>	1030	2/19		
1982	TOP GEAR INC	55283	8038	<u>KIDS BIKE HELMETS, FOR BIKE SAFETY RODEO, A. WELKER, JAN. '19</u>	02/05/2019	194.20	.00	<u>01-6265 TRAINING &amp; SCH00LING</u>	1086	2/19		
Total 55283:						394.20	.00					
Total TOP GEAR INC:						394.20	.00					
<b>TREASURE VALLEY COFFEE</b>												
992	TREASURE VALLEY COFFEE	2160:05958755	8072	<u>2 EA. 5 GALLON WATER BOTTLES, MAINTENANCE SHOP, FEB. '19</u>	02/15/2019	11.40	.00	<u>01-6165 OFFICE SUPPLIES</u>	1004	2/19		
Total 2160:05958755:						11.40	.00					
992	TREASURE VALLEY COFFEE	2160:05958783	8072	<u>5 EA. 5-GALLON WATER BOTTLES, CITY HALL, FEB. '19</u>	02/15/2019	28.50	.00	<u>01-6165 OFFICE SUPPLIES</u>	0	2/19		
Total 2160:05958783:						28.50	.00					
992	TREASURE VALLEY COFFEE	2160:05985686	8095	<u>4 EA 5-GALLON WATER BOTTLES, 1 CANISTER OF SUGAR, 1 BOX OF HOT CHOCOLATE, C. OSWALD, FEB. '19 - WATER</u>	02/21/2019	15.71	.00	<u>20-6165 OFFICE SUPPLIES</u>	0	2/19		

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992	TREASURE VALLEY COFFEE	2160:05985686	8095	4 EA 5-GALLON WATER BOTTLES, 1 CANISTER OF SUGAR, 1 BOX OF HOT CHOCOLATE, C. OSWALD, FEB. '19 - SEWER	02/21/2019	15.71	.00	21-6165 OFFICE SUPPLIES	0	2/19		
992	TREASURE VALLEY COFFEE	2160:05985686	8095	4 EA 5-GALLON WATER BOTTLES, 1 CANISTER OF SUGAR, 1 BOX OF HOT CHOCOLATE, C. OSWALD, FEB. '19 - P.I.	02/21/2019	5.98	.00	25-6165 OFFICE SUPPLIES	0	2/19		
Total 2160:05985686:						37.40	.00					
Total TREASURE VALLEY COFFEE:						77.30	.00					
<b>U.S. BANK NATIONAL ASSOC (EQUIP FINANCE)</b>												
1891	U.S. BANK NATIONAL ASSOC (EQUIP FINANCE)	378520530		COPIER CONTRACT #500- 0519539-000, MODEL #MCP4504EX, SERIAL #C737M540938 & C737M540155 - ADMIN	02/15/2019	115.60	.00	01-6212 RENT- EQUIPMENT	0	2/19		
1891	U.S. BANK NATIONAL ASSOC (EQUIP FINANCE)	378520530		COPIER CONTRACT #500- 0519539-000, MODEL #MPC4504EX, SERIAL # C737M540938 & C737M540155 - P & Z	02/15/2019	41.29	.00	01-6212 RENT- EQUIPMENT	1003	2/19		
1891	U.S. BANK NATIONAL ASSOC (EQUIP FINANCE)	378520530		COPIER CONTRACT #500- 0519539-000, MODEL #MPC4504EX, SERIAL #C737M540938 & C737M540155 - WATER	02/15/2019	107.34	.00	20-6212 RENT - EQUIPMENT	0	2/19		
1891	U.S. BANK NATIONAL ASSOC (EQUIP FINANCE)	378520530		COPIER CONTRACT #500- 0519539-000, MODEL #MPC4504EX, SERIAL #C737M540938 & C737M540155 - SEWER	02/15/2019	107.34	.00	21-6212 RENT- EQUIPMENT	0	2/19		
1891	U.S. BANK NATIONAL ASSOC (EQUIP FINANCE)	378520530		COPIER CONTRACT #500- 0519539-000, MODEL #MPC4504EX, SERIAL #C737M540938 & C737M540155 - P.I.	02/15/2019	41.28	.00	25-6212 RENT - EQUIPMENT	0	2/19		

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Total 378520530:						412.85	.00					
Total U.S. BANK NATIONAL ASSOC (EQUIP FINANCE):						412.85	.00					
<b>UNITED OIL</b>												
316	UNITED OIL	530529	8045	1500 GALLONS UNLEADED FUEL, TREATMENT PLANT, T. SHAFFER, FEB.'19	02/08/2019	2,859.00	.00	21-6300 FUEL	0	2/19		
Total 530529:						2,859.00	.00					
316	UNITED OIL	530530		CREDIT - INCORRECT PRICE ENTERED	02/08/2019	-2,859.00	.00	21-6300 FUEL	0	2/19		
Total 530530:						-2,859.00	.00					
316	UNITED OIL	530531		1500 GALLONS OF UNLEADED, T.SHAFFER, FEB.'19	02/08/2019	3,133.50	.00	21-6300 FUEL	0	2/19		
Total 530531:						3,133.50	.00					
Total UNITED OIL:						3,133.50	.00					
<b>UNIVAR USA, INC.</b>												
1410	UNIVAR USA, INC.	NA369990		CREDIT/REIMBURSEMENT, RETURNABLE POLY CONTAINER, FEB.'19	02/11/2019	-700.00	.00	21-6097 DEPOSITS ON ACCOUNT	0	2/19		
Total NA369990:						-700.00	.00					
1410	UNIVAR USA, INC.	NA601866	8048	2 TOTES OF SODIUM HYPOCHLORITE, 2 TOTES OF CITRIC ACID, 2 BARRELS OF SODIUM BISULFITE, T. SHAFFER, FEB.'19	02/08/2019	2,244.39	.00	21-6151 M & R - PROCESS CHEMICALS	0	2/19		
1410	UNIVAR USA, INC.	NA601866	8048	DEPOSIT ON 2 POLY RETURNABLE CONTAINERS, T.SHAFFER, FEB.'19	02/08/2019	80.00	.00	21-6097 DEPOSITS ON ACCOUNT	0	2/19		

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Total NA601866:						2,324.39	.00					
1410	UNIVAR USA, INC.	NA601909	8048	<u>2700 LBS CITRIC ACID, T.SHAFFER, FEB.'19</u>	02/11/2019	1,676.70	.00	<u>21-6151 M &amp; R - PROCESS CHEMICALS</u>	0	2/19		
Total NA601909:						1,676.70	.00					
1410	UNIVAR USA, INC.	NA601928	8048	<u>2 CONTAINERS LIQUICHLOR, T.SHAFFER, FEB.'19</u>	02/11/2019	1,920.60	.00	<u>21-6151 M &amp; R - PROCESS CHEMICALS</u>	0	2/19		
1410	UNIVAR USA, INC.	NA601928	8048	<u>DEPOSIT ON 2 RETURNABLE POLY CONTAINERS, T.SHAFFER, FEB.'19</u>	02/11/2019	1,400.00	.00	<u>21-6097 DEPOSITS ON ACCOUNT</u>	0	2/19		
Total NA601928:						3,320.60	.00					
Total UNIVAR USA, INC.:						6,621.69	.00					
<b>USA BLUE BOOK</b>												
265	USA BLUE BOOK	816798	8079	<u>HAND SANITIZER, T.SHAFFER, FEB.'19</u>	02/19/2019	55.30	.00	<u>21-6230 SAFETY TRAINING &amp; EQUIPMENT</u>	0	2/19		
265	USA BLUE BOOK	816798	8079	<u>AMMONIA REAGENT, AMMONIA TESTS, PHOSPHORUS REACTIVE, TEST KITS, COD REAGENT, PHOSPHATE REAGENT, BLANKING AGENT, CHLORINE TESTS, T.SHAFFER, FEB.'19</u>	02/19/2019	977.52	.00	<u>21-6152 M &amp; R - LABORATORY COSTS</u>	0	2/19		
Total 816798:						1,032.82	.00					
Total USA BLUE BOOK:						1,032.82	.00					
<b>UTILITY REFUND #6</b>												
1951	UTILITY REFUND #6	110450.02		<u>THOMAS B ORTZ JR, 340 E BOISE ST, UTILITY REFUND</u>	02/22/2019	16.71	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		

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1951	UTILITY REFUND #6	110450.02		<u>THOMAS B ORTZ JR, 340 E BOISE ST, UTILITY REFUND</u>	02/22/2019	21.69	.00	<u>21-4600_SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	110450.02		<u>THOMAS B ORTZ JR, 340 E BOISE ST, UTILITY REFUND</u>	02/22/2019	17.86	.00	<u>26-4975_SOLID WASTE USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	110450.02		<u>THOMAS B ORTZ JR, 340 E BOISE ST, UTILITY REFUND</u>	02/22/2019	6.42	.00	<u>25-4700_PRESS, IRRIGATION USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	110450.02		<u>THOMAS B ORTZ JR, 340 E BOISE ST, UTILITY REFUND</u>	02/22/2019	1.33	.00	<u>20-4510_SERVICE RECONNECT FEES</u>	0	2/19		
Total 110450.02:						64.01	.00					
1951	UTILITY REFUND #6	111020.04		<u>DAWN M BREWBAKER, 685 E MOONHILL ST, UTILITY REFUND</u>	02/22/2019	7.48	.00	<u>20-4500_METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	111020.04		<u>DAWN M BREWBAKER, 685 E MOONHILL ST, UTILITY REFUND</u>	02/22/2019	9.72	.00	<u>21-4600_SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	111020.04		<u>DAWN M BREWBAKER, 685 E MOONHILL ST, UTILITY REFUND</u>	02/22/2019	7.47	.00	<u>26-4975_SOLID WASTE USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	111020.04		<u>DAWN M BREWBAKER, 685 E MOONHILL ST, UTILITY REFUND</u>	02/22/2019	5.43	.00	<u>25-4700_PRESS, IRRIGATION USER FEES</u>	0	2/19		
Total 111020.04:						30.10	.00					
1951	UTILITY REFUND #6	161000.03		<u>JASON F OSWALD, 356 W WHITE WAY, UTILITY REFUND</u>	02/22/2019	2.06	.00	<u>20-4500_METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	161000.03		<u>JASON F OSWALD, 356 W WHITE WAY, UTILITY REFUND</u>	02/22/2019	2.66	.00	<u>21-4600_SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	161000.03		<u>JASON F OSWALD, 356 W WHITE WAY, UTILITY REFUND</u>	02/22/2019	2.21	.00	<u>26-4975_SOLID WASTE USER FEES</u>	0	2/19		

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Vendor #	Vendor Name	Invoice Number	PO #	Description	Invoice Date	Net Invoice Amount	Amount Paid	GL Account and Title	GL Activity #	GL Period	Date Paid	Voided
1951	UTILITY REFUND #6	161000.03		<u>JASON F OSWALD, 356 W WHITE WAY, UTILITY REFUND</u>	02/22/2019	.78	.00	25-4700_PRESS. IRRIGATION USER FEES	0	2/19		
1951	UTILITY REFUND #6	161000.03		<u>JASON F OSWALD, 356 W WHITE WAY, UTILITY REFUND</u>	02/22/2019	.27	.00	20-4510_SERVICE RECONNECT FEES	0	2/19		
Total 161000.03:						7.98	.00					
1951	UTILITY REFUND #6	170480.01		<u>TRISTAN THORNE, 402 S WAGONTOWN AVE, UTILITY REFUND</u>	02/22/2019	17.77	.00	20-4500_METERED WATER SALES	0	2/19		
1951	UTILITY REFUND #6	170480.01		<u>TRISTAN THORNE, 402 S WAGONTOWN AVE, UTILITY REFUND</u>	02/22/2019	22.65	.00	21-4600_SEWER USER FEES	0	2/19		
1951	UTILITY REFUND #6	170480.01		<u>TRISTAN THORNE, 402 S WAGONTOWN AVE, UTILITY REFUND</u>	02/22/2019	18.10	.00	26-4975_SOLID WASTE USER FEES	0	2/19		
1951	UTILITY REFUND #6	170480.01		<u>TRISTAN THORNE, 402 S WAGONTOWN AVE, UTILITY REFUND</u>	02/22/2019	6.97	.00	25-4700_PRESS. IRRIGATION USER FEES	0	2/19		
1951	UTILITY REFUND #6	170480.01		<u>TRISTAN THORNE, 402 S WAGONTOWN AVE, UTILITY REFUND</u>	02/22/2019	.69	.00	20-4500_METERED WATER SALES	0	2/19		
Total 170480.01:						66.18	.00					
1951	UTILITY REFUND #6	174104.01A		<u>CBH HOMES, 1739 W SAHARA DR, UTILITY REFUND</u>	02/22/2019	58.76	.00	20-4500_METERED WATER SALES	0	2/19		
Total 174104.01A:						58.76	.00					
1951	UTILITY REFUND #6	175009.01A		<u>CBH HOMES, 1430 W BAYHORSE ST, UTILITY REFUND</u>	02/22/2019	26.80	.00	20-4500_METERED WATER SALES	0	2/19		
1951	UTILITY REFUND #6	175009.01A		<u>CBH HOMES, 1430 W BAYHORSE ST, UTILITY REFUND</u>	02/22/2019	33.27	.00	21-4600_SEWER USER FEES	0	2/19		

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Total 175009.01A:						60.07	.00					
1951	UTILITY REFUND #6	205025.02		<u>TRAVIS TAYLOR, 398 E WHITBECK ST. UTILITY REFUND</u>	02/27/2019	1.27	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	205025.02		<u>TRAVIS TAYLOR, 398 E WHITBECK ST. UTILITY REFUND</u>	02/27/2019	1.46	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	205025.02		<u>TRAVIS TAYLOR, 398 E WHITBECK ST. UTILITY REFUND</u>	02/27/2019	1.02	.00	<u>26-4975 SOLID WASTE USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	205025.02		<u>TRAVIS TAYLOR, 398 E WHITBECK ST. UTILITY REFUND</u>	02/27/2019	5.65	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 205025.02:						9.40	.00					
1951	UTILITY REFUND #6	221350.02		<u>KEVIN YATES, 1065 E LUDLOW ST. UTILITY REFUND</u>	02/22/2019	6.94	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	221350.02		<u>KEVIN YATES, 1065 E LUDLOW ST. UTILITY REFUND</u>	02/22/2019	9.02	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	221350.02		<u>KEVIN YATES, 1065 E LUDLOW ST. UTILITY REFUND</u>	02/22/2019	6.91	.00	<u>26-4975 SOLID WASTE USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	221350.02		<u>KEVIN YATES, 1065 E LUDLOW ST. UTILITY REFUND</u>	02/22/2019	2.67	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 221350.02:						25.54	.00					
1951	UTILITY REFUND #6	250960.03		<u>KARL STOLTZ, 99 W STEPH ST. UTILITY REFUND</u>	02/27/2019	13.85	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	250960.03		<u>KARL STOLTZ, 99 W STEPH ST. UTILITY REFUND</u>	02/27/2019	17.30	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		

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1951	UTILITY REFUND #6	250960.03		<u>KARL STOLTZ, 99 W STEPH ST, UTILITY REFUND</u>	02/27/2019	13.53	.00	<u>26-4975 SOLID WASTE USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	250960.03		<u>KARL STOLTZ, 99 W STEPH ST, UTILITY REFUND</u>	02/27/2019	21.01	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 250960.03:						65.69	.00					
1951	UTILITY REFUND #6	252040.02		<u>WATAHZ JOHNSON, 1046 S CHALKBOARD PL, UTILITY REFUND</u>	02/22/2019	7.78	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	252040.02		<u>WATAHZ JOHNSON, 1046 S CHALKBOARD PL, UTILITY REFUND</u>	02/22/2019	10.12	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	252040.02		<u>WATAHZ JOHNSON, 1046 S CHALKBOARD PL, UTILITY REFUND</u>	02/22/2019	7.84	.00	<u>26-4975 SOLID WASTE USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	252040.02		<u>WATAHZ JOHNSON, 1046 S CHALKBOARD PL, UTILITY REFUND</u>	02/22/2019	3.11	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 252040.02:						28.85	.00					
1951	UTILITY REFUND #6	264460.02		<u>ERIC ROBERTSON, 1975 W MELON DR, UTILITY REFUND</u>	02/22/2019	17.08	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	264460.02		<u>ERIC ROBERTSON, 1975 W MELON DR, UTILITY REFUND</u>	02/22/2019	20.92	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	264460.02		<u>ERIC ROBERTSON, 1975 W MELON DR, UTILITY REFUND</u>	02/22/2019	17.80	.00	<u>26-4975 SOLID WASTE USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	264460.02		<u>ERIC ROBERTSON, 1975 W MELON DR, UTILITY REFUND</u>	02/22/2019	6.33	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 264460.02:						62.13	.00					

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1951	UTILITY REFUND #6	264700.02		<u>ARLENE FRANK, 2010 N VAN DYKE AVE, UTILITY REFUND</u>	02/27/2019	17.43	.00	<u>20-4500_METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	264700.02		<u>ARLENE FRANK, 2010 N VAN DYKE AVE, UTILITY REFUND</u>	02/27/2019	22.61	.00	<u>21-4600_SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	264700.02		<u>ARLENE FRANK, 2010 N VAN DYKE AVE, UTILITY REFUND</u>	02/27/2019	19.28	.00	<u>26-4975_SOLID WASTE USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	264700.02		<u>ARLENE FRANK, 2010 N VAN DYKE AVE, UTILITY REFUND</u>	02/27/2019	12.68	.00	<u>25-4700_PRESS. IRRIGATION USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	264700.02		<u>ARLENE FRANK, 2010 N VAN DYKE AVE, UTILITY REFUND</u>	02/27/2019	.12	.00	<u>20-4500_METERED WATER SALES</u>	0	2/19		
Total 264700.02:						72.12	.00					
1951	UTILITY REFUND #6	268131.01		<u>CBH HOMES, 1631 N VERIDIAN AVE, UTILITY REFUND</u>	02/22/2019	25.66	.00	<u>20-4500_METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	268131.01		<u>CBH HOMES, 1631 N VERIDIAN AVE, UTILITY REFUND</u>	02/22/2019	33.33	.00	<u>21-4600_SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	268131.01		<u>CBH HOMES, 1631 N VERIDIAN AVE, UTILITY REFUND</u>	02/22/2019	15.53	.00	<u>25-4700_PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 268131.01:						74.52	.00					
1951	UTILITY REFUND #6	268134.01A		<u>CBH HOMES, 1667 N VERIDIAN AVE, UTILITY REFUND</u>	02/22/2019	58.76	.00	<u>20-4500_METERED WATER SALES</u>	0	2/19		
Total 268134.01A:						58.76	.00					
1951	UTILITY REFUND #6	277002.02		<u>MARK E EVANS, 651 W ALLSPICE ST, UTILITY REFUND</u>	02/22/2019	36.17	.00	<u>20-4500_METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	277002.02		<u>MARK E EVANS, 651 W ALLSPICE ST, UTILITY REFUND</u>	02/22/2019	46.98	.00	<u>21-4600_SEWER USER FEES</u>	0	2/19		

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1951	UTILITY REFUND #6	277002.02		<u>MARK E EVANS, 651 W ALLSPICE ST, UTILITY REFUND</u>	02/22/2019	40.93	.00	<u>26-4975 SOLID WASTE USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	277002.02		<u>MARK E EVANS, 651 W ALLSPICE ST, UTILITY REFUND</u>	02/22/2019	19.88	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 277002.02:						143.96	.00					
1951	UTILITY REFUND #6	277108.01		<u>CBH HOMES, 731 W ALLSPICE ST, UTILITY REFUND</u>	02/22/2019	24.75	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	277108.01		<u>CBH HOMES, 731 W ALLSPICE ST, UTILITY REFUND</u>	02/22/2019	32.12	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	277108.01		<u>CBH HOMES, 731 W ALLSPICE ST, UTILITY REFUND</u>	02/22/2019	15.14	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 277108.01:						72.01	.00					
1951	UTILITY REFUND #6	277112.01		<u>CBH HOMES, 688 W ALLSPICE ST, UTILITY REFUND</u>	02/22/2019	26.57	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	277112.01		<u>CBH HOMES, 688 W ALLSPICE ST, UTILITY REFUND</u>	02/22/2019	34.50	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	277112.01		<u>CBH HOMES, 688 W ALLSPICE ST, UTILITY REFUND</u>	02/22/2019	15.01	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 277112.01:						76.08	.00					
1951	UTILITY REFUND #6	277316.01		<u>CBH HOMES, 308 W SCREECH OWL DR, UTILITY REFUND</u>	02/22/2019	14.07	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	277316.01		<u>CBH HOMES, 308 W SCREECH OWL DR, UTILITY REFUND</u>	02/22/2019	18.27	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	277316.01		<u>CBH HOMES, 308 W SCREECH OWL DR, UTILITY REFUND</u>	02/22/2019	13.80	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		

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Total 277316.01:						46.14	.00					
1951	UTILITY REFUND #6	277328.01		<u>CBH HOMES, 307 W SCREECH OWL DR, UTILITY REFUND</u>	02/22/2019	20.63	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	277328.01		<u>CBH HOMES, 307 W SCREECH OWL DR, UTILITY REFUND</u>	02/22/2019	26.79	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	277328.01		<u>CBH HOMES, 307 W SCREECH OWL DR, UTILITY REFUND</u>	02/22/2019	18.14	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 277328.01:						65.56	.00					
1951	UTILITY REFUND #6	277415.01A		<u>CBH HOMES, 64 W SNOWY OWL ST, UTILITY REFUND</u>	02/22/2019	58.73	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
Total 277415.01A:						58.73	.00					
1951	UTILITY REFUND #6	277416.01		<u>CBH HOMES, 46 W SNOWY OWL ST, UTILITY REFUND</u>	02/22/2019	25.66	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	277416.01		<u>CBH HOMES, 46 W SNOWY OWL ST, UTILITY REFUND</u>	02/22/2019	33.33	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	277416.01		<u>CBH HOMES, 46 W SNOWY OWL ST, UTILITY REFUND</u>	02/22/2019	17.50	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 277416.01:						76.49	.00					
1951	UTILITY REFUND #6	277423.01		<u>CBH HOMES, 2313 N DOE AVE, UTILITY REFUND</u>	02/22/2019	9.78	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	277423.01		<u>CBH HOMES, 2313 N DOE AVE, UTILITY REFUND</u>	02/22/2019	12.71	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	277423.01		<u>CBH HOMES, 2313 N DOE AVE, UTILITY REFUND</u>	02/22/2019	17.24	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		

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Total 277423.01:						39.73	.00					
1951	UTILITY REFUND #6	278219.01		<u>CBH HOMES, 3100 W GRANNY SMITH CT. UTILITY REFUND</u>	02/22/2019	2.95	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	278219.01		<u>CBH HOMES, 3100 W GRANNY SMITH CT. UTILITY REFUND</u>	02/22/2019	3.84	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	278219.01		<u>CBH HOMES, 3100 W GRANNY SMITH CT. UTILITY REFUND</u>	02/22/2019	4.73	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 278219.01:						11.52	.00					
1951	UTILITY REFUND #6	278224.01		<u>CBH HOMES, 9254 S BRAEBURN AVE. UTILITY REFUND</u>	02/22/2019	36.00	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	278224.01		<u>CBH HOMES, 9254 S BRAEBURN AVE. UTILITY REFUND</u>	02/22/2019	46.75	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	278224.01		<u>CBH HOMES, 9254 S BRAEBURN AVE. UTILITY REFUND</u>	02/22/2019	15.78	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 278224.01:						98.53	.00					
1951	UTILITY REFUND #6	278227.01		<u>CBH HOMES, 9320 S BRAEBURN AVE. UTILITY REFUND</u>	02/22/2019	8.32	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	278227.01		<u>CBH HOMES, 9320 S BRAEBURN AVE. UTILITY REFUND</u>	02/22/2019	10.80	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	278227.01		<u>CBH HOMES, 9320 S BRAEBURN AVE. UTILITY REFUND</u>	02/22/2019	7.64	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 278227.01:						26.76	.00					

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1951	UTILITY REFUND #6	291004.01A		<u>CBH HOMES, 3345 W DEVOTION DR, UTILITY REFUND</u>	02/22/2019	58.76	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
Total 291004.01A:						58.76	.00					
1951	UTILITY REFUND #6	291007.01		<u>CBH HOMES, 6953 S NORDEAN AVE, UTILITY REFUND</u>	02/22/2019	22.44	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	291007.01		<u>CBH HOMES, 6953 S NORDEAN AVE, UTILITY REFUND</u>	02/22/2019	29.14	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	291007.01		<u>CBH HOMES, 6953 S NORDEAN AVE, UTILITY REFUND</u>	02/22/2019	20.89	.00	<u>25-4700 PRESS, IRRIGATION USER FEES</u>	0	2/19		
Total 291007.01:						72.47	.00					
1951	UTILITY REFUND #6	291022.01A		<u>CBH HOMES, 6906 S NORDEAN AVE, UTILITY REFUND</u>	02/22/2019	58.76	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
Total 291022.01A:						58.76	.00					
1951	UTILITY REFUND #6	291042.01		<u>CBH HOMES, 6932 S DONAWAY AVE, UTILITY REFUND</u>	02/22/2019	21.06	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	291042.01		<u>CBH HOMES, 6932 S DONAWAY AVE, UTILITY REFUND</u>	02/22/2019	27.34	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	291042.01		<u>CBH HOMES, 6932 S DONAWAY AVE, UTILITY REFUND</u>	02/22/2019	19.16	.00	<u>25-4700 PRESS, IRRIGATION USER FEES</u>	0	2/19		
Total 291042.01:						67.56	.00					
1951	UTILITY REFUND #6	302109.01		<u>FLAHERTY CUSTOM HOMES, 773 E ANDES DR, UTILITY REFUND</u>	02/22/2019	19.91	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	302109.01		<u>FLAHERTY CUSTOM HOMES, 773 E ANDES DR, UTILITY REFUND</u>	02/22/2019	25.86	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		

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1951	UTILITY REFUND #6	302109.01		<u>FLAHERTY CUSTOM HOMES, 773 E ANDES DR, UTILITY REFUND</u>	02/22/2019	19.96	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 302109.01:						65.73	.00					
1951	UTILITY REFUND #6	302135.01A		<u>RIVERWOOD HOMES, 1078 E ANDES DR, UTILITY REFUND</u>	02/22/2019	58.76	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
Total 302135.01A:						58.76	.00					
1951	UTILITY REFUND #6	302145.01		<u>FLAHERTY CUSTOM HOMES, 918 E ANDES DR, UTILITY REFUND</u>	02/22/2019	22.21	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	302145.01		<u>FLAHERTY CUSTOM HOMES, 918 E ANDES DR, UTILITY REFUND</u>	02/22/2019	28.84	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	302145.01		<u>FLAHERTY CUSTOM HOMES, 918 E ANDES DR, UTILITY REFUND</u>	02/22/2019	16.24	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 302145.01:						67.29	.00					
1951	UTILITY REFUND #6	302154.01		<u>RIVERWOOD HOMES, 742 E ANDES DR, UTILITY REFUND</u>	02/22/2019	20.29	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	302154.01		<u>RIVERWOOD HOMES, 742 E ANDES DR, UTILITY REFUND</u>	02/22/2019	26.35	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	302154.01		<u>RIVERWOOD HOMES, 742 E ANDES DR, UTILITY REFUND</u>	02/22/2019	26.10	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 302154.01:						72.74	.00					
1951	UTILITY REFUND #6	302155.01		<u>EAGLEWOOD HOMES, 726 E ANDES DR, UTILITY REFUND</u>	02/22/2019	19.40	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	302155.01		<u>EAGLEWOOD HOMES, 726 E ANDES DR, UTILITY REFUND</u>	02/22/2019	25.18	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		

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1951	UTILITY REFUND #6	302155.01		<u>EAGLEWOOD HOMES, 726 E ANDES DR, UTILITY REFUND</u>	02/22/2019	13.68	.00	<u>25-4700_PRESS, IRRIGATION USER FEES</u>	0	2/19		
Total 302155.01:						58.26	.00					
1951	UTILITY REFUND #6	303203.01A		<u>HUBBLE HOMES, 2595 N ELK CREEK AVE, UTILITY REFUND</u>	02/22/2019	58.76	.00	<u>20-4500_METERED WATER SALES</u>	0	2/19		
Total 303203.01A:						58.76	.00					
1951	UTILITY REFUND #6	303204.01A		<u>HUBBLE HOMES, 2575 N ELK CREEK AVE, UTILITY REFUND</u>	02/22/2019	58.76	.00	<u>20-4500_METERED WATER SALES</u>	0	2/19		
Total 303204.01A:						58.76	.00					
1951	UTILITY REFUND #6	303214.01A		<u>HUBBLE HOMES, 2270 N HOSE GULCH AVE, UTILITY REFUND</u>	02/22/2019	58.76	.00	<u>20-4500_METERED WATER SALES</u>	0	2/19		
Total 303214.01A:						58.76	.00					
1951	UTILITY REFUND #6	303215.01		<u>HUBBLE HOMES, 2284 N HOSE GULCH AVE, UTILITY REFUND</u>	02/22/2019	44.11	.00	<u>20-4500_METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	303215.01		<u>HUBBLE HOMES, 2284 N HOSE GULCH AVE, UTILITY REFUND</u>	02/22/2019	57.28	.00	<u>21-4600_SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	303215.01		<u>HUBBLE HOMES, 2284 N HOSE GULCH AVE, UTILITY REFUND</u>	02/22/2019	24.71	.00	<u>25-4700_PRESS, IRRIGATION USER FEES</u>	0	2/19		
Total 303215.01:						126.10	.00					
1951	UTILITY REFUND #6	303217.01A		<u>HUBBLE HOMES, 2312 N HOSE GULCH, UTILITY REFUND</u>	02/04/2019	4.18	4.18	<u>20-4500_METERED WATER SALES</u>	0	2/19	02/20/2019	
1951	UTILITY REFUND #6	303217.01A		<u>HUBBLE HOMES, 2312 N HOSE GULCH, UTILITY REFUND</u>	02/04/2019	5.41	5.41	<u>21-4600_SEWER USER FEES</u>	0	2/19	02/20/2019	

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1951	UTILITY REFUND #6	303217.01A		<u>HUBBLE HOMES, 2312 N HOSE GULCH, UTILITY REFUND</u>	02/04/2019	3.05	3.05	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19	02/20/2019	
Total 303217.01A:						12.64	12.64					
1951	UTILITY REFUND #6	303217.01B		<u>HUBBLE HOMES, 2312 N HOSE GULCH AVE, UTILITY REFUND</u>	02/22/2019	58.76	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
Total 303217.01B:						58.76	.00					
1951	UTILITY REFUND #6	303221.01		<u>HUBBLE HOMES, 1110 E JACK CREEK ST, UTILITY REFUND</u>	02/22/2019	18.78	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	303221.01		<u>HUBBLE HOMES, 1110 E JACK CREEK ST, UTILITY REFUND</u>	02/22/2019	24.40	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	303221.01		<u>HUBBLE HOMES, 1110 E JACK CREEK ST, UTILITY REFUND</u>	02/22/2019	13.92	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 303221.01:						57.10	.00					
1951	UTILITY REFUND #6	303224.01		<u>HUBBLE HOMES, 1044 E JACK CREEK ST, UTILITY REFUND</u>	02/22/2019	22.33	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	303224.01		<u>HUBBLE HOMES, 1044 E JACK CREEK ST, UTILITY REFUND</u>	02/22/2019	28.99	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	303224.01		<u>HUBBLE HOMES, 1044 E JACK CREEK ST, UTILITY REFUND</u>	02/22/2019	13.84	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 303224.01:						65.16	.00					
1951	UTILITY REFUND #6	303226.01A		<u>HUBBLE HOMES, 1051 E FIRESTONE DR, UTILITY REFUND</u>	02/22/2019	58.76	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		

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Total 303226.01A:						58.76	.00					
1951	UTILITY REFUND #6	303228.01		<u>HUBBLE HOMES, 1075 E FIRESTONE DR, UTILITY REFUND</u>	02/22/2019	22.72	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	303228.01		<u>HUBBLE HOMES, 1075 E FIRESTONE DR, UTILITY REFUND</u>	02/22/2019	29.51	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	303228.01		<u>HUBBLE HOMES, 1075 E FIRESTONE DR, UTILITY REFUND</u>	02/22/2019	14.76	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 303228.01:						66.99	.00					
1951	UTILITY REFUND #6	303229.01		<u>HUBBLE HOMES, 1087 E FIRESTONE DR, UTILITY REFUND</u>	02/27/2019	57.13	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	303229.01		<u>HUBBLE HOMES, 1087 E FIRESTONE DR, UTILITY REFUND</u>	02/27/2019	33.24	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	303229.01		<u>HUBBLE HOMES, 1087 E FIRESTONE DR, UTILITY REFUND</u>	02/27/2019	20.88	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 303229.01:						111.25	.00					
1951	UTILITY REFUND #6	303230.01		<u>HUBBLE HOMES, 1101 E FIRESTONE DR, UTILITY REFUND</u>	02/22/2019	47.60	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	303230.01		<u>HUBBLE HOMES, 1101 E FIRESTONE DR, UTILITY REFUND</u>	02/22/2019	63.22	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	303230.01		<u>HUBBLE HOMES, 1101 E FIRESTONE DR, UTILITY REFUND</u>	02/22/2019	24.55	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		

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Total 303230.01:						135.37	.00					
1951	UTILITY REFUND #6	310022.01A		<u>COLEMAN HOMES, 9458 S SARATOV WAY, UTILITY REFUND</u>	02/22/2019	58.76	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
Total 310022.01A:						58.76	.00					
1951	UTILITY REFUND #6	310042.01		<u>TOLL BROS INC, 9455 S SARATOV WAY, UTILITY REFUND</u>	02/27/2019	34.18	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	310042.01		<u>TOLL BROS INC, 9455 S SARATOV WAY, UTILITY REFUND</u>	02/27/2019	13.27	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	310042.01		<u>TOLL BROS INC, 9455 S SARATOV WAY, UTILITY REFUND</u>	02/27/2019	22.47	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 310042.01:						69.92	.00					
1951	UTILITY REFUND #6	318322.01		<u>HOPE DEVELOPMENT, 1108 E SAILER SHORES ST, UTILITY REFUND</u>	02/22/2019	38.05	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	318322.01		<u>HOPE DEVELOPMENT, 1108 E SAILER SHORES ST, UTILITY REFUND</u>	02/22/2019	49.43	.00	<u>21-4600 SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	318322.01		<u>HOPE DEVELOPMENT, 1108 E SAILER SHORES ST, UTILITY REFUND</u>	02/22/2019	39.86	.00	<u>25-4700 PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 318322.01:						127.34	.00					
1951	UTILITY REFUND #6	320011.00A		<u>HAYDEN HOMES, 1443 N STEENS AVE, UTILITY REFUND</u>	02/22/2019	58.76	.00	<u>20-4500 METERED WATER SALES</u>	0	2/19		
Total 320011.00A:						58.76	.00					

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1951	UTILITY REFUND #6	320020.00		<u>HAYDEN HOMES, 1900 E JOHN DEERE ST, UTILITY REFUND</u>	02/22/2019	13.88	.00	<u>20-4500_METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	320020.00		<u>HAYDEN HOMES, 1900 E JOHN DEERE ST, UTILITY REFUND</u>	02/22/2019	18.03	.00	<u>21-4600_SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	320020.00		<u>HAYDEN HOMES, 1900 E JOHN DEERE ST, UTILITY REFUND</u>	02/22/2019	15.33	.00	<u>25-4700_PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 320020.00:						47.24	.00					
1951	UTILITY REFUND #6	320021.00A		<u>HAYDEN HOMES, 1878 E JOHN DEERE ST, UTILITY REFUND</u>	02/22/2019	58.76	.00	<u>20-4500_METERED WATER SALES</u>	0	2/19		
Total 320021.00A:						58.76	.00					
1951	UTILITY REFUND #6	320029.00		<u>HAYDEN HOMES, 1835 E JOHN DEERE ST, UTILITY REFUND</u>	02/22/2019	19.30	.00	<u>20-4500_METERED WATER SALES</u>	0	2/19		
1951	UTILITY REFUND #6	320029.00		<u>HAYDEN HOMES, 1835 E JOHN DEERE ST, UTILITY REFUND</u>	02/22/2019	25.06	.00	<u>21-4600_SEWER USER FEES</u>	0	2/19		
1951	UTILITY REFUND #6	320029.00		<u>HAYDEN HOMES, 1835 E JOHN DEERE ST, UTILITY REFUND</u>	02/22/2019	23.37	.00	<u>25-4700_PRESS. IRRIGATION USER FEES</u>	0	2/19		
Total 320029.00:						67.73	.00					
Total UTILITY REFUND #6:						3,336.87	12.64					
<b>VALLI INFORMATION SYSTEMS, INC</b>												
857	VALLI INFORMATION SYSTEMS, INC	51248		<u>POSTAGE, ESTATEMENTS, IMAGING, JAN.'19 - ADMIN</u>	01/31/2019	943.66	.00	<u>01-6190_POSTAGE &amp; BILLING</u>	0	2/19		
857	VALLI INFORMATION SYSTEMS, INC	51248		<u>POSTAGE, ESTATEMENTS, IMAGING, JAN.'19 - WATER</u>	01/31/2019	1,482.89	.00	<u>20-6190_POSTAGE &amp; BILLING</u>	0	2/19		
857	VALLI INFORMATION SYSTEMS, INC	51248		<u>POSTAGE, ESTATEMENTS, IMAGING, JAN.'19 - SEWER</u>	01/31/2019	1,482.89	.00	<u>21-6190_POSTAGE &amp; BILLING</u>	0	2/19		

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857	VALLI INFORMATION SYSTEMS, INC	51248		<u>POSTAGE, ESTATEMENTS, IMAGING, JAN.'19 - P.I</u>	01/31/2019	584.16	.00	<u>25-6190 POSTAGE &amp; BILLING</u>	0	2/19		
Total 51248:						4,493.60	.00					
857	VALLI INFORMATION SYSTEMS, INC	51249		<u>LOCKBOX TRANSACTIONS AND EPAY MONTHLY MAINTENANCE FOR JAN.'19 - ADMIN</u>	01/31/2019	53.91	.00	<u>01-6190 POSTAGE &amp; BILLING</u>	0	2/19		
857	VALLI INFORMATION SYSTEMS, INC	51249		<u>LOCKBOX TRANSACTIONS AND EPAY MONTHLY MAINTENANCE FOR JAN.'19 - WATER</u>	01/31/2019	84.71	.00	<u>20-6190 POSTAGE &amp; BILLING</u>	0	2/19		
857	VALLI INFORMATION SYSTEMS, INC	51249		<u>LOCKBOX TRANSACTIONS AND EPAY MONTHLY MAINTENANCE FOR JAN.'19 - SEWER</u>	01/31/2019	84.71	.00	<u>21-6190 POSTAGE &amp; BILLING</u>	0	2/19		
857	VALLI INFORMATION SYSTEMS, INC	51249		<u>LOCKBOX TRANSACTIONS AND EPAY MONTHLY MAINTENANCE FOR JAN.'19 - P.I</u>	01/31/2019	33.38	.00	<u>25-6190 POSTAGE &amp; BILLING</u>	0	2/19		
Total 51249:						256.71	.00					
Total VALLI INFORMATION SYSTEMS, INC:						4,750.31	.00					
Grand Totals:						540,641.75	162,927.09					

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Dated: \_\_\_\_\_

Mayor: \_\_\_\_\_

City Council: \_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

City Treasurer: \_\_\_\_\_

Report Criteria:

Detail report.

Invoices with totals above \$0.00 included.

Paid and unpaid invoices included.

**RESOLUTION NO. R15-2019  
CITY OF KUNA, IDAHO**

**A RESOLUTION OF THE CITY COUNCIL FOR KUNA, IDAHO APPROVING A PERFORMANCE BOND BY DB DEVELOPMENT, LLC, FOR THE DESERTHAWK SUBDIVISION NO. 4 FOR UNCOMPLETED WORK INCLUDING STREET LIGHTING, FENCING AND LANDSCAPING PURSUANT TO THE TERMS OF THIS RESOLUTION.**

**WHEREAS** Deserthawk Subdivision No. 4 exists as part of an approved preliminary plat; and

**WHEREAS** construction plans for Deserthawk Subdivision No. 4 were approved by the Kuna City Engineer on September 6, 2018; and

**WHEREAS** construction was commenced but not completed for certain items, per the approved plans; and

**WHEREAS** the street lighting, fencing and landscaping have not been completed for Deserthawk Subdivision No. 4 according to the approved construction plans and developer seeks to bond for the unfinished work; and

**WHEREAS** the street lighting completion has been estimated at forty-four thousand eight hundred seventy-five dollars and zero cents (\$44,875.00) adding 25% for a total of fifty-six thousand ninety-three dollars and seventy-five cents (\$56,093.75); and

**WHEREAS** the fencing completion has been estimated at sixty-three thousand two hundred sixty-one dollars and thirty cents (\$63,261.30) adding 25% for a total of seventy-nine thousand seventy-six dollars and sixty-three cents (\$79,076.63); and

**WHEREAS** the landscaping completion has been estimated at eighty-five thousand two hundred nine dollars and eighty-nine cents (\$85,209.89) adding 25% for a total of one hundred six thousand five hundred twelve dollars and thirty-six cents (\$106,512.36); and

**WHEREAS** developer desires to record the final plat for Deserthawk Subdivision No. 4 prior to completion of construction; and

**WHEREAS** Kuna City Code 6-2-4 and 6-4-3 allows for and sets the conditions for recording a final plat prior to the completion of construction:

**BE IT HEREBY RESOLVED** by the Mayor and Council of the City of Kuna, Idaho, that the Kuna City Engineer is hereby authorized to accept an irrevocable standby Letter of Credit in lieu of construction for Deserthawk Subdivision No. 4 under the following terms and conditions:

1. All bids amounts submitted for unfinished construction are valid for the life of the Letter of Credit;

2. The Letter of Credit is irrevocable, is drawn upon an FDIC or FSLIC insured institution, is an institution with an office where presentment can be made within 50 miles of Kuna City Hall, the Letter of Credit is claimable up to 30 days prior to expiry and expiry is not more than one year from the date of issuance;
3. The face amount of the Letter of Credit is at least two hundred forty-one thousand six hundred eighty-two dollars and seventy-four cents (\$241,682.74);
4. No more than fifty percent of available permits can be claimed during the life of the Letter of Credit and if improvements are not completed within 120 days of issuance of the Letter of Credit, no further building permits can be issued.

**PASSED BY THE COUNCIL** of Kuna, Idaho this 5<sup>th</sup> day of March, 2019.

**APPROVED BY THE MAYOR** of Kuna, Idaho this 5<sup>th</sup> day of March, 2019.

\_\_\_\_\_  
Joe L. Stear, Mayor

ATTEST:

\_\_\_\_\_  
Chris Engels, City Clerk

## CITY OF KUNA IMPROVEMENT AGREEMENT (CASH BOND)

THIS AGREEMENT is made by and between DB DEVELOPMENT, LLC, (hereinafter “Developer”); whose address is 2228 W Piazza St., Meridian, ID 83646, and CITY OF KUNA, a municipal corporation of the State of Idaho, (hereinafter “City”); whose address is Post Office Box 13, Kuna, Idaho 83634.

WHEREAS, Developer desires to record its final plat for Phase No. 4 of the development known as Deserthawk Subdivision, (“Development”) located in the City of Kuna; and

WHEREAS, City will not sign the final plat unless Developer promises to install and warrant certain Improvements as herein provided and security is provided for that promise as set forth herein.

NOW, THEREFORE, THE PARTIES HEREBY AGREE AS FOLLOWS:

1. Installation of Improvements. Developer agrees to complete and pay the total costs of all Improvements required by City and those specified in the following:
  - a. Approved Street Light Plan for Deserthawk Subdivision No. 4 and the associated Bid for Street Lights by Alloway Electric Co, Inc.,
  - b. Approved Fencing Plant for Deserthawk Subdivision No. 4 and the associated Bid for Fencing by Butte Fence, Inc.,
  - c. Approved Landscape Plan for Deserthawk Subdivision No. 4 and the associated Bid for Landscaping by Power Enterprises.

The bids for said Improvements are attached hereto as **Exhibit A**. The required Improvements are shown on the plans, drawings and specifications previously reviewed and approved by City in connection with the above described Development, and in accordance with the standards and specifications established by the City and adopted by the City Council.

2. Cash Deposit. Developer has executed and delivered to City cash, cashier’s check or wired funds (City to provide financial institution information upon execution of agreement) to the City’s trust account in the aggregate amount of two hundred forty-one thousand six hundred eighty-two dollars and seventy-four cents (\$241,682.74), for deposit with City in its accounts (the “Cash Deposit”), which includes:
  - a. The initial City Engineer or Public Works Director’s estimated cost of the remaining work shall, as determined, in part, from the detailed bids provided by the sub-divider's contractors in an amount, plus twenty-five (25) percent, for an amount of one hundred twenty-five (125) percent;
  - b. To that total, the following additional sums may be added upon the following considerations:

- i. Three (3) to ten (10) percent for inflation; ten (10) to fifteen (15) percent for the City's bidding disadvantage; and twelve (12) percent to twenty (20) percent for city project management as determined by the City Engineer or Public Works Director.
3. The Developer and City stipulate the amount to be a reasonable estimate, pursuant to Kuna City Code.
4. If construction of all financially pledged improvements are not completed within one hundred twenty (120) days following the date of recordation of the final plat, no further building permits shall be issued by the City until final completion of all improvements has occurred and the City has inspected and approved them. However, if the remaining improvements are not completed within the one hundred twenty-day period, through no fault of the Developer, the City Engineer or Public Works Director may grant a one-time, one hundred twenty-day (120) time extension. The determination of what may be considered a "no fault circumstance" shall be determined by the City Engineer or Public Works Director.
5. Refund or Withdrawal. City may withdraw funds from Cash Deposit if (1) Improvements are not completed as required by this Agreement within the time period specified in Paragraph 6, or if (2) Improvements are not installed strictly in accordance with Paragraph 1 and written notice of the deficiency has been given to Developer, who has failed to remedy the deficiency within ten (10) days after the notice is sent. In said event, City may withdraw funds from Cash Deposit both (1) those amounts necessary to either complete Improvements as required herein or alter or repair Improvements to conform to the requirements hereof, and (2) City's cost of administration incurred in obtaining Cash Deposit, including attorney's fees and court costs, which shall be deducted from any Cash Deposit. If the amount of Cash Deposit is inadequate to pay the cost of the completion of Improvements according to City's standards or specifications for whatever reason, including previous reductions, Developer shall be responsible for the deficiency and no further building permits shall be issued in the subdivision or development until Improvements are completed or, with City Council approval, a new, satisfactory security has been executed and delivered to City or other satisfactory arrangements have been made to insure completion of the remaining improvements.
6. Preliminary Release. At the time herein provided, but no later than at the time of final inspection and acceptance of all Improvements by City, City will authorize release of all funds comprising Cash Deposit. The release provided for in this paragraph shall occur when City certifies that Improvements are complete, which shall be when Improvements have been installed as required and fully inspected and approved by City, and after as-built drawings have been supplied as required.
7. Non-Release of Developer's Obligations. It is understood and agreed between the parties that the establishment and availability to City of Cash Deposit as herein provided, and any withdrawals there from by City shall not constitute a waiver or estoppel against City and shall not release or relieve Developer from its obligation to install and fully pay for Improvements as required in Paragraph 1 above, and the right of City to withdraw from

Cash Deposit shall not affect any rights and remedies of City against Developer for breach of any covenant herein, including the covenants of Paragraph 1 of this Agreement. Further, Developer agrees that if City withdraws from Cash Deposit and performs or causes to be performed the installation or warranty work required of Developer hereunder, then any and all costs incurred by City in so doing which are not collected by City by withdrawing from Cash Deposit shall be paid by Developer, including administrative, engineering, legal, labor and materials and other procurement fees and costs.

8. Upon satisfaction of this Agreement, Developer shall provide the City with its financial institution information including account wire transfer information.
9. Binding Effect and Assignment. This Agreement shall be binding upon, and inure to the benefit of, the heirs, officers, agents, legal representatives, successors and assigns of the parties hereto. No party shall assign or transfer any rights under this Agreement without the prior written consent of the other first obtained, which consent shall not be unreasonably withheld.
10. Notices. Any notice required or desired to be given hereunder as shall be deemed sufficient if sent by certified mail, postage prepaid, addressed to the respective parties at the addresses shown in the preamble.
11. Severability. Should any portion of this Agreement for any reason be declared invalid or unenforceable, the invalidity of such portion shall not affect the validity of any of the remaining portions and the same shall be deemed in full force and effect as if this Agreement had been executed with the invalid portions eliminated.
12. Governing Law. This Agreement and the performances hereunder shall be governed by the laws of the State of Idaho.
13. Counterparts. The fact that the parties hereto execute multiple but identical counterparts of this Agreement shall not affect the validity or efficacy of their execution, and such counterparts, taken together, shall constitute one and the same instrument, and each counterpart shall be deemed an original.
14. Waiver. No waiver of any of the provisions of this Agreement shall operate as a waiver of any other provision, regardless of any similarity that may exist between such provisions, nor shall a waiver in one instance operate as a waiver in any future event. No waiver shall be binding unless executed in writing by the waiving party.
15. Captions. The captions preceding the paragraphs of this Agreement are for convenience only and shall not affect the interpretation of any provision herein.
16. Entire Agreement. This Agreement, together with its exhibits and the approved plans and specifications referred to, contains the entire and integrated agreement of the parties with respect to the subject matter hereof, and no prior or contemporaneous promises, representations, warranties, inducements, or understandings between the parties pertaining to the subject matter hereof which are not contained herein shall be of any force or effect.

17. Default. In the event either party hereto defaults in any of the covenants or agreements contained herein, the defaulting party shall pay all costs and expenses, including a reasonable attorney's fee, incurred by the other party in enforcing its rights hereunder whether incurred through litigation or otherwise.
18. Time of Essence. The parties agree that time is of the essence in the performance of all duties herein.
19. Exhibits. Any exhibit(s) to this Agreement are incorporated herein by this reference, and failure to attach any such exhibit shall not affect the validity of this Agreement or of such exhibit. An unattached exhibit is available from the records of the parties.
20. Amendment. Any amendment or modifications of this Agreement shall be made in writing, signed by the parties, and attached hereto.
21. Extension. The Bond Agreement and security for completion of Improvements described in Paragraph 1 may be extended by written modification of this Agreement only.
22. Change of Address. It is the obligation of Developer to provide an updated address should it change during the pendency of this agreement.

IN WITNESS WHEREOF, the parties have caused this Agreement to be duly executed this \_\_\_\_ day of March, 2019.

---

By DB Development, LLC  
General Partner

---

City of Kuna, Idaho

By Joe Stear  
Mayor

(seal)

STATE OF IDAHO )  
 : SS  
County of Ada )

On this \_\_\_\_ day of March, 2019, before me \_\_\_\_\_, personally appeared \_\_\_\_\_ known or identified to me to be the person whose name is subscribed to the within instrument, and acknowledged to me that he/she executed the same as General Partner and on behalf of the DB DEVELOPMENT, LLC.

S  
E  
A  
L

\_\_\_\_\_  
Notary Public for \_\_\_\_\_  
My commission expires on \_\_\_\_\_

*Alloway Electric Co., Inc.**"Green Before Green Was Cool"*502 E. 45<sup>th</sup> St.

Boise, Idaho 83714

Tel. (208) 344-2508 Fax (208) 345-9844

[www.allowayelectric.com](http://www.allowayelectric.com)

Date: February 7, 2019

To: Highmark Development  
Attn: Justin Blackstock  
2228 W. Piazza St.  
Meridian, Idaho 83646  
Tel. (208) 871-0642 email [highmark.development@gmail.com](mailto:highmark.development@gmail.com)

Bid for Highmark Development to provide street lighting for Desert Hawk Subdivision #4 located in Kuna, Idaho. This work is per sheet 3.2 by B&A Engineers and last dated 08/21/2018. Scope of work to include the following:

- (1) Provide installation of (8 ea.) interior street lights consisting of black 25 ft. anchor base 4" x 4" square steel poles fitted with USSL A01 LED luminaires. These poles will be mounted on 24" diameter reinforced concrete bases poured flush with the ground.
- (2) Provide installation of (2 ea.) frontage street lights along W. Sunbeam St. consisting of black 30 ft. anchor base 5" x 5" square steel poles fitted with USSL A02 LED luminaires. These poles will be mounted on 24" diameter reinforced concrete bases poured flush with the ground.
- (2) Install a 125 amp 120/240 volt metered pedestal to power the frontage street lights. Extend a 2" pvc conduit underground from this pedestal to the Idaho Power source, including approximately 125 lineal ft. of trench, conduit, and backfill. Service entrance conductors to be supplied and installed by Idaho Power, not a part of this contract.
- (4) Provide approximately 1125 lineal ft. of trench, conduit, wire, and backfill, along with the required non-metered service junction boxes and pull boxes, to extend power to the above-mentioned street lights.
- (5) Roadway crossing sleeve required across S Tanami Ave near W. Sunbeam St. Sleeve to be provided by others; not a part of this contract.
- (6) Provide City of Kuna electrical permit.

Labor and Materials-\$44,875.00

Thank you for considering Alloway!

Joe Deaver



Signature: Please sign and return when accepted.

Excludes: SWPPP, IPCO connections fees.

This quote is valid until the end of business day 02/21/2019.



**Butte Fence, Inc.**  
2049 E. Wilson Lane  
Meridian, Idaho 83642  
(p) 208-884-0203  
(f) 208-884-8929

**Sales Order**

Order Number: 0068104  
Order Date: 11/6/2018

Salesperson: 0004 Chuck Elliott  
Customer Number: 10-DBDEV

**Sold To:**  
DB Development LLC  
2228 W. Piazza St.  
Meridian, ID 83646

**Ship To:**  
Desert Hawk #4  
apprx. 2922' of 6' vinyl  
white t&g

Phone:

Customer P.O.	Ship VIA	F.O.B.	Terms			
Desert Hawk #4			NET 30 DAYS			
Item Code		Unit	Ordered	Shipped	Price	Amount
PFVF	6' white t&g	EACH	2,922.00	0.00	15.10	44,122.20
PFCONCRETE	CONCRETE PER FOOT	EACH	2,922.00	0.00	0.90	2,629.80
VFPS.8W	5 X 5 X 8 WHITE POST	EA	494.00	0.00	0.00	0.00
/INSTALLATION	PROFESSIONAL INSTALLATION	FREET	2,922.00	0.00	5.65	16,509.30
VFC5W-SQ	5 WHITE FLAT POST CAP	EA	494.00	0.00	0.00	0.00
VFK6W-TG	6' WHITE T/G PRIV 6'OC 7/8 X 6 T/G	EACH	487.00	0.00	0.00	0.00

Net Order: 63,261.30  
Less Discount: 0.00  
Freight: 0.00  
Sales Tax: 0.00  
**Order Total: 63,261.30**

<b>Proposal</b> <b>POWER ENTERPRISES</b> 16131 Franklin Road Nampa, ID 83687 Phone (208) 461-4670 Fax (208) 461-4354						
PROPOSAL SUBMITTED TO:  Highmark Development			PHONE:  Justin Blackstock		DATE:  2/22/2019	
STREET:			JOB NAME/LOCATION:  Desert Hawk #4 Bonding Estimate			
CITY, STATE and ZIP CODE:			DATE OF PLANS: 4/18/2017 last revised 12/6/2018			
			ID Bureau of Occupational License #		RCE-616	
			State of ID Public Works License #		16668-AAA-4	
Item	Description	Quantity	Unit	Unit Cost	Sub Total	
1	Sprinkler System	1	ls	\$23,000.00	\$23,000.00	
2	Sleeving	1	ls	\$1,400.00	\$1,400.00	
3	Machine/Crew Grading	1	ls	\$2,797.00	\$2,797.00	
4	Sod	15,723	sf	\$0.43	\$6,760.89	
5	Bark Mulch	151	cy	\$92.00	\$13,892.00	
6	Trees: 2" Deciduous	24	ea	\$360.00	\$8,640.00	
7	Trees: 6-8' Conifer	23	ea	\$345.00	\$7,935.00	
8	Plants: 15 gallon	30	ea	\$175.00	\$5,250.00	
9	Plants: 5 gallon	99	ea	\$45.00	\$4,455.00	
10	Plants: 3 gallon	186	ea	\$36.00	\$6,696.00	
11	Plants: 1 gallon	274	ea	\$16.00	\$4,384.00	
12						
13						
14						
15						
16						
17						
18						
19						
20	Exclusions: Soil amendments, topsoil import, removal of existing trees and all site fixtures.					
21	Notes: 1. Topsoil from onsite stockpiles shall be placed and rough graded to 2/10 by site contractor.					
22						
					<b>Total</b>	<b>\$85,209.89</b>
<b>QUALIFICATIONS</b>						
If additional off-site topsoil is required, cost will be \$25.00 / CY						
Rough Grade within 2/10						
Bond Not Included, but available at 2.5%						
Plant material Warranted 1 year						
No Signage						
Adequate pressure and volume of water to be supplied by Developer/General Contractor/Owner						
All material is guaranteed to be as specified. All work to be completed in a workmanlike manner according to standard practices. Any alteration or deviation from above specifications involving extra costs will be executed only upon written orders, and will become an extra charge over and above the estimate. All agreements contingent upon strikes, accidents or delays beyond our control. Owner to carry fire, tornado and other necessary insurance. Our workers are fully covered by Workman's Compensation Insurance.				<b>Aaron Dressen</b> 208-948-0870		
				Note: This proposal may be withdrawn by us if not accepted within 30 days.		
				Signature		
				Signature _____		
Acceptance of Proposal—The above prices, specifications and conditions are satisfactory and are hereby accepted. You are authorized to do the work as specified. Payment will be made as outlined above.						
Date of Acceptance: _____						

**RESOLUTION NO. R16-2019  
CITY OF KUNA, IDAHO**

**A RESOLUTION OF THE CITY COUNCIL FOR KUNA, IDAHO APPROVING A PERFORMANCE BOND BY DB DEVELOPMENT, LLC, FOR THE CAZADOR SUBDIVISION NO. 1 FOR UNCOMPLETED WORK INCLUDING STREET LIGHTING, FENCING AND LANDSCAPING PURSUANT TO THE TERMS OF THIS RESOLUTION.**

**WHEREAS** Cazador Subdivision No. 1 exists as part of an approved preliminary plat; and

**WHEREAS** construction plans for Cazador Subdivision No. 1 were approved by the Kuna City Engineer on January 29, 2019; and

**WHEREAS** construction was commenced but not completed for certain items, per the approved plans; and

**WHEREAS** the street lighting, fencing and landscaping have not been completed for Cazador Subdivision No. 1 according to the approved construction plans and developer seeks to bond for the unfinished work; and

**WHEREAS** the street lighting completion has been estimated at forty-five thousand six hundred sixty-six dollars and zero cents (\$45,663.00) adding 25% for a total of fifty-seven thousand seventy-eight dollars and seventy-five cents (\$57,078.75); and

**WHEREAS** the fencing completion has been estimated at eighty-three thousand one hundred forty-four dollars and twenty-five cents (\$83,144.25) adding 25% for a total of one hundred three thousand nine hundred thirty dollars and thirty-one cents (\$103,930.31); and

**WHEREAS** the landscaping completion has been estimated at sixty thousand one hundred seventy-five dollars and sixty-five cents (\$60,175.65) adding 25% for a total of seventy-five thousand two hundred nineteen dollars and fifty-six cents (\$75,219.56); and

**WHEREAS** developer desires to record the final plat for Cazador Subdivision No. 1 prior to completion of construction; and

**WHEREAS** Kuna City Code 6-2-4 and 6-4-3 allows for and sets the conditions for recording a final plat prior to the completion of construction:

**BE IT HEREBY RESOLVED** by the Mayor and Council of the City of Kuna, Idaho, that the Kuna City Engineer is hereby authorized to accept an irrevocable standby Letter of Credit in lieu of construction for Cazador Subdivision No. 1 under the following terms and conditions:

1. All bids amounts submitted for unfinished construction are valid for the life of the Letter of Credit;

2. The Letter of Credit is irrevocable, is drawn upon an FDIC or FSLIC insured institution, is an institution with an office where presentment can be made within 50 miles of Kuna City Hall, the Letter of Credit is claimable up to 30 days prior to expiry and expiry is not more than one year from the date of issuance;
3. The face amount of the Letter of Credit is at least two hundred thirty-six thousand two hundred eighty-eight dollars and sixty-two cents (\$236,288.62);
4. No more than fifty percent of available permits can be claimed during the life of the Letter of Credit and if improvements are not completed within 120 days of issuance of the Letter of Credit, no further building permits can be issued.

**PASSED BY THE COUNCIL** of Kuna, Idaho this 5<sup>th</sup> day of March, 2019.

**APPROVED BY THE MAYOR** of Kuna, Idaho this 5<sup>th</sup> day of March, 2019.

\_\_\_\_\_  
Joe L. Stear, Mayor

ATTEST:

\_\_\_\_\_  
Chris Engels, City Clerk

**CITY OF KUNA IMPROVEMENT AGREEMENT (CASH BOND)**

THIS AGREEMENT is made by and between DB DEVELOPMENT, LLC, (hereinafter “Developer”); whose address is 2228 W Piazza St., Meridian, ID 83646, and CITY OF KUNA, a municipal corporation of the State of Idaho, (hereinafter “City”); whose address is Post Office Box 13, Kuna, Idaho 83634.

WHEREAS, Developer desires to record its final plat for Phase No. 1 of the development known as Cazador Subdivision, (“Development”) located in the City of Kuna; and

WHEREAS, City will not sign the final plat unless Developer promises to install and warrant certain Improvements as herein provided and security is provided for that promise as set forth herein.

NOW, THEREFORE, THE PARTIES HEREBY AGREE AS FOLLOWS:

1. Installation of Improvements. Developer agrees to complete and pay the total costs of all Improvements required by City and those specified in the following:
  - a. Approved Street Light Plan for Cazador Subdivision No. 1 and the associated Bid for Street Lights by Alloway Electric Co, Inc.,
  - b. Approved Fencing Plant for Cazador Subdivision No. 1 and the associated Bid for Fencing by Butte Fence, Inc.,
  - c. Approved Landscape Plan for Cazador Subdivision No. 1 and the associated Bid for Landscaping by Power Enterprises.

The bids for said Improvements are attached hereto as **Exhibit A**. The required Improvements are shown on the plans, drawings and specifications previously reviewed and approved by City in connection with the above described Development, and in accordance with the standards and specifications established by the City and adopted by the City Council.

2. Cash Deposit. Developer has executed and delivered to City cash, cashier’s check or wired funds (City to provide financial institution information upon execution of agreement) to the City’s trust account in the aggregate amount of two hundred thirty-six thousand two hundred eighty-eight dollars and sixty-two cents (\$236,288.62), for deposit with City in its accounts (the “Cash Deposit”), which includes:
  - a. The initial City Engineer or Public Works Director’s estimated cost of the remaining work shall, as determined, in part, from the detailed bids provided by the sub-divider's contractors in an amount, plus twenty-five (25) percent, for an amount of one hundred twenty-five (125) percent;
  - b. To that total, the following additional sums may be added upon the following considerations:

- i. Three (3) to ten (10) percent for inflation; ten (10) to fifteen (15) percent for the City's bidding disadvantage; and twelve (12) percent to twenty (20) percent for city project management as determined by the City Engineer or Public Works Director.
3. The Developer and City stipulate the amount to be a reasonable estimate, pursuant to Kuna City Code.
4. If construction of all financially pledged improvements are not completed within one hundred twenty (120) days following the date of recordation of the final plat, no further building permits shall be issued by the City until final completion of all improvements has occurred and the City has inspected and approved them. However, if the remaining improvements are not completed within the one hundred twenty-day period, through no fault of the Developer, the City Engineer or Public Works Director may grant a one-time, one hundred twenty-day (120) time extension. The determination of what may be considered a "no fault circumstance" shall be determined by the City Engineer or Public Works Director.
5. Refund or Withdrawal. City may withdraw funds from Cash Deposit if (1) Improvements are not completed as required by this Agreement within the time period specified in Paragraph 6, or if (2) Improvements are not installed strictly in accordance with Paragraph 1 and written notice of the deficiency has been given to Developer, who has failed to remedy the deficiency within ten (10) days after the notice is sent. In said event, City may withdraw funds from Cash Deposit both (1) those amounts necessary to either complete Improvements as required herein or alter or repair Improvements to conform to the requirements hereof, and (2) City's cost of administration incurred in obtaining Cash Deposit, including attorney's fees and court costs, which shall be deducted from any Cash Deposit. If the amount of Cash Deposit is inadequate to pay the cost of the completion of Improvements according to City's standards or specifications for whatever reason, including previous reductions, Developer shall be responsible for the deficiency and no further building permits shall be issued in the subdivision or development until Improvements are completed or, with City Council approval, a new, satisfactory security has been executed and delivered to City or other satisfactory arrangements have been made to insure completion of the remaining improvements.
6. Preliminary Release. At the time herein provided, but no later than at the time of final inspection and acceptance of all Improvements by City, City will authorize release of all funds comprising Cash Deposit. The release provided for in this paragraph shall occur when City certifies that Improvements are complete, which shall be when Improvements have been installed as required and fully inspected and approved by City, and after as-built drawings have been supplied as required.
7. Non-Release of Developer's Obligations. It is understood and agreed between the parties that the establishment and availability to City of Cash Deposit as herein provided, and any withdrawals there from by City shall not constitute a waiver or estoppel against City and shall not release or relieve Developer from its obligation to install and fully pay for Improvements as required in Paragraph 1 above, and the right of City to withdraw from

Cash Deposit shall not affect any rights and remedies of City against Developer for breach of any covenant herein, including the covenants of Paragraph 1 of this Agreement. Further, Developer agrees that if City withdraws from Cash Deposit and performs or causes to be performed the installation or warranty work required of Developer hereunder, then any and all costs incurred by City in so doing which are not collected by City by withdrawing from Cash Deposit shall be paid by Developer, including administrative, engineering, legal, labor and materials and other procurement fees and costs.

8. Upon satisfaction of this Agreement, Developer shall provide the City with its financial institution information including account wire transfer information.
9. Binding Effect and Assignment. This Agreement shall be binding upon, and inure to the benefit of, the heirs, officers, agents, legal representatives, successors and assigns of the parties hereto. No party shall assign or transfer any rights under this Agreement without the prior written consent of the other first obtained, which consent shall not be unreasonably withheld.
10. Notices. Any notice required or desired to be given hereunder as shall be deemed sufficient if sent by certified mail, postage prepaid, addressed to the respective parties at the addresses shown in the preamble.
11. Severability. Should any portion of this Agreement for any reason be declared invalid or unenforceable, the invalidity of such portion shall not affect the validity of any of the remaining portions and the same shall be deemed in full force and effect as if this Agreement had been executed with the invalid portions eliminated.
12. Governing Law. This Agreement and the performances hereunder shall be governed by the laws of the State of Idaho.
13. Counterparts. The fact that the parties hereto execute multiple but identical counterparts of this Agreement shall not affect the validity or efficacy of their execution, and such counterparts, taken together, shall constitute one and the same instrument, and each counterpart shall be deemed an original.
14. Waiver. No waiver of any of the provisions of this Agreement shall operate as a waiver of any other provision, regardless of any similarity that may exist between such provisions, nor shall a waiver in one instance operate as a waiver in any future event. No waiver shall be binding unless executed in writing by the waiving party.
15. Captions. The captions preceding the paragraphs of this Agreement are for convenience only and shall not affect the interpretation of any provision herein.
16. Entire Agreement. This Agreement, together with its exhibits and the approved plans and specifications referred to, contains the entire and integrated agreement of the parties with respect to the subject matter hereof, and no prior or contemporaneous promises, representations, warranties, inducements, or understandings between the parties pertaining to the subject matter hereof which are not contained herein shall be of any force or effect.

17. Default. In the event either party hereto defaults in any of the covenants or agreements contained herein, the defaulting party shall pay all costs and expenses, including a reasonable attorney's fee, incurred by the other party in enforcing its rights hereunder whether incurred through litigation or otherwise.
18. Time of Essence. The parties agree that time is of the essence in the performance of all duties herein.
19. Exhibits. Any exhibit(s) to this Agreement are incorporated herein by this reference, and failure to attach any such exhibit shall not affect the validity of this Agreement or of such exhibit. An unattached exhibit is available from the records of the parties.
20. Amendment. Any amendment or modifications of this Agreement shall be made in writing, signed by the parties, and attached hereto.
21. Extension. The Bond Agreement and security for completion of Improvements described in Paragraph 1 may be extended by written modification of this Agreement only.
22. Change of Address. It is the obligation of Developer to provide an updated address should it change during the pendency of this agreement.

IN WITNESS WHEREOF, the parties have caused this Agreement to be duly executed this \_\_\_\_\_ day of March, 2019.

\_\_\_\_\_  
By DB Development, LLC  
General Partner

\_\_\_\_\_  
City of Kuna, Idaho

By Joe Stear  
Mayor

(seal)

STATE OF IDAHO )  
 : SS  
County of Ada )

On this \_\_\_\_ day of March, 2019, before me \_\_\_\_\_, personally appeared \_\_\_\_\_ known or identified to me to be the person whose name is subscribed to the within instrument, and acknowledged to me that he/she executed the same as General Partner and on behalf of the DB DEVELOPMENT, LLC.

S  
E  
A  
L

\_\_\_\_\_  
Notary Public for \_\_\_\_\_  
My commission expires on \_\_\_\_\_

*Alloway Electric Co., Inc.**"Green Before Green Was Cool"*502 E. 45<sup>th</sup> St.

Boise, Idaho 83714

Tel. (208) 344-2508 Fax (208) 345-9844

[www.allowayelectric.com](http://www.allowayelectric.com)

To: *DB Development, LLC*  
~~Highmark Development~~  
 Attn: Justin Blackstock  
 2228 W. Piazza St.  
 Meridian, Idaho 83646  
 Tel. 871-0642 email [highmark.development@gmail.com](mailto:highmark.development@gmail.com)

Date: March 13, 2018

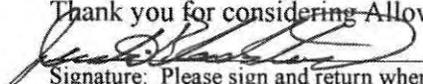
Bid for Highmark Development to provide street lighting for Cazador #1 Subdivision located in Kuna, Idaho. This work is per the drawings provided by KM Engineering and dated 2/23/2018. Scope of work to include the following:

- (1) Provide installation of (9 ea.) interior street lights consisting of 25 ft. anchor base 4" x 4" square steel bronze poles fitted with 50 watt class 2-Bar LED luminaires. These poles will be mounted on 24" diameter reinforced concrete bases poured flush with the ground.
- (2) Provide installation of (3 ea.) frontage street lights along N. Ten Mile Rd. consisting of 25 ft. anchor base 4" x 4" square steel bronze poles fitted with 90 watt class 4-Bar LED luminaires. These poles will be mounted on 24" diameter reinforced concrete bases poured flush with the ground.
- (3) Install a 100 amp 120/240 volt metered pedestal along the frontage of N. Ten Mile Rd. to power the frontage street lights. Extend a 2" pvc conduit underground from this pedestal to the Idaho Power source, including approximately 110 lineal ft. of trench, conduit, and backfill. Service entrance conductors to be supplied and installed by Idaho Power, not a part of this contract.
- (4) Provide approximately 1100 lineal ft. of trench, conduit, wire, and backfill, along with the required non-metered service junction boxes and pull boxes, to extend power to the above mentioned street lights. Sleeving across roadway (N. Sunset Farm Ave.) to be provided by others.
- (5) Provide City of Kuna electrical permit for these facilities.

Labor and Materials-\$45,663.00

Thank you for considering Alloway!

Michael L. Johnson

  
 Signature: Please sign and return when accepted.

This quote is valid until the end of the business day 04/05/2018.



# Butte Fence, Inc.

2049 E. Wilson Lane  
Meridian, Idaho 83642  
(p) 208-884-0203  
(f) 208-884-8929

## Quote

Order Number: 0069740  
Order Date: 3/17/2018

Salesperson: 0004 Chuck Elliott  
Customer Number: 10-DBDEV

Sold To:  
DB Development LLC  
2228 W Piazza  
Meridian, ID 83646

Ship To:  
Cazador  
apprx. 1351' of 6' t&g vinyl  
apprx. 2048' of t&g w/lattice

Phone:

Customer P.O.	Ship VIA	F.O.B.	Terms			
Cazador #2			No Terms			
Item Code		Unit	Ordered	Shipped	Price	Amount
PFVF	6' t&g sand	EACH	1,351.00	0.00	16.00	21,616.00
PFVF	6' t&g w/lattice sand	EACH	2,048.00	0.00	20.50	41,984.00
/INSTALLATION	installation	FEET	3,399.00	0.00	5.75	19,544.25
	installation					
VFC5S-SQ	5 SAND FLAT POST CAP	EACH	570.00	0.00	0.00	0.00
VFP5.8S	5 X 5 X 8 SAND POST	EACH	570.00	0.00	0.00	0.00
VFK6S6-TG	6' SAND T/G PRIV 6"OC	EACH	225.00	0.00	0.00	0.00
VFK6S6-TG-LAT	6' SAND T/G W/ LAT 6"OC	EACH	342.00	0.00	0.00	0.00

Net Order: 83,144.25  
Less Discount: 0.00  
Freight: 0.00  
Sales Tax: 0.00  
Order Total: 83,144.25

<b>Proposal</b> <b>POWER ENTERPRISES</b> 16131 Franklin Road Nampa, ID 83687 Phone (208) 461-4670 Fax (208) 461-4354						
<b>PROPOSAL SUBMITTED TO:</b> Justin Blackstock			<b>PHONE:</b>		<b>DATE:</b> 3/19/2018 revised 5/31/2018	
<b>STREET:</b> <i>DB Development, LLC</i> highmark.development@gmail.com			<b>JOB NAME/LOCATION:</b> Cazador 1			
<b>CITY, STATE and ZIP CODE:</b>			<b>DATE OF PLANS:</b> 2/21/18 no revisions			
			<b>ID Bureau of Occupational License #</b>		RCE-616	
			<b>State of ID Public Works License #</b>		16668-AAA-4	
Item	Description	Quantity	Unit	Unit Cost	Sub Total	
1	Sprinkler System	1	ls	\$16,722.00	\$16,722.00	
2	Sleeving	1	ls	\$785.00	\$785.00	
3	Machine/Crew Grading	1	ls	\$3,952.00	\$3,952.00	
4	Kentucky Blue Grass Sod	14,227	sf	\$0.39	\$5,548.53	
5	Hydroseed Native Seed if needed	22,784	sf	\$0.18	\$4,101.12	
6	Bark Mulch	26	cy	\$82.00	\$2,132.00	
7	Trees: 2" Deciduous (substitutions may be req'd)	38	ea	\$340.00	\$12,920.00	
8	Trees: 6-8' Conifer (substitutions may be req'd)	31	ea	\$295.00	\$9,145.00	
9	Trees: 10-12' Conifer (substitutions may be req'd)	0	ea	\$520.00	\$0.00	
10	Plants: 5 gallon	0	ea	\$42.00	\$0.00	
11	Plants: 2 gallon	125	ea	\$28.00	\$3,500.00	
12	Plants: 1 gallon	25	ea	\$14.00	\$350.00	
13	Perma Bark (3' tree ring) no weed fabric	12	cy	\$85.00	\$1,020.00	
14						
15						
16	<b>Exclusions:</b> Sidewalks and pathways, concrete, fencing, soil amendments, and imported topsoil.					
17	<b>Notes:</b> 1. Topsoil from onsite stockpiles shall be placed and rough graded to 2/10 by site contractor.					
18						
					<b>Total</b>	<b>\$60,175.65</b>
<b>QUALIFICATIONS</b>						
If additional off-site topsoil is required, cost will be \$25.00 / CY						
Rough Grade within 2/10						
Bond not included, but available at 1.5%						
Plant material warranted 1 year						
No signage						
Adequate pressure and volume of water to be supplied by Developer/General Contractor/Owner						
All material is guaranteed to be as specified. All work to be completed in a workmanlike manner according to standard practices. Any alteration or deviation from above specifications involving extra costs will be executed only upon written orders, and will become an extra charge over and above the estimate. All agreements contingent upon strikes, accidents or delays beyond our control. Owner to carry fire, tornado and other necessary insurance. Our workers are fully covered by Workman's Compensation Insurance.				<b>Aaron Dressen</b> 208-949-0870		
				Note: This proposal may be withdrawn by us if not accepted within 30 days.		
<b>Acceptance of Proposal</b> —The above prices, specifications and conditions are satisfactory and are hereby accepted. You are authorized to do the work as specified. Payment will be made as outlined above.				Signature  Signature _____		
Date of Acceptance: _____						

5/31/2018

proposal

**RESOLUTION NO. R17-2019  
CITY OF KUNA, IDAHO**

**A RESOLUTION OF THE CITY OF KUNA, IDAHO APPROVING THE PARK USE AGREEMENT BY AND BETWEEN THE CITY OF KUNA, IDAHO AND THE KUNA POLICE ACTIVITIES LEAGUE (KPAL) AT ARBOR RIDGE PARK, WINCHESTER PARK, FARM ESTATES PARK AND PURSUANT TO THE TERMS OF THE AGREEMENT; AND AUTHORIZING THE MAYOR TO EXECUTE THE AGREEMENT.**

**BE IT HEREBY RESOLVED** by the Mayor and Council of the City of Kuna, Idaho as follows:

Section 1. The Park Use Agreement by and between the City of Kuna, Idaho and the Kuna Police Activities League, in substantially the form as attached hereto as **EXHIBIT A** is hereby approved.

Section 2. The Mayor of the City of Kuna, Idaho is hereby authorized to execute the Agreement, and the City Clerk is hereby authorized to attest to said execution as so authorized and approved for on behalf of the City of Kuna, Idaho.

**PASSED BY THE COUNCIL** of Kuna, Idaho this 5<sup>th</sup> day of March, 2019.

**APPROVED BY THE MAYOR** of Kuna, Idaho this 5<sup>th</sup> day of March, 2019.

\_\_\_\_\_  
Joe L. Stear, Mayor

ATTEST:

\_\_\_\_\_  
Chris Engels, City Clerk

**PARK USE AGREEMENT  
CITY OF KUNA, IDAHO AND KUNA POLICE ACTIVITIES LEAGUE  
ARBOR RIDGE CITY PARK, WINCHESTER CITY PARK, AND FARM ESTATES  
CITY PARK**

This Park Use Agreement (AGREEMENT) is entered into by and between the City of Kuna, Idaho, (CITY), an Idaho municipal corporation and the Kuna Police Activities League, Inc. (KPAL), an Idaho non-profit corporation, jointly referred to herein as the PARTIES.

WHEREAS, CITY owns and maintains Arbor Ridge City Park (PARK), Winchester City Park (PARK) and Farm Estates City Park (PARK) located at Kuna, Idaho; and

WHEREAS, the PARK has certain improvements including football and soccer fields; and

WHEREAS, KPAL desires to use the PARK for its soccer, lacrosse, and football clinics, practices, and games during specified times of the year; and

WHEREAS, CITY grants KPAL permission to use the PARK in exchange for certain contributions including maintenance, care, improvements, and sanitation at the PARK.

NOW, THEREFORE, in consideration of the above recitals, which recitals are contractual in nature, the mutual covenants herein contained and such other and further consideration as is hereby acknowledged, the parties agree as follows:

1. USE: KPAL agrees to use the PARK for the sole purpose of operating the Kuna Police Athletic League, Inc. program to provide athletic opportunities for the children of Kuna, Idaho.
2. RESPONSIBILITIES:

The CITY Will:

- 2.1. Have the PARK fields reserved for KPAL's exclusive use according to its Reserved Schedule, on file with the City Parks and Recreation Department; all other groups may use the fields at all other times. Other groups wishing to use the fields must contact the City Parks and Recreation Department to ensure fields are available and register for their use.
- 2.2. Maintain the fields and parking lot including mowing, over-seeding, and irrigating during the schedule set by the CITY. Maintain the irrigation system.

KPAL Will:

- 2.3. Run and operate the soccer, lacrosse and football programs, including practices, games, and clinics.
- 2.4. Provide the City Parks and Recreation Department with a written schedule (Reserved Schedule) at least thirty (30) days prior to the commencement of the sport season,

including practices. If KPAL fails to provide the Reserved Schedule as provided for herein, the fields are first come, first serve.

- 2.5. Hire and staff the umpires.
- 2.6. Ensure that the fields are in presentable and playable condition prior to each use.
- 2.7. Chalk or paint the fields.
- 2.8. Provide trashcans during KPAL events, practices, and games and keep the area clean.
- 2.9. Provide portable bathrooms if needed during the season(s). The portable bathrooms may be locked while not in use by KPAL.
3. USAGE FEE: There will be no usage fee.
4. TERM: The term of this AGREEMENT shall be February 1, 2019 to January 31, 2019. The PARTIES may negotiate an extension of this AGREEMENT for additional one (1) year terms.
5. INSPECTION: KPAL has inspected and hereby accepts the PARK in the condition at the beginning of this AGREEMENT.
6. ALTERATIONS: KPAL shall not make any renovations, alterations, additions, or install any fixtures, equipment, fencing, etc., without the express written consent of CITY. All fixtures and improvements shall become the property of CITY at no cost to the City at termination or expiration of this AGREEMENT.
7. OTHER REQUIREMENTS: KPAL shall maintain oversight and control of the participants in its program. Littering, vandalizing, destroying or otherwise damaging CITY property is prohibited.
8. COMPLIANCE WITH GOVERNING LAW: Each PARTY shall comply with all federal, state, and local laws, rules, and regulations now in force or which may hereafter be in force pertaining to the use of the PARK.
9. INDEMNIFICATION: KPAL shall indemnify, hold harmless, and defend the CITY and its elected or appointed officials, employees, and agents from and against all liabilities, claims, suits, losses, and expenses, including reasonable attorney's fees and court costs, arising out of or in the course of the use contemplated by this AGREEMENT that is caused in whole or in part by any act or omission of KPAL, any act or omission of anyone directly or indirectly employed by KPAL, or any act or omission for which KPAL may be liable, whether or not it is caused in whole or in part by the party indemnified.
10. INSURANCE: KPAL shall, at all times, obtain, pay for, and maintain insurance for the coverages and amounts of coverage not less than those set forth as follows:
  - 10.1 Worker's Compensation – in compliance with the state and federal laws, if required.

- 10.2 Comprehensive Premises Liability Policy – a minimum coverage of \$1,000,000.00 including the following coverage:
- 10.2.1 Premises and operations; and
  - 10.2.2 Broad Form Commercial General Liability Endorsement to include blanket contractual liability (specifically covering but not limited to the contractual obligations assumed by the organization; and
  - 10.2.3 Personal injury (with employment and contractual exclusions deleted); and
  - 10.2.4 Broad Form Property Damages coverage.
- 10.3 KPAL shall provide to CITY original Certificates of Insurance satisfactory to evidence such coverages before operations pursuant to this AGREEMENT commence.
- 10.4 CITY shall be named as an additional insured on all policies related to this Agreement, excluding Workers Compensation. Such policies shall provide that there shall be no termination, non-renewal, modification, or expiration of such coverage without forty-five (45) calendar days written notice to the CITY.
- 10.5 All insurance shall be written with a company having an A.M. Best rating of at least the “A” category and size categories of VII.
11. UTILITIES: CITY will provide general field maintenance, including lawn mowing, fertilizing, seeding, irrigation and irrigation system maintenance. Any other utility, including electricity shall be provided by KPAL.
12. RUBBISH: KPAL shall be responsible for excess garbage or refuse removal during its use of the PARK.
13. DAMAGE BY FIRE OR OTHER CASUALTY: If the PARK, or any part thereof is damaged by fire or other casualty to such an extent that it is rendered unusable, in whole or in part, the CITY shall have the option to terminate this AGREEMENT, or repair the premises. KPAL shall have no claim for any loss of use during the period that the PARK is being repaired.
14. NOTICES: Any notice required or intended to be given to the other PARTY under the terms of this AGREEMENT shall be in writing and shall be deemed to be duly delivered the earlier of (a) actual receipt by personal delivery to the representative (as defined herein), or in lieu of personal service, (b) by way of private courier, such as Federal Express or United Parcel Service addressed to the appropriate party and address as set forth herein, or in lieu of private courier, (c) U.S. Mail, registered, postage pre-paid, return receipt requested.
- 14.1. CITY notice shall be sent to:

City Clerk for the City of Kuna, Idaho  
Post Office Box 13  
Kuna, Idaho 83634

14.2. KPAL notice shall be sent to:

Registered Agent  
Kuna Police Activities League, Inc.  
1450 W. Boise Street  
Kuna, Idaho 83634

15. TERMINATION: Either Party reserves the right to terminate this AGREEMENT for cause (breach of any provision of this AGREEMENT) by providing written notice of its election to terminate to the other Party at least sixty (60) days prior to the date of termination.

16. ASSIGNMENT: This AGREEMENT is not assignable without the written consent of CITY.

17. WAIVER: No failure of the CITY to enforce any term hereof shall be deemed to be a waiver.

18. ENTIRE AGREEMENT: This AGREEMENT contains the entire agreement between the parties and may be modified only by an addendum to this AGREEMENT or by a new AGREEMENT in writing, signed by CITY and KPAL.

19. ATTORNEY'S FEES: If either PARTY is required to commence any proceeding or legal action to enforce or interpret any term, covenant or condition of the AGREEMENT, the prevailing party in such proceeding or action shall be entitled to recover from the other PARTY its reasonable attorney's fees and legal costs.

In witness whereof, the parties hereto have set their hands and seals on the day and year written below.

CITY OF KUNA, IDAHO

KUNA POLICE ACTIVITIES  
LEAGUE, INC.

Dated this \_\_\_\_ day of \_\_\_\_\_, 2019.

Dated this <sup>26</sup> day of February, 2019.

BY: \_\_\_\_\_  
Joe L. Stear, Mayor

BY:   
Erica Kallin, President

ATTEST:

Dated this \_\_\_\_ day of \_\_\_\_\_, 2019.

BY: \_\_\_\_\_  
Chris Engels, City Clerk

**RESOLUTION NO. R18-2019**

**A RESOLUTION OF THE CITY COUNCIL OF KUNA, IDAHO AUTHORIZING THE MAYOR TO EXECUTE THE CONSENT TO USE AGREEMENT WITH THE UNITED STATES DEPARTMENT OF THE INTERIOR BUREAU OF RECLAMATION.**

**BE IT HEREBY RESOLVED** by the Mayor and Council of the City of Kuna, Idaho that the Mayor of the City is hereby authorized to execute the Consent to Use Agreement with the United States Department of the Interior Bureau of Reclamation.

**PASSED BY THE COUNCIL** of Kuna, Idaho this 5<sup>th</sup> day of March, 2019.

**APPROVED BY THE MAYOR** of Kuna, Idaho this 5<sup>th</sup> day of March, 2019.

\_\_\_\_\_  
Joe L. Stear, Mayor

ATTEST:

\_\_\_\_\_  
Chris Engels, City Clerk

RETURN TO:  
Bureau of Reclamation  
Attn: MSF 6125  
230 Collins Road  
Boise, ID 83702

Contract No. 8-07-11-L4093

**UNITED STATES  
DEPARTMENT OF THE INTERIOR  
BUREAU OF RECLAMATION  
Arrowrock Division, Boise Project, Idaho**

**CONSENT TO USE**

**THIS CONSENT TO USE**, hereinafter referred to as Consent, made the \_\_\_\_\_ day of \_\_\_\_\_, 2019, pursuant to the Act of Congress June 17, 1902 (32 Stat. 388; 43 U.S.C. §391), and acts amendatory thereof or supplementary thereto, collectively referred to as the Federal Reclamation Laws, specifically, Section 10 of the Reclamation Project Act of 1939 (53 Stat. 1196), between the **UNITED STATES OF AMERICA**, hereinafter called the **United States**, by and through the Bureau of Reclamation, Department of the Interior, hereinafter referred to as Reclamation, and **CITY OF KUNA**, hereinafter referred to as **Consentee**.

WITNESSETH, THAT:

WHEREAS, in connection with the Arrowrock Division, Boise Project, Idaho, the United States, pursuant to Federal Reclamation Laws, constructed, operates, and maintains certain irrigation works hereinafter referred to as the New York Canal; and

WHEREAS, said New York Canal, is operated and maintained by the Boise Project Board of Control, hereinafter referred to as the Board, under a repayment contract with the United States; and

WHEREAS, the Consentee desires the United States to give consent to construct, install, operate and maintain two pressurized sewer force main pipelines and a water main pipeline over and across said New York Canal; and

WHEREAS, the proposed use has been reviewed and determined to be compatible with project purposes.

NOW, THEREFORE, in consideration of the mutual covenants and stipulations hereinafter stated, the parties hereto do mutually agree as follows:

1. **CONSENT AND LEGAL**

Subject to the terms and conditions of this Consent, the United States does hereby consent to the Consentee, or its agents, installation, operation and maintenance of a 4 inch PVC pressurized sewer force main pipeline in a 16 inch steel sleeve, a 12 inch PVC pressurized force main pipeline in a 20 inch steel sleeve, encased in flowable fill 25 feet from the end of the casing pipes; a 12 inch PVC water main pipeline in a 20 inch steel sleeve, encased in flowable fill 25 feet from the end of the casing pipe, over and across the New York Canal. Sewer lines are located on the south side of E. Kuna Road and the water line is located on

the north side of E. Kuna Road and east of S. Eagle Road, in a portion of the NW¼ of Section 28, Township 2 North, Range 1 East of the Boise Meridian, situated in Ada County, Idaho, and shall be installed in the manner and at the location shown on the attached Exhibits A, B, C, D, E, F, G, H, I, J, K, and L, by this reference made a part hereof.

2. SUBJECT TO FEE OWNER APPROVAL

Unless the Consentee is the underlying fee owner, this Consent is subject to Consentee securing approval of the underlying fee owner to cross or use the identified portion of Reclamation's right of way. By executing this document the Consentee acknowledges this notice and assumes all liability, if any, in this regard.

3. FEES

The Consentee has provided an application to obtain this Consent in conformance with Title 43 of the Code of Federal Regulations, Part 429 and a payment of \$100.00 for the application fee and \$573.00 for administrative fees is hereby acknowledged.

4. TERM

The term of this Consent is to be perpetual from the date of this document, subject to the terms and conditions herein.

5. SPECIAL STIPULATIONS

Consentee agrees that the construction, installation, operation and maintenance (O&M) shall be conducted as follows:

a. The Consentee shall notify the Board at least seven (7) days prior to beginning construction work.

b. Consentee's structures shall be constructed, operated and maintained by the Consentee without cost to the United States or its assigns, and in such a manner as to cause no interference with the normal operation of project facilities. All construction, reconstruction and maintenance work performed by the Consentee upon the premises of the United States shall be undertaken only at times, according to plans, and in a manner satisfactory to the authorized official.

c. All backfill placed in the facility or its embankments shall be placed and compacted to the satisfaction of the authorized official. The facility and appurtenant structures shall be restored by the Consentee to a condition at least as good as before the work was accomplished.

d. This Consent is limited to the crossing, construction, installation, operation and maintenance of a 4 inch PVC pressurized sewer force main pipeline in a 16 inch steel sleeve, a 12 inch PVC pressurized force main pipeline in a 20 inch steel sleeve, encased in flowable fill 25 feet from the end of the casing pipes; a 12 inch PVC water main pipeline in a 20 inch steel sleeve, encased in flowable fill 25 feet from the end of the casing pipe, over and across the New York Canal. Sewer lines are located on the south side of E. Kuna Road and the water line is located on the north side of E. Kuna Road and east of S. Eagle Road, in a portion of the NW¼ of Section 28, Township 2 North, Range 1 East of the Boise Meridian, situated in Ada County, Idaho. In accordance with Idaho Code section 42-1102, et seq, additional encroachments onto the

right of way by public or private roads, utilities, fences, gates, pipelines, structures, or other construction or placement of objects, need specific written permission from the United States or its successor or assign. Encroachments of any kind placed in such right of way without express written permission shall be removed at the expense of the person or entity causing or permitting such encroachment upon the request of the owner of the right of way.

e. Any alteration in design of the facilities to be constructed, or any alteration, modification, or reconstruction of facilities, once the original work is accepted by Reclamation, its successors or assigns, must be approved in writing prior to commencing any work on facilities in the area subject to this Consent.

6. UNRESTRICTED ACCESS

The United States officers, agents, and employees shall at all times have unrestricted access and ingress to, passage over, and egress from all of said lands, to make investigations of all kinds, dig test pits and drill test holes, to survey for and construct reclamation and irrigation works and other structures incident to Federal Reclamation Projects, or for any purpose whatsoever. Reclamation will make every reasonable effort to keep damages to a minimum.

7. HOLD HARMLESS

The Consentee agrees to indemnify the United States for, and hold the United States and all of its representatives harmless from, all damages resulting from suits, actions, or claims of any character brought on account of any injury to any person or property arising out of any act, omission, neglect, or misconduct in the manner or method of performing any construction, care, operation, maintenance, supervision, examination, inspection, or other activities of the Consentee.

8. PROTECTION OF UNITED STATES INTERESTS

The Consentee shall construct, operate, and maintain its structures in a good workmanlike manner, as determined by Reclamation, to ensure that the authorized uses are compatible with the Reclamation purposes for which the land rights were acquired, and to protect the interests of the United States. The Consentee shall insure compliance with all laws, regulations, and orders of the United States, and any other public authority affecting such works.

9. TERMINATION

This Consent, a use authorization, will terminate and all rights of the Consentee hereunder will cease, and the Consentee will quietly deliver to the United States possession of the premises in like condition as when taken, reasonable wear and damage by the elements excepted:

a. At the end of the term of this Consent.

b. Reclamation may, at any time and at no cost or liability to the United States, terminate this Consent if Consentee fails to comply with any of the terms and conditions hereof, or upon mutual agreement of all parties.

c. The United States, acting through Reclamation, Department of the Interior, reserves rights to construct, operate, and maintain public works now or hereafter authorized by the Congress without liability for termination of the use authorization or other damage to the Consentee's activities or facilities.

d. Reclamation may, at any time and at no cost or liability to the United States, terminate this Consent in the event of a natural disaster, a national emergency, a need arising from security requirements, or an immediate and overriding threat to public health and safety.

e. Reclamation may, at any time and at no cost or liability to the United States, terminate this Consent, for activities other than existing authorized private exclusive recreational or residential use as defined under 43 CFR § 429.2 if Reclamation determines that any of the following apply:

- i. The use has become incompatible with authorized project purposes, project operations, safety, and security;
- ii. A higher public use is identified through a public process described at 43 CFR § 429.32(a)(1); or
- iii. Termination is necessary for operational needs of the project.

f. Reclamation may, at any time and at no cost or liability to the United States, terminate this Consent if Reclamation determines that the Consentee has failed to use the use authorization for its intended purpose. Further, failure to construct within the timeframe specified in the terms of the use authorization may constitute a presumption of abandonment of the requested use and cause termination of the use authorization.

g. Reclamation may, at any time and at no cost or liability to the United States, terminate this Consent if the Consentee fails to comply with all applicable Federal, State, and local laws, regulations, ordinances, or terms and conditions of any use authorization, or to obtain any required permits or authorizations.

## 10. NOTICES

a. Notices served under this Consent shall be in writing and may be delivered by personal delivery; via recognized delivery services such as United Parcel Services (UPS) or Federal Express (FedEx); or by deposit in the United States Mail, postage prepaid, as certified mail, return receipt requested, and addressed as follows:

UNITED STATES	CONSENTEE	BOARD
Deputy Area Manager Bureau of Reclamation 230 Collins Road Boise, ID 83702 (208) 383-2200	Public Works Director City of Kuna P.O. Box 13 Kuna, ID 83634 (208) 922-5546	Manager Boise Project Board of Control 2465 Overland Road Boise, ID 83705 (208) 344-1141

b. Any notice delivered by personal delivery shall be deemed received by the addressee upon actual delivery. Any notice delivered by certified mail or delivery service shall be deemed received by the addressee on the third business day after deposit. The addresses to which notices are to be delivered may be changed by giving notice of such change in accordance with this paragraph. This paragraph shall apply where notice is required under this Consent, and no specific requirements are set forth. Where this Consent provides for a specific notice in a different manner, the more specific requirements shall prevail.

c. The parties hereby designate the Deputy Area Manager of the Snake River Area Office, Bureau of Reclamation; Manager of the Boise Project Board of Control; and the Public Works Director for the City of Kuna, as their respective authorized representative for this Consent. These individuals shall have authority to take any action allowed or required under this Consent, on behalf of their employer. The parties may change their designated representatives at any time by giving notice of such change in accordance with this Article.

11. INCREASED COSTS

If the construction, operation, or maintenance of any or all of such structures and facilities of the United States across, over, under, or upon said right of way should be made more expensive by reason of the existence of improvements or works of Consentee thereon, such additional expense is to be estimated by the Reclamation, whose estimate is to be final and binding upon the parties hereto. Consentee will make payment thereof to the United States or any of its successors or assigns within (30) days after demand is made upon Consentee for payment of any such sums. As an alternative to payment, Consentee, at its sole cost and expense and within the time limits established by the United States, may remove or adapt its facilities constructed and operated by it on said right of way to accommodate the aforementioned structures and facilities of the United States. Consentee shall bear any costs incurred by the United States occasioned by the failure of Consentee to remove or adapt its facilities within the time limits specified.

12. REMOVAL OF STRUCTURES

Upon expiration, termination, or revocation of the Consent, the Consentee shall remove all structures, equipment, or other improvements made by it from the premises at no cost to the United States. Upon failure to remove any such improvements within sixty (60) days of expiration, termination, or revocation, any remaining improvements shall, at the option of the United States, be removed or become the property of the United States. The Consentee shall pay all the expenses of the United States, or its assigns or successors, related to removal of such improvements.

13. DISCOVERY OF CULTURAL RESOURCES

The Consentee shall immediately provide an oral notification to Reclamation of the discovery of any and all antiquities or other objects of archaeological, cultural, historic, or scientific interest. The Consentee shall follow up with a written report of their finding(s) to Reclamation within forty-eight (48) hours. Objects under consideration include, but are not limited to, historic or prehistoric ruins, human remains, funerary objects, and artifacts discovered as a result of activities under this authorization. The Consentee shall immediately cease the activity in the area of the discovery, make a reasonable effort to protect such discovery, and wait for written approval from Reclamation before resuming the activity. Protective and mitigative measures specified by Reclamation shall be the responsibility of the Consentee.

14. HAZARDOUS MATERIALS

a. The Consentee may not allow contamination or pollution of Federal lands, waters, or facilities. The Consentee has the responsibility for care, operation, and maintenance by its employees or agents and shall take reasonable precautions to prevent such contamination or pollution by third parties. Substances causing contamination or pollution shall include, but are not limited to, hazardous materials, thermal pollution, refuse, garbage, sewage effluent, industrial waste, petroleum products, mine tailings, mineral salts, misused pesticides, pesticide containers, or any other pollutants.

b. The Consentee shall comply with all applicable Federal, State, and local laws and regulations, and Reclamation policies and directives and standards, existing or hereafter enacted or promulgated, concerning any hazardous material that will be used, produced, transported, stored, or disposed of, on or, in Federal lands, waters, or facilities.

c. "Hazardous material" means any substance, pollutant, or contaminant listed as hazardous under the Comprehensive Environmental Response, Compensation, and Liability Act of 1980, as amended, 42 U.S.C. § 9601, et seq., and the regulations promulgated pursuant to that Act.

d. Upon discovery of any event which may or does result in contamination or pollution of Federal lands, waters, or facilities, the Consentee shall initiate any necessary emergency measures to protect health, safety, and the environment and shall report such discovery with full details of the actions taken to Reclamation. Reporting may be within a reasonable time period. A reasonable time period means: within twenty-four (24) hours of the time of discovery if it is an emergency, or by the first working day if it is a non-emergency. An emergency is any situation that requires immediate action to reduce or avoid endangering public health and safety or the environment.

e. Violation of any of the provisions of this Article, as determined by Reclamation, may constitute grounds for termination of this Consent. Such violations require immediate corrective action by the Consentee and shall make the Consentee liable for the cost of full and complete remediation and/or restoration of any Federal resources or facilities that are adversely affected as a result of the violation.

f. The Consentee agrees to include the provisions contained in paragraphs (a) through (e) of this Article in any subcontract or third-party contract it may enter into pursuant to this Consent.

g. Reclamation agrees to provide information necessary for the Consentee using reasonable diligence, to comply with the provisions of this Article.

15. THIRD PARTY RIGHTS

This Consent is granted subject to all rights previously acquired by third parties.

16. SEVERABILITY

Each provision of this Consent shall be interpreted in such a manner as to be valid under applicable law, but if any provision of this Consent shall be deemed or determined by competent authority to be invalid or prohibited hereunder, such provision shall be ineffective and void only to the extent of such invalidity or prohibition, but shall not be deemed ineffective or invalid as to the remainder of such provision or any other remaining provisions, or of this Consent as a whole.

17. BINDING

The provisions of this Consent shall apply to, and bind, the successors and assigns of the parties hereto, but no assignment or transfer of this Consent or any part or interest therein shall be valid until approved by Reclamation.

18. OFFICIALS NOT TO BENEFIT

No Member of Congress shall be admitted to any share or part of any contract or agreement made, entered into, or accepted by or on behalf of the United States, or to any benefit to arise thereupon.

IN WITNESS WHEREOF, the parties hereto have executed this Consent the day and year first above written.

CONSENTEE

UNITED STATES OF AMERICA

By [Signature]  
Title Mayor

By \_\_\_\_\_  
Melanie J. Paquin  
Deputy Area Manager  
Bureau of Reclamation  
230 Collins Road  
Boise, ID 83702-4520

This Consent has been considered and is hereby approved by the **Boise Project Board of Control** the 29 day of JANUARY, 2019

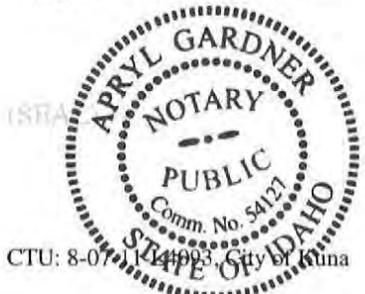
By [Signature]  
Robert Carter, Manager

ACKNOWLEDGEMENT

STATE OF IDAHO )  
                          )ss  
County of Ada    )

On the 29 day of January, 2019, personally appeared before me, the undersigned notary, Robert Carter, known to me to be the official of the **Boise Project Board of Control** who executed the within and foregoing instrument and acknowledged said instrument to be the free and voluntary act and deed of said Board, for the uses and purposes therein mentioned, and on oath stated that he was authorized to execute said instrument.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my official seal the day and year first above written.



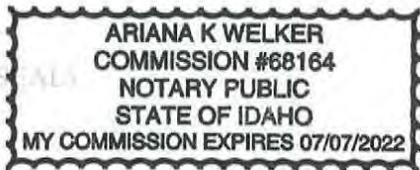
[Signature]  
Notary Public in and for the  
State of Idaho  
Residing at Meridian ID  
My commission expires 3-12-2021

ACKNOWLEDGEMENT

STATE OF IDAHO )  
 )ss  
County of Ada )

On the 20<sup>th</sup> day of February, 2019, personally appeared before me Joe L. Stear, known to me to be the official of the **City of Kuna** who executed the within and foregoing instrument and acknowledged said instrument to be the free and voluntary act and deed of said City of Kuna, for the uses and purposes therein mentioned, and on oath stated that he was authorized to execute said instrument.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my official seal the day and year first above written.



Ariana K Welker  
Notary Public in and for the  
State of Idaho  
Residing at Ada County  
My commission expires 7/7/2022

ACKNOWLEDGEMENT

STATE OF IDAHO )  
 )ss  
County of Ada )

On the \_\_\_\_\_ day of \_\_\_\_\_, 2019, personally appeared before me Melanie J. Paquin, to me known to be the official of the **United States of America** that executed the within and foregoing instrument and acknowledged said instrument to be the free and voluntary act and deed of said United States for the uses and purposes therein mentioned, and on oath stated that she was authorized to execute said instrument.

IN WITNESS WHEREOF, I have hereunto set my hand and affixed my official seal the day and year first above written.

(SEAL)

\_\_\_\_\_  
Notary Public in and for the  
State of Idaho  
Residing at \_\_\_\_\_  
My commission expires \_\_\_\_\_





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 Suite 201  
 Boise, ID 83709-0944  
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PRELIMINARY  
 PLANS  
 NOT FOR  
 CONSTRUCTION

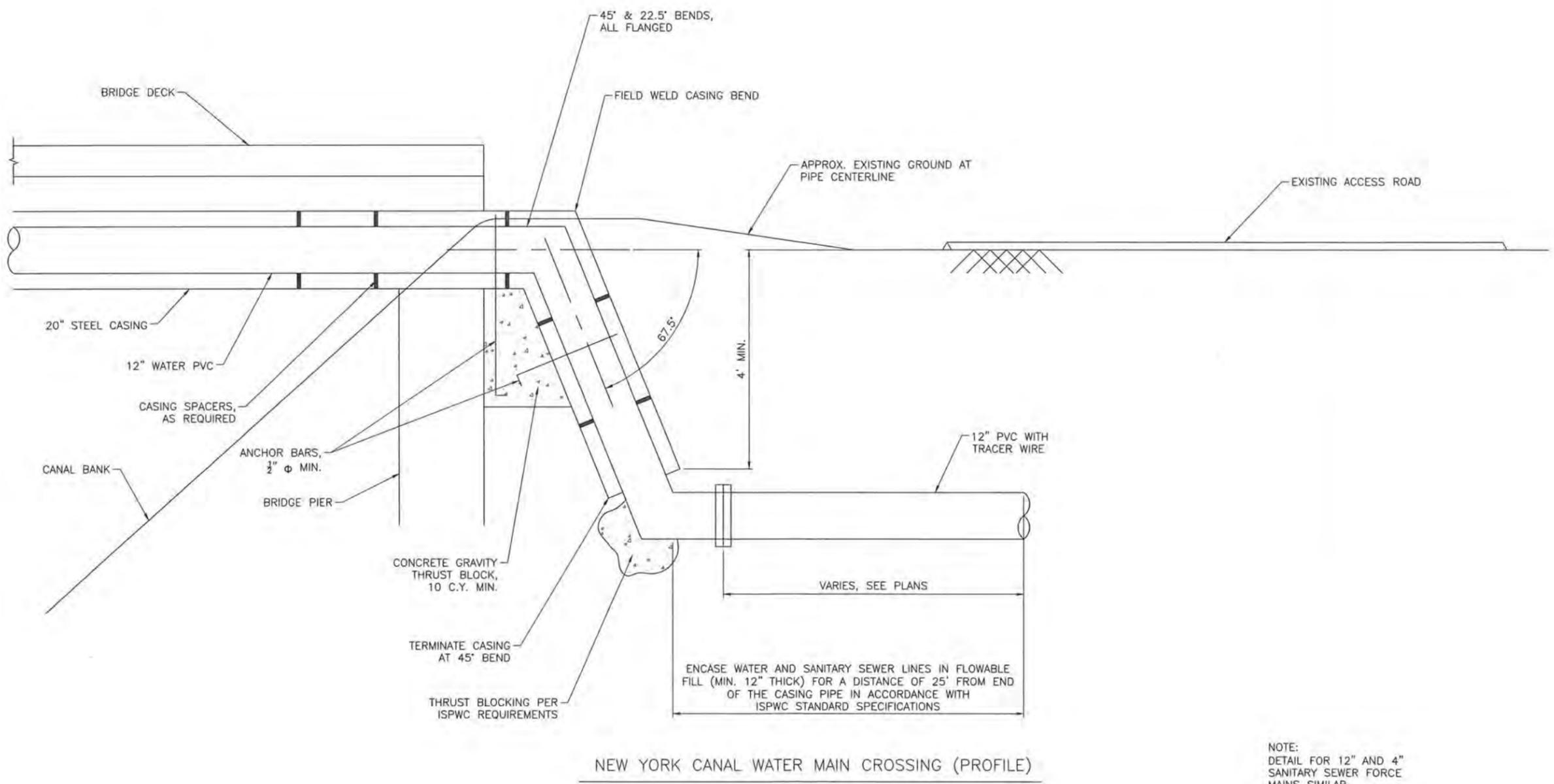
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NO.	DESCRIPTION	BY	DATE

KUNA ROAD SEWER EXTENSION  
 PROPOSED NEW WATER AND SEWER FORCE MAIN CROSSING

FILE: NY CANAL\_002  
 JUB PROJ #: 10-18-125  
 DRAWN BY: KWC  
 DESIGN BY: BRW  
 CHECKED BY: BOS  
 ONE INCH  
 AT FULL SIZE, IF NOT ONE  
 INCH, SCALE ACCORDINGLY  
 LAST UPDATED: 1/10/2019  
 SHEET NUMBER:  
**C2.0**

AGENCY REVIEW



NEW YORK CANAL WATER MAIN CROSSING (PROFILE)

NOTE:  
 DETAIL FOR 12" AND 4"  
 SANITARY SEWER FORCE  
 MAINS SIMILAR.

Plot Date: 1/10/2019 10:24 AM, Printed By: Brian Thompson  
 Date Created: 1/10/2019 10:24 AM, Project: NY CANAL CROSSING, Agency: AGENCY REVIEW, Drawing: NY CANAL\_002.DWG

GENERAL NOTES

CONSTRUCTION SPECIFICATIONS

MATERIALS AND CONSTRUCTION WORKMANSHIP SHALL BE IN ACCORDANCE WITH IDAHO STANDARDS FOR PUBLIC WORKS CONSTRUCTION 2015 ED., AND THE PROJECT PLANS AND SPECIFICATIONS.

MATERIALS

CONCRETE:	CLASS 4000A
METAL REINFORCEMENT:	ASTM A615/A GRADE 60
EPOXY FOR ANCHORING METAL REINF.:	HILTI HIT-RE 500 V3 (OR APPROVED EQUAL)
STEEL CASING:	A500 GR. B
ELASTOMERIC BEARINGS:	60 DURO. (SHORE A) GRADE 3; SHEAR MOD. BTWN. 120-200 PSI
STRUCTURAL STEEL:	AASHTO M270 GR 50
ANCHOR RODS:	ASTM F1554 GRADE 55
SUBGRADE SEPARATION GEOTEXTILE:	TYPE III
GEO GRID GEOSYNTHETIC:	MIRAFI BXG110 (OR APPROVED EQUAL)

CONSTRUCTION

PERFORM WELDING IN ACCORDANCE WITH AASHTO/AWS D1.5  
GALVANIZE STEEL PIPE CASING, STEEL SADDLE ASSEMBLY AT ABUTMENTS AND PIERS IN ACCORDANCE WITH ASTM A123, (AASHTO M111). FABRICATE UTILITY PIPE SPACERS TO ACCOUNT FOR REQUIRED SLOPE OF UTILITY PIPES AFTER FIELD SURVEYING THE AS-BUILT STEEL CASING IN PLACE, SURVEY OF CASING PROFILES SHOULD BE AT 10TH POINTS ALONG CENTERLINE OF PIPES.

STEEL CASINGS

FABRICATE THE 20" DIA. X 3/8" THICK STEEL CASING AND 16" DIA. X 1/2" THICK STEEL CASING TO BE STRUCTURALLY CONTINUOUS FROM ABUTMENT 1 TO ABUTMENT 2 SUPPORTS. FABRICATE 20" DIA. X 3/8" THICK STEEL CASING AND 16" DIA. X 1/2" THICK STEEL CASING TO ELIMINATE THE CASING DEFLECTIONS FRO THE SELF WEIGHT OF THE CASING, THE UTILITY PIPE SELF WEIGHT AND WEIGHT OF FLUIDS/SOLIDS IN THE UTILITY PIPE WHEN FLOWING FULL. USE FLUID/SOLID DENSITY OF 64 PCF. 20" DIA. CASING CARRIES A 12" DIA. UTILITY PIPE. 16" DIA. CASING CARRIES A 4" DIA. UTILITY PIPE. SUBMIT SHOP DRAWINGS AND COORDINATE WEIGHT OF UTILITY PIPES WITH CONTRACTOR CALCULATIONS PRIOR TO FABRICATING CASING FOR CAMBER AND FOR ANY SPLICES.

Plot Date: 10/20/2019 12:56 PM Plotted By: Kurt Cox  
Date Created: 7/16/2019 WROSETELSPUBLICPROJECTS\JUB\05-16-19\05-16-19\CON\CREST SEWER GRADING\CA\DEVELOPMENT\SS\SS\12-18-19\SS-12-18-19.DWG



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NO.	REVISION	DESCRIPTION	BY	DATE

KUNA ROAD SEWER EXTENSION  
KUNA, ID

STRUCTURAL NOTES

FILE: 10-18-125 S1-0  
 JUB PROJ #: 10-18-125  
 DRAWN BY: ARB  
 DESIGN BY: RT  
 CHECKED BY: BDS  
 ONE INCH  
 AT FULL SIZE. IF NOT ONE  
 INCH SCALE ACCORDINGLY  
 LAST UPDATED: 1/8/2019  
 SHEET NUMBER:  
**S1.0**



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NO.	REVISION	DESCRIPTION	BY	DATE

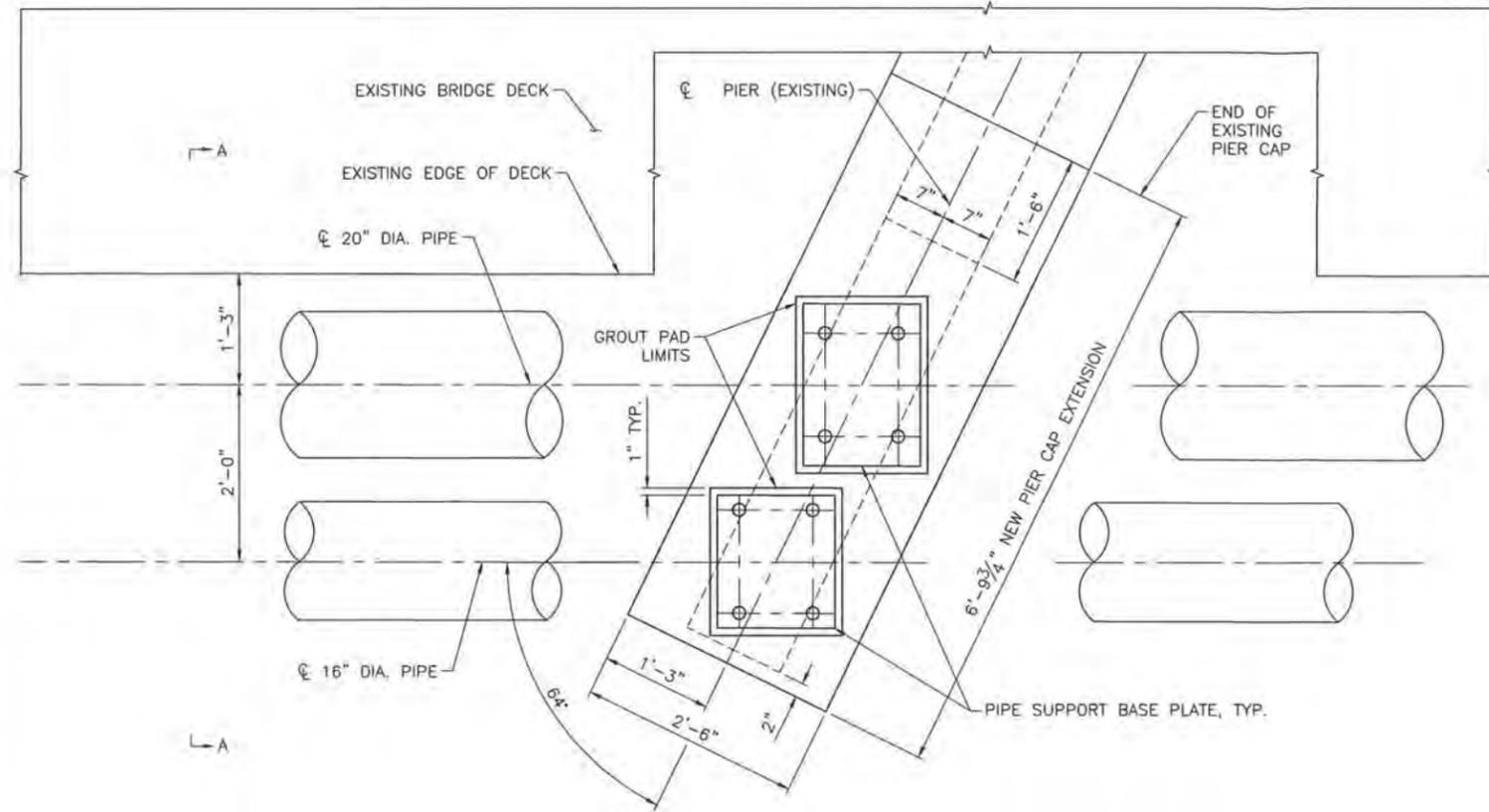
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KUNA ROAD SEWER EXTENSION  
KUNA, ID

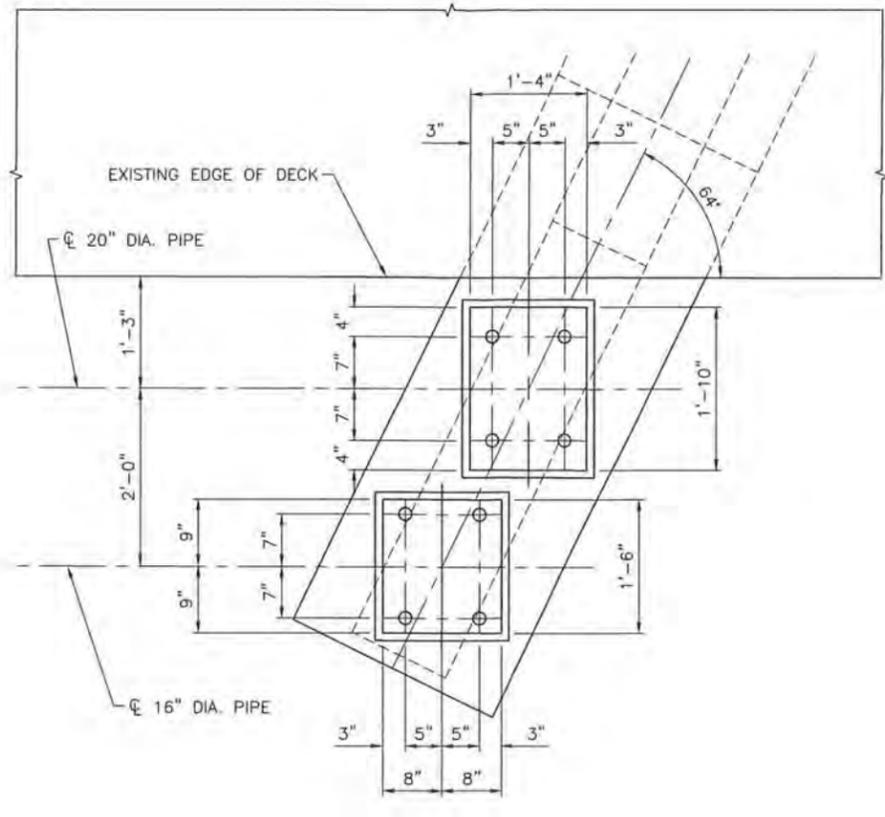
PIER - PIPE SUPPORT LAYOUT

FILE: 10-18-125-S2.0  
JUB PROJ #: 10-18-125  
DRAWN BY: ARB  
DESIGN BY: RT  
CHECKED BY: BDS  
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AT FULL SIZE, IF NOT ONE INCH SCALE ACCORDINGLY.  
LAST UPDATED: 10/2019  
SHEET NUMBER:

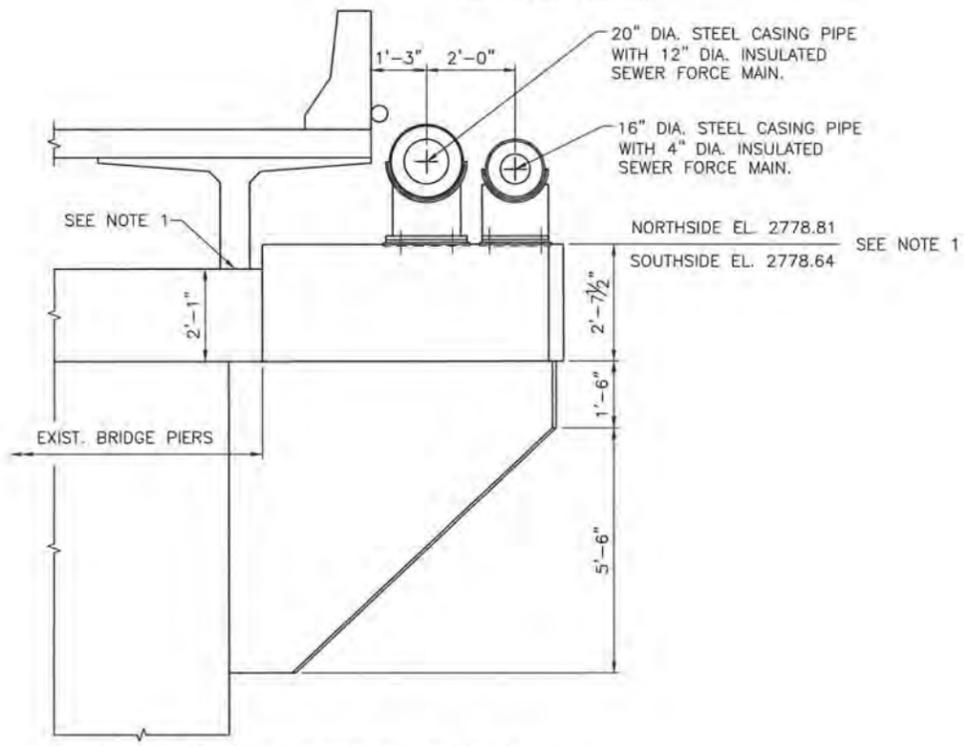
S2.0



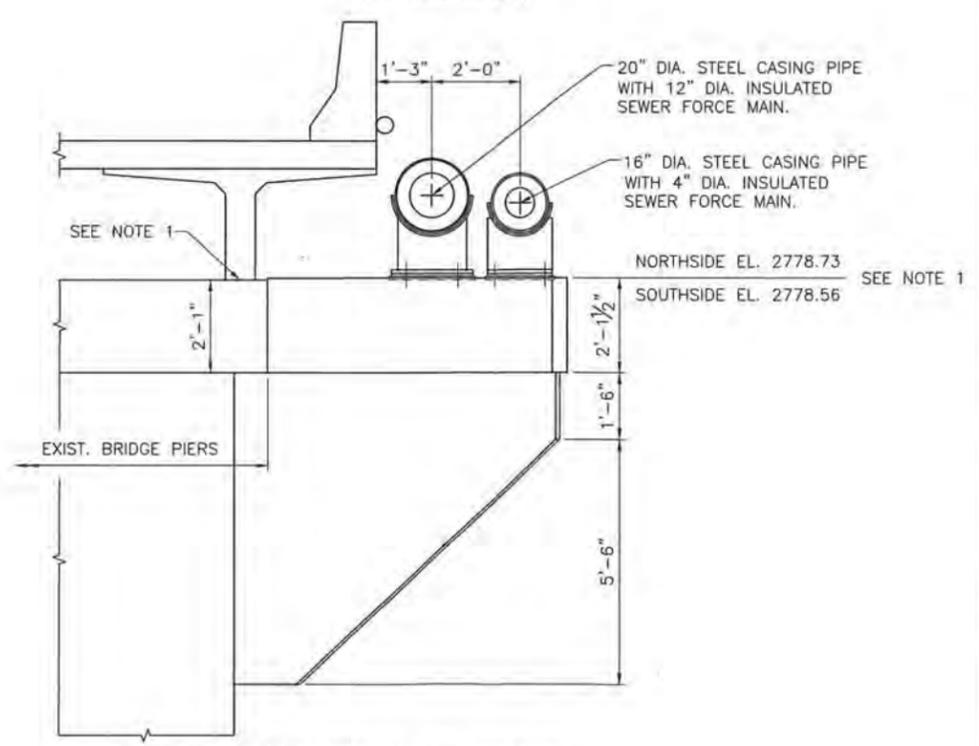
PLAN DIMENSIONS  
SOUTH SIDE OF BRIDGE  
(NORTH SIDE SIMILAR)



PLAN ANCHOR LAYOUT  
SOUTH SIDE OF BRIDGE  
(NORTH SIDE SIMILAR)



SECTION A-A PEDESTAL AT PIER 1  
SOUTH SIDE OF BRIDGE LOOKING EAST  
(NORTH SIDE SIMILAR)



SECTION A-A PEDESTAL AT PIER 2  
SOUTH SIDE OF BRIDGE LOOKING EAST  
(NORTH SIDE SIMILAR)

- NOTES:
- FIELD VERIFY ELEVATIONS AND LOCATION OF EXISTING CONCRETE BY SURVEY PRIOR TO CONSTRUCTION.

Plot Date: 10/20/19 12:55 PM Plotted By: Kurt Cox  
 Date Created: 10/18/2019 10:05:51 AM Project Path: C:\PROJECTS\10-18-125-S2.0\DWG



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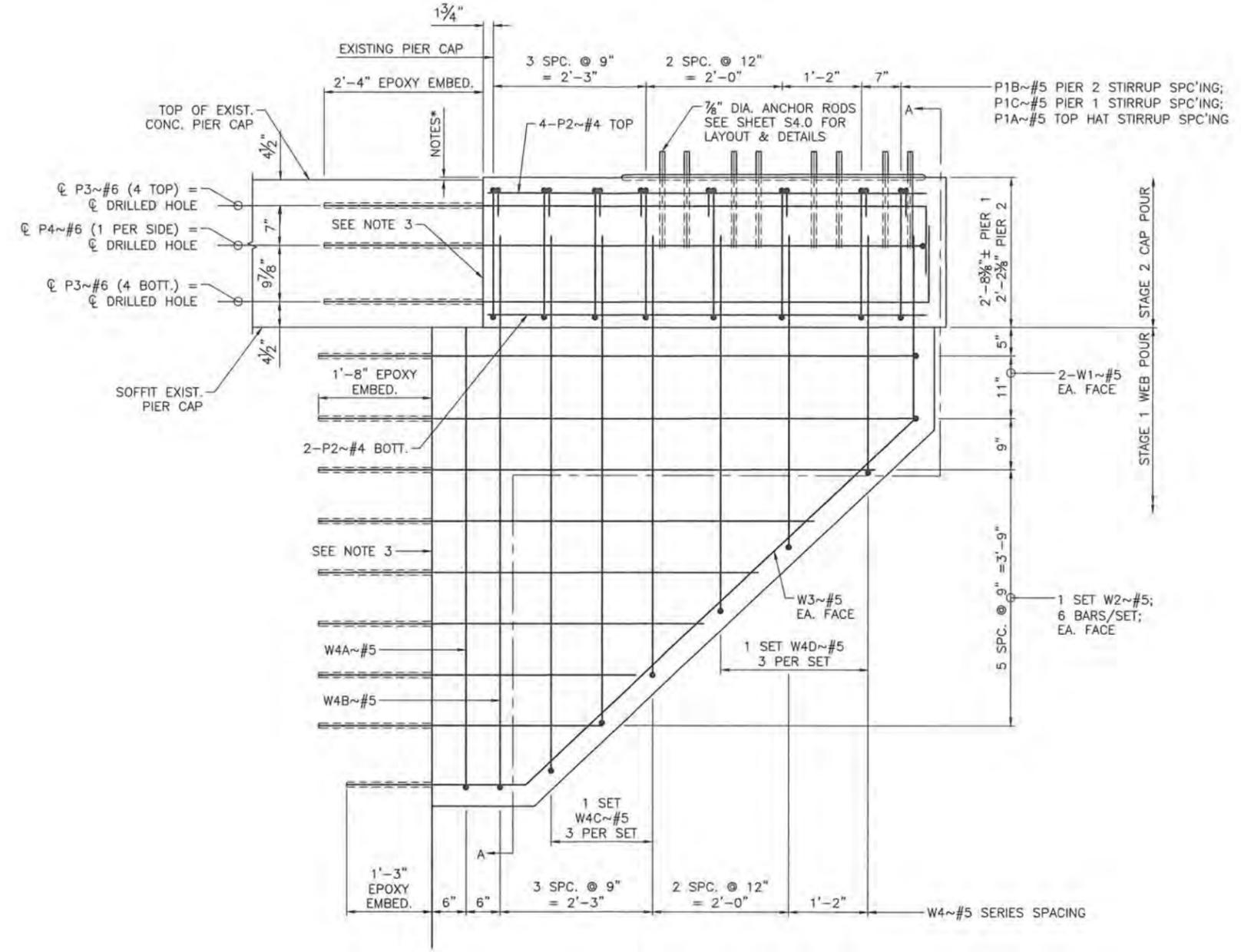
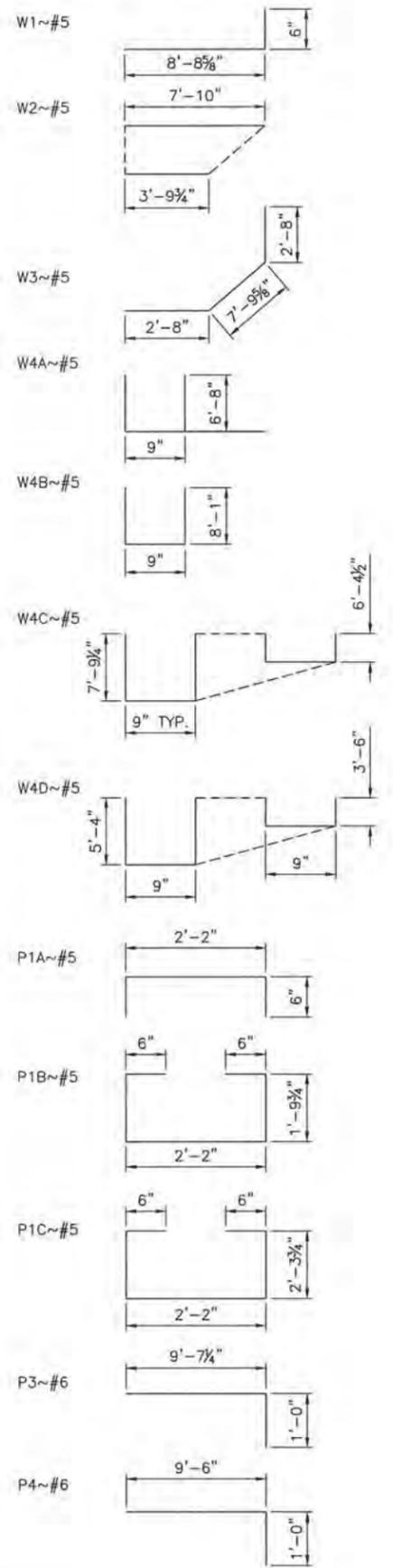
NO.	DESCRIPTION	BY	DATE

KUNA ROAD SEWER EXTENSION  
KUNA, ID

PIER - CORBEL REINFORCEMENT 1

FILE: 10-16-125-S3.0  
JUB PROJ #: 10-16-125  
DRAWN BY: ARB  
DESIGN BY: RT  
CHECKED BY: BDS  
ONE INCH  
AT FULL SIZE, IF NOT ONE INCH SCALE ACCORDINGLY  
LAST UPDATED: 1/9/2019  
SHEET NUMBER:  
**S3.0**

BENT BAR DETAILS



PEDESTAL ELEVATION  
PIER 2 SHOWN  
(PIER 1 SIMILAR)

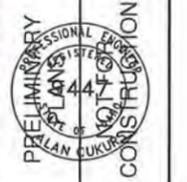
- NOTES:
1. CAST STAGE 1 WEB FIRST. AFTER 5 DAYS CAST STAGE 2 CAP POUR.
  2. COORDINATE PLACEMENT OF REINFORCEMENT TO ACCOMMODATE ANCHORS.
  3. ROUGHEN EXISTING CONCRETE SURFACES TO 1/4" AMPLITUDE AT INTERFACE W/NEW CONCRETE.
  4. SUBMIT SHOP DRAWINGS FOR STEEL REINFORCEMENT PRIOR TO FABRICATION.
  5. \* 6 1/2" PIER 1 SOUTHSIDE  
\* 1/2" PIER 2 SOUTHSIDE  
\* 6" PIER 1 NORTHSIDE  
\* 1/2" PIER 2 SOUTHSIDE

Plot Date: 10/20/19 12:55 PM, Plotted By: Kurt Cox  
 Date Created: 10/20/19, PROJECT: KUNA ROAD SEWER EXTENSION, PROJECT: KUNA ROAD SEWER EXTENSION, DRAWING: S3.0.DWG



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NO.	DESCRIPTION	BY	DATE

NEW YORK CANAL AT KUNA ROAD

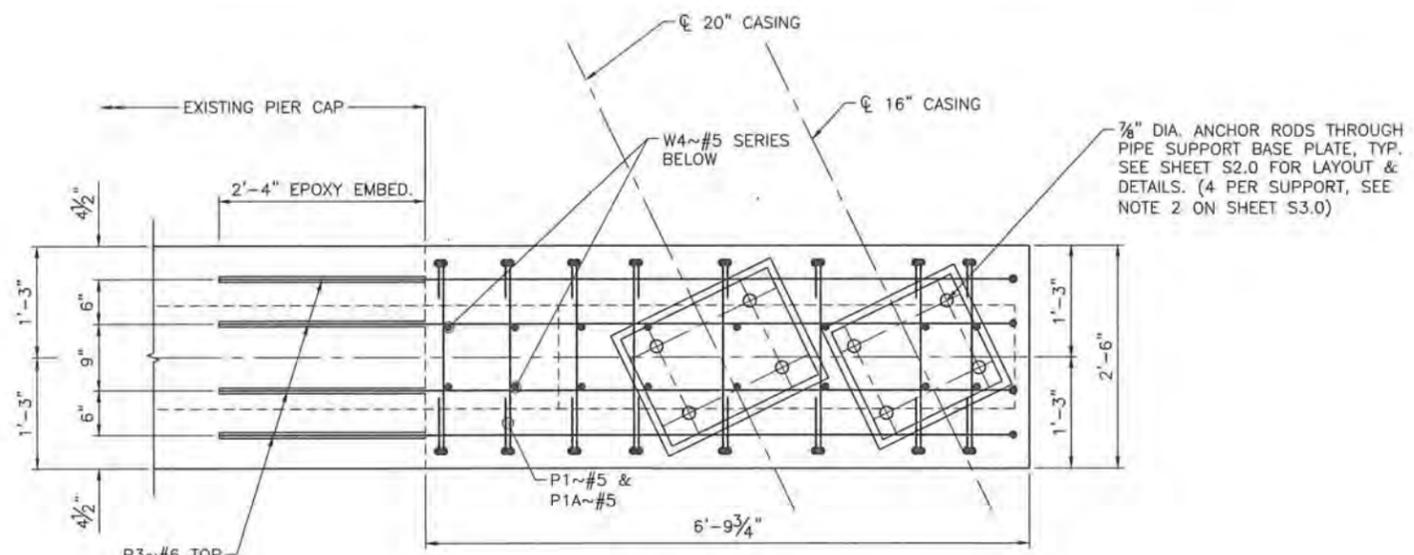
PIER - CORBEL REINFORCEMENT - 2

FILE: 10-18-125-54-0
JUB PROJ #: 10-18-125
DRAWN BY: KWC
DESIGN BY: AJC
CHECKED BY: BDS

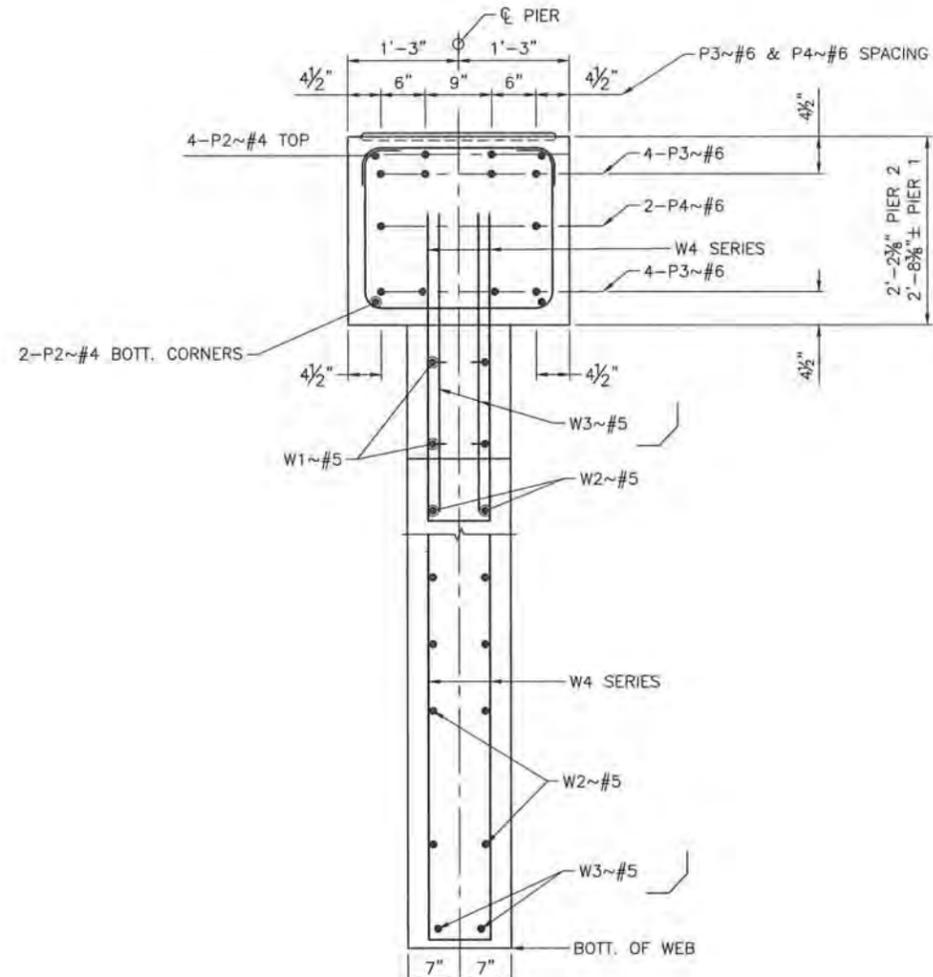
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LAST UPDATED: 10/2019

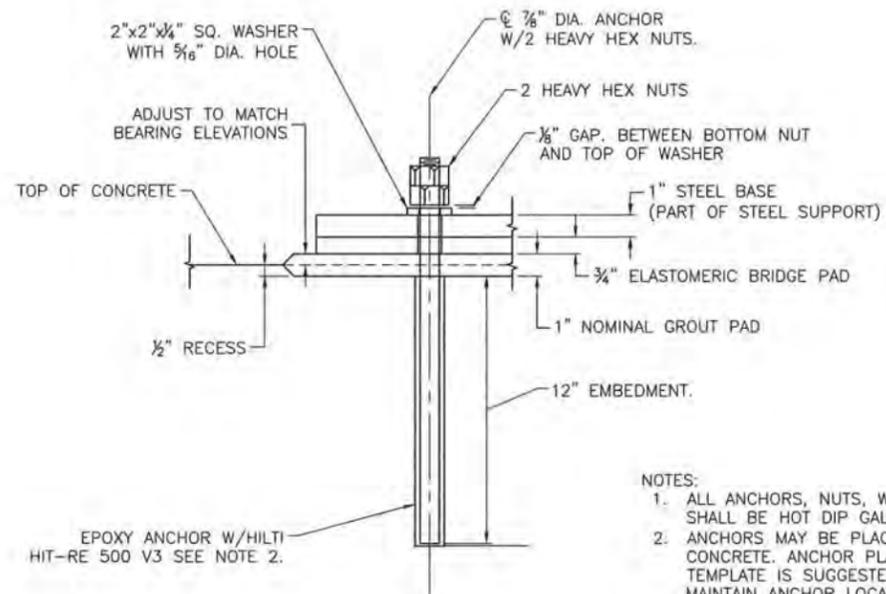
SHEET NUMBER:  
**S4.0**



**PLAN**  
 SOUTH SIDE OF BRIDGE  
 (NORTH SIDE SIMILAR)  
 NOT TO SCALE



**SECTION A-A**  
 NOT TO SCALE



**ANCHOR DETAIL**  
 NOT TO SCALE

- NOTES:
1. ALL ANCHORS, NUTS, WASHERS SHALL BE HOT DIP GALVANIZED.
  2. ANCHORS MAY BE PLACED W/CAST CONCRETE. ANCHOR PLACEMENT TEMPLATE IS SUGGESTED TO MAINTAIN ANCHOR LOCATIONS.
  3. USE THREADED ANCHOR ROD THAT MEETS ASTM F1554 GRADE 55.

File Date: 10/2019 12:55 PM. Plotted By: Kim Cox  
 Date Created: 10/2019. USER: ES/PAUL/IC/PROJECTS/10-18-100-FALCON-CREST-SEWER-GRADING/CADD/EXHIBITS/CANAL-CROSSING/10-18-125-S4-0.DWG



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NO.	REVISION	DESCRIPTION	BY	DATE

KUNA ROAD SEWER EXTENSION  
KUNA, ID

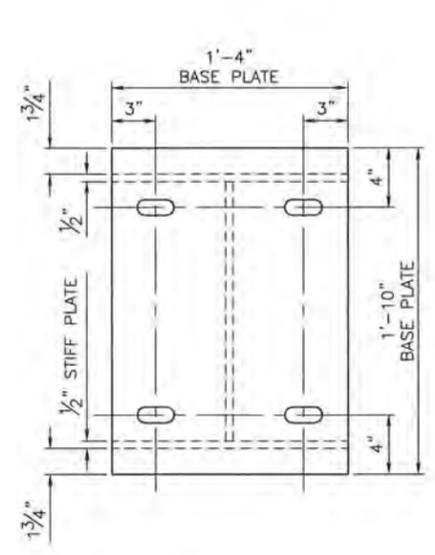
PIER - METAL SUPPORT DETAILS

FILE: 18-18-125-S5-0  
JUB PROJ #: 18-18-125  
DRAWN BY: ARB  
DESIGN BY: RT  
CHECKED BY: BDS  
DATE: ONE NCH  
AT FULL SIZE, IF NOT ONE  
INCH SCALE ACCORDINGLY  
LAST UPDATED: 1/8/2019  
SHEET NUMBER:

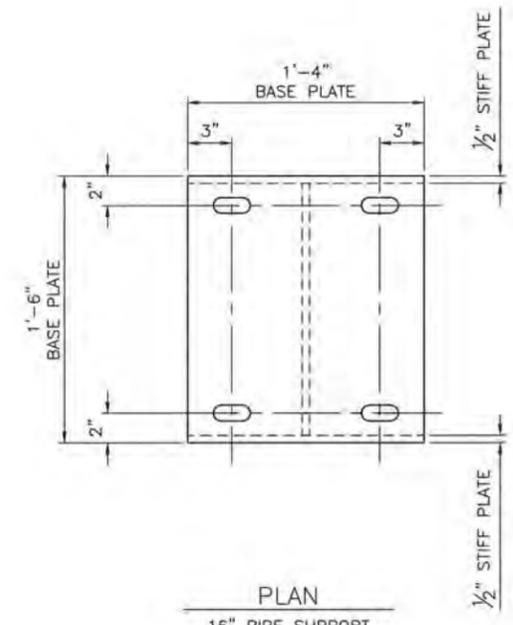
S5.0

NOTES:

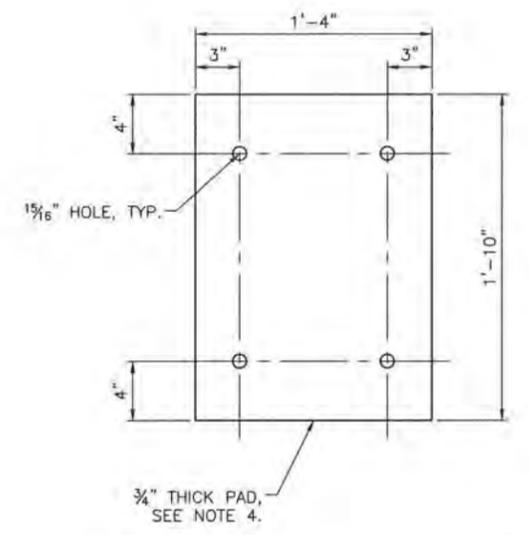
1. USE STRUCTURAL STEEL THAT MEETS M270 GRADE 50 REQUIREMENTS.
2. USE ANCHOR RODS THAT MEET ASTM F1554 GRADE 55.
3. PROVIDE WELDS CONFORMING TO AASHTO/AWS 1.5.
4. USE 60 DUROMETER HARDNESS (SHORE A) GRADE 3 ELASTOMER WITH MINIMUM AND MAXIMUM SHEAR MODULUS OF 120 TO 200 PSI.
5. GALVANIZE COMPLETE ASSEMBLY PER AASHTO M111.
6. SUBMIT SHOP DRAWINGS PRIOR TO FABRICATION.



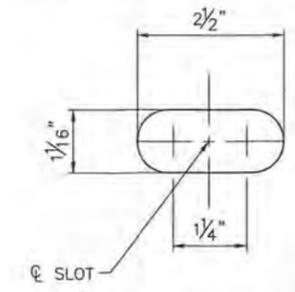
PLAN  
20" PIPE SUPPORT



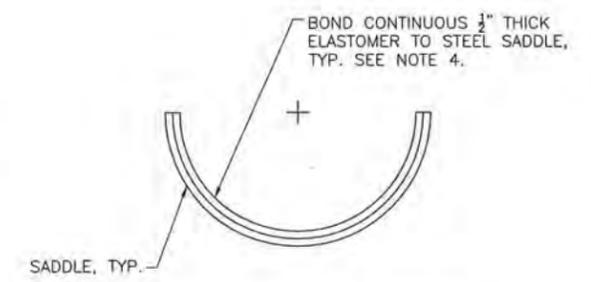
PLAN  
16" PIPE SUPPORT



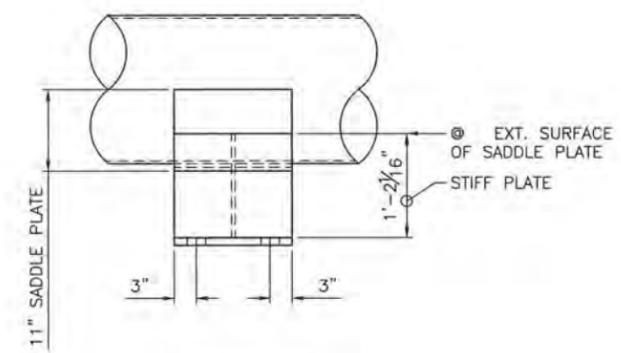
PLAN ELASTOMERIC BEARING PAD  
20" PIPE



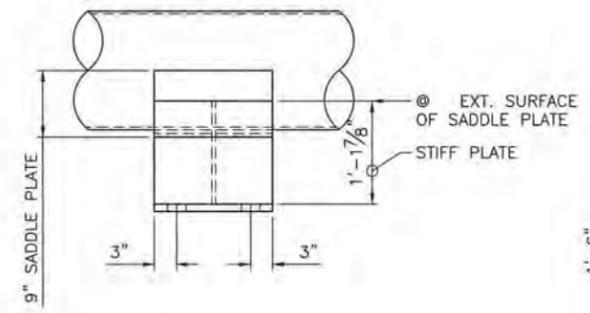
SLOTTED HOLE DETAIL



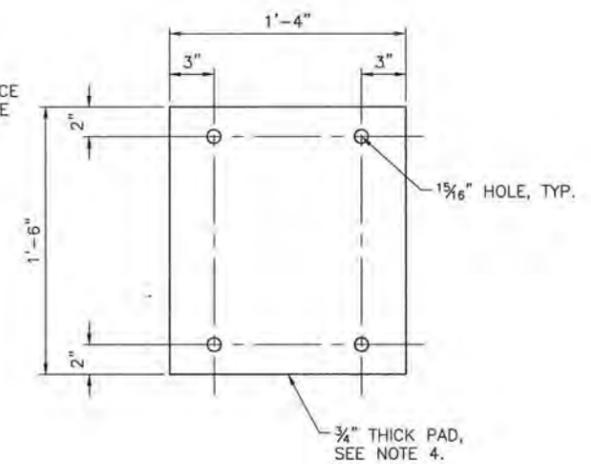
ELASTOMERIC LINING DETAIL



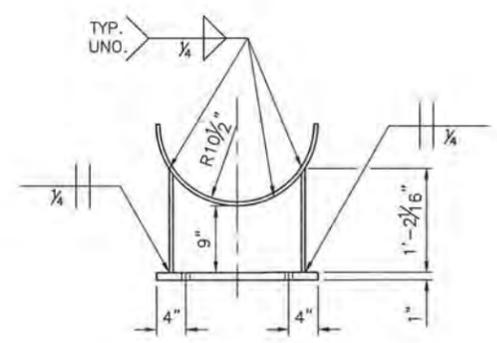
ELEVATION  
20" PIPE SUPPORT



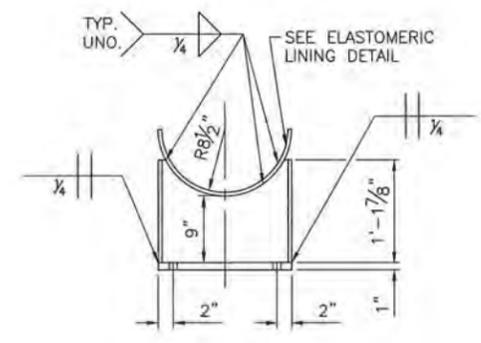
ELEVATION  
16" PIPE SUPPORT



PLAN ELASTOMERIC BEARING PAD  
16" PIPE



SECTION  
20" PIPE SUPPORT



SECTION  
16" PIPE SUPPORT

Plot Date: 1/8/2019 12:58 PM Plotted By: Bart Cox  
Data Checked: 1/8/2019 10:08 AM PROJECT: PUBLIC PROJECTS/UB-16-100 FALCON CREST SEWER GRADING/NEW/UB-16-125-S5-0.DWG



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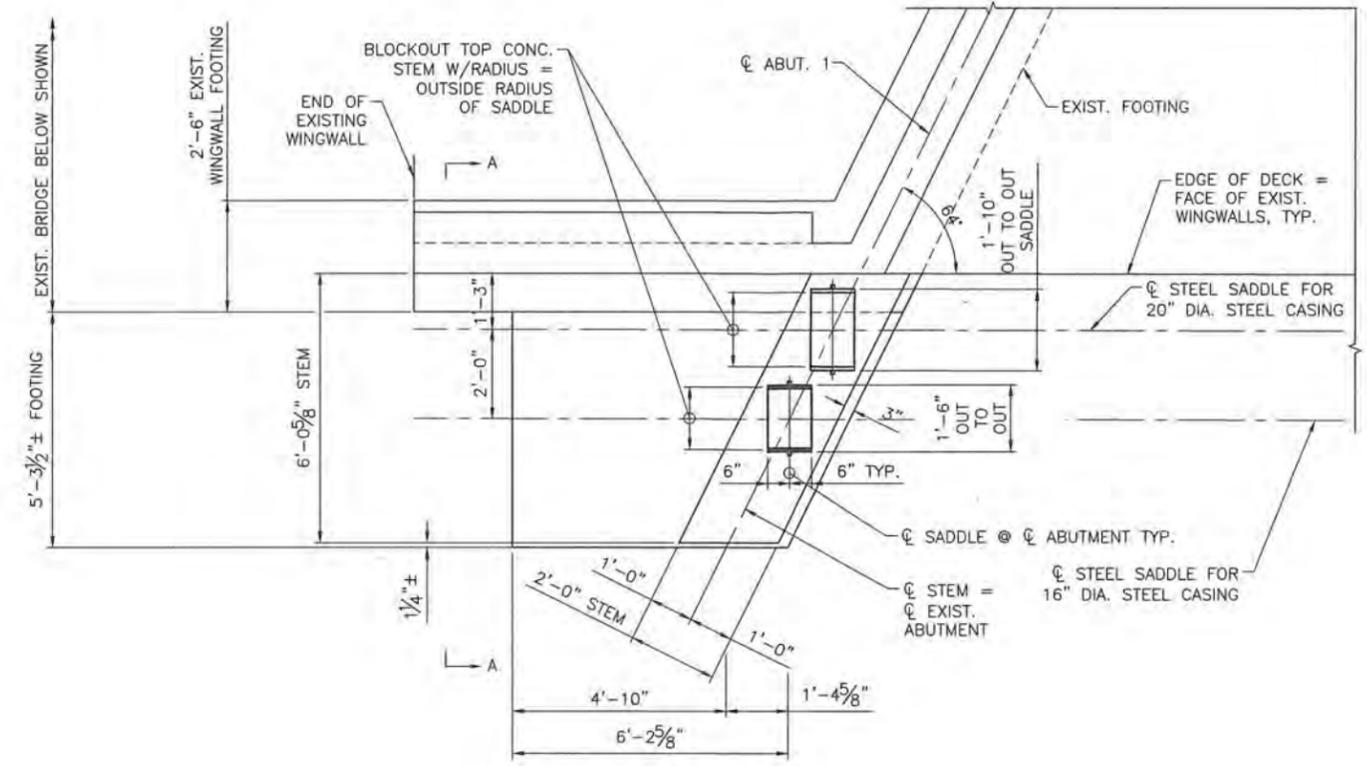
NO.	REVISION	DESCRIPTION	BY	DATE

KUNA ROAD SEWER EXTENSION  
 KUNA, ID

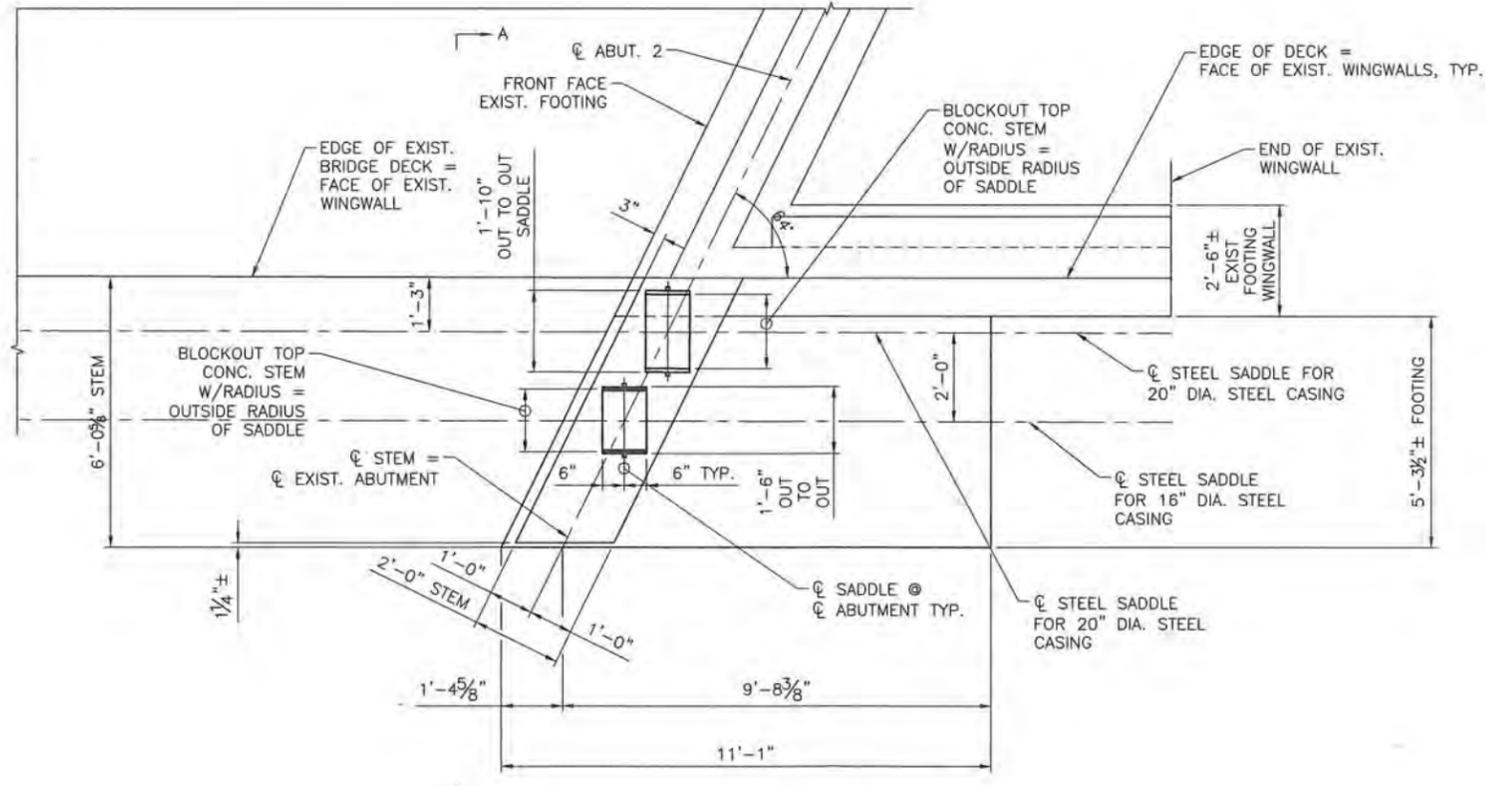
ABUTMENT SUPPORT LAYOUT

FILE: 10_18_125_S6.0
JUB PROJ #: 10-18-125
DRAWN BY: ARB
DESIGN BY: ACC
CHECKED BY: BCS
DATE: 1/8/2019
SCALE: ONE INCH = 1'-0"
AT FULL SIZE IF NOT ONE INCH SCALE ACCORDINGLY
LAST UPDATED: 1/8/2019
SHEET NUMBER: S6.0

AGENCY REVIEW



PLAN S.W. ABUTMENT CORNER  
 (N.E. ABUTMENT CORNER SIMILAR)



PLAN S.E. ABUTMENT CORNER  
 (N.W. ABUTMENT CORNER SIMILAR)

- NOTES:
1. FIELD VERIFY LOCATION OF EXISTING ABUTMENT CONCRETE PRIOR TO CONSTRUCTION.
  2. PROTECT & SHORE SOIL DURING EXCAVATION TO PREVENT UNDERMINING EXISTING FOOTING. FLOWABLE CONCRETE FILL MAY BE USED TO MITIGATE UNINTENTIONAL UNDERMINING OF EXISTING FOOTING.

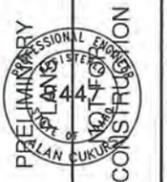
Plot Date: 1/8/2019 12:55 PM Plotted By: Kent Cox Date Created: 1/8/2019 \\BOISEFILES\PUBLIC\PROJECTS\KUNA\100\_FALCON\_CREST\_SEWER\_GRADING\DWG\HBITSCANAL\_CROSSING\10\_18\_S6.0.DWG



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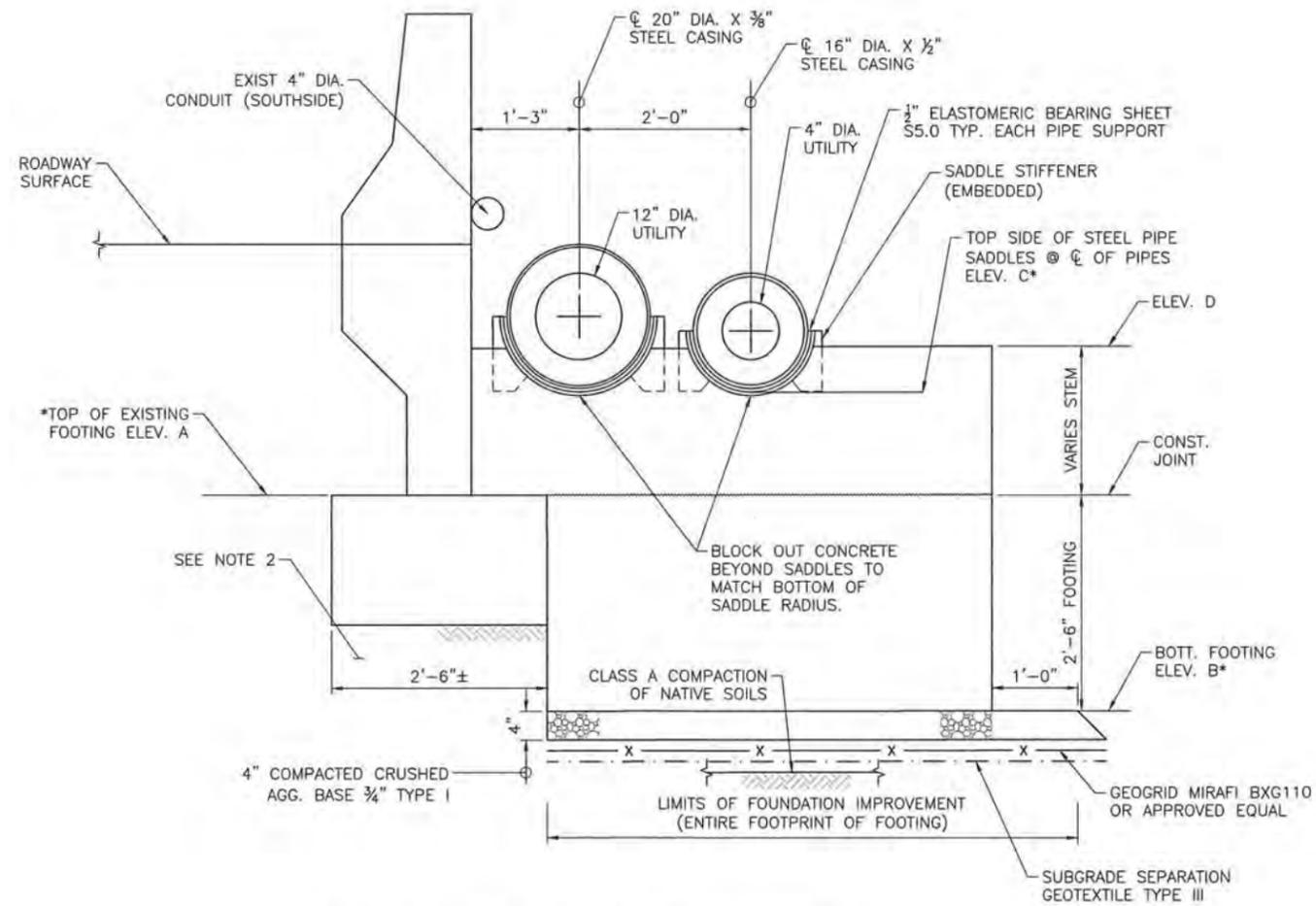
NO.	REVISION	DESCRIPTION	BY	DATE

KUNA ROAD SEWER EXTENSION  
KUNA, ID

ABUTMENT SUPPORT LAYOUT

ELEVATION TABLE (FEET)				
LOCATION	A	B	C	D
ABUT. 1 SOUTH	2778.51	2776.01	2779.66	2780.16
ABUT. 1 NORTH	2778.31	2775.81	2779.83	2780.33
ABUT. 2 SOUTH	2778.69	2776.19	2779.42	2779.92
ABUT. 2 NORTH	2778.65	2776.15	2779.59	2780.09

• (NOTE: FIELD VERIFY EXISTING ABUTMENT CONCRETE ELEVATIONS PRIOR TO CONSTRUCTION.)



SECTION A-A (ABUTMENT PIPE SUPPORT)  
(LOOKING UP STATION SOUTH SIDE OF BRIDGE  
NORTH SIDE SIMILAR)

NOTES:

1. FIELD VERIFY LOCATION OF EXISTING ABUTMENT CONCRETE PRIOR TO CONSTRUCTION.
2. PROTECT & SHORE SOIL DURING EXCAVATION TO PREVENT UNDERMINING EXISTING FOOTING. FLOWABLE CONCRETE FILL MAY BE USED TO MITIGATE UNINTENTIONAL UNDERMINING OF EXISTING FOOTING.

FILE: 19_18_125_S6-1
JUB PROJ #: 19-18-125
DRAWN BY: ARB
DESIGN BY: ACC
CHECKED BY: BDB
ONE INCH
AT FULL SIZE, IF NOT ONE INCH SCALE ACCORDINGLY
LAST UPDATED: 1/8/2019
SHEET NUMBER:

S6.1



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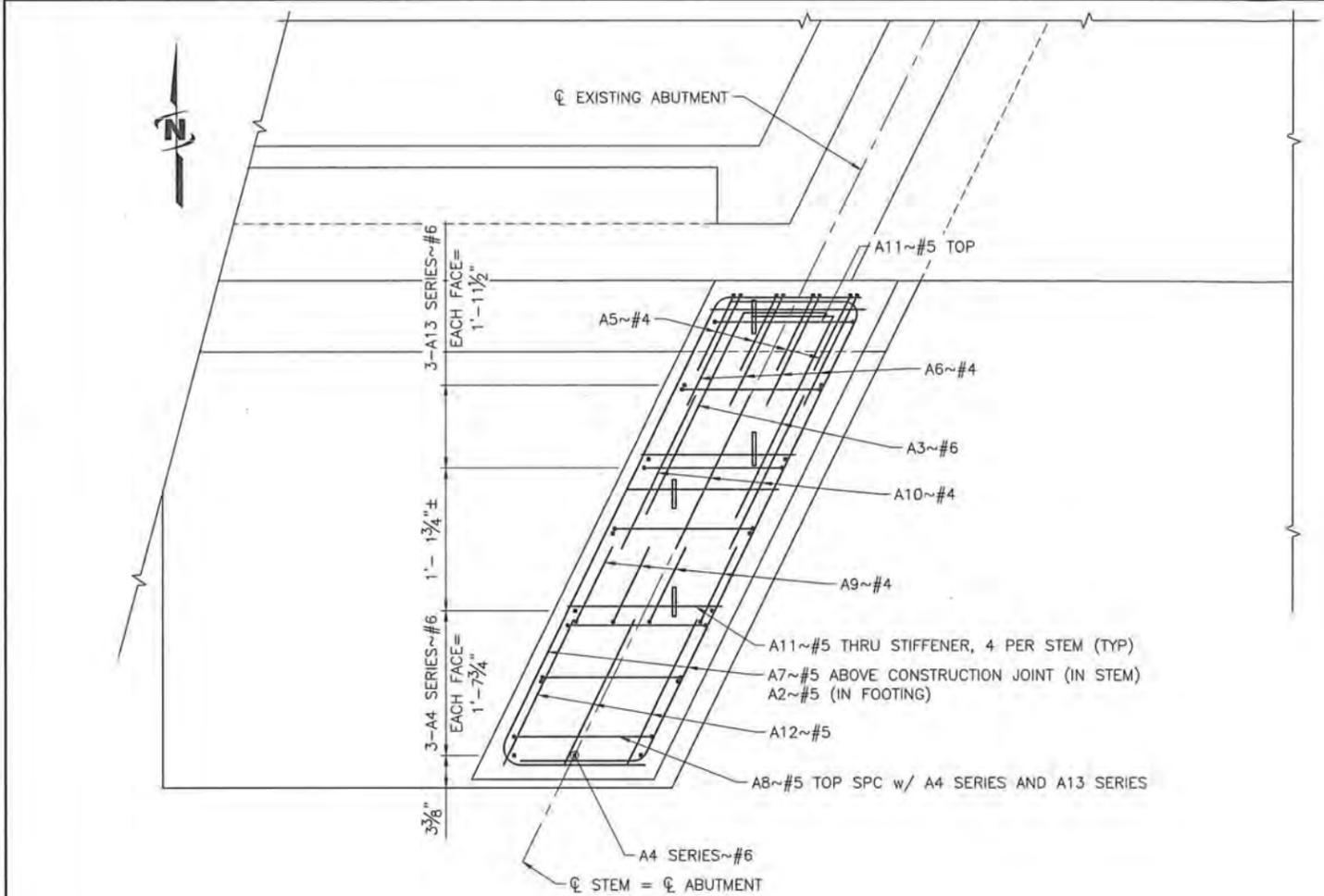
KUNA ROAD SEWER EXTENSION  
 KUNA, ID

ABUTMENT REINFORCEMENT - 1

SHEET NUMBER:  
**S7.0**

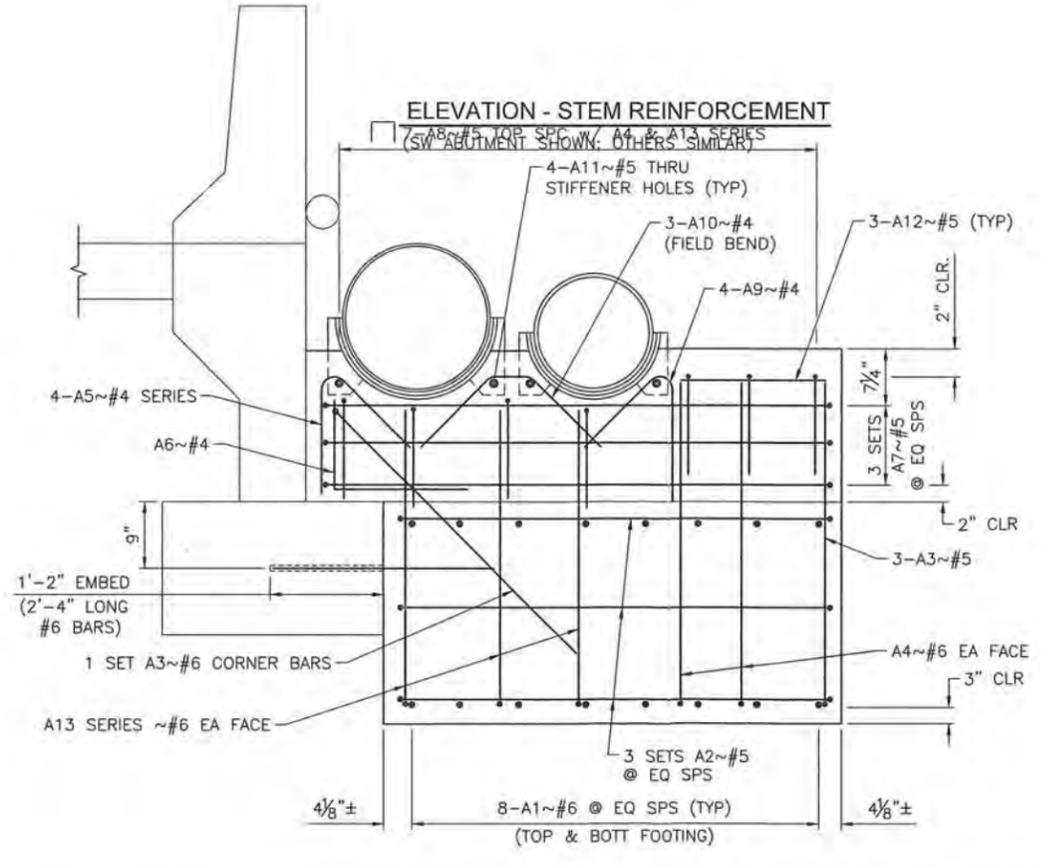
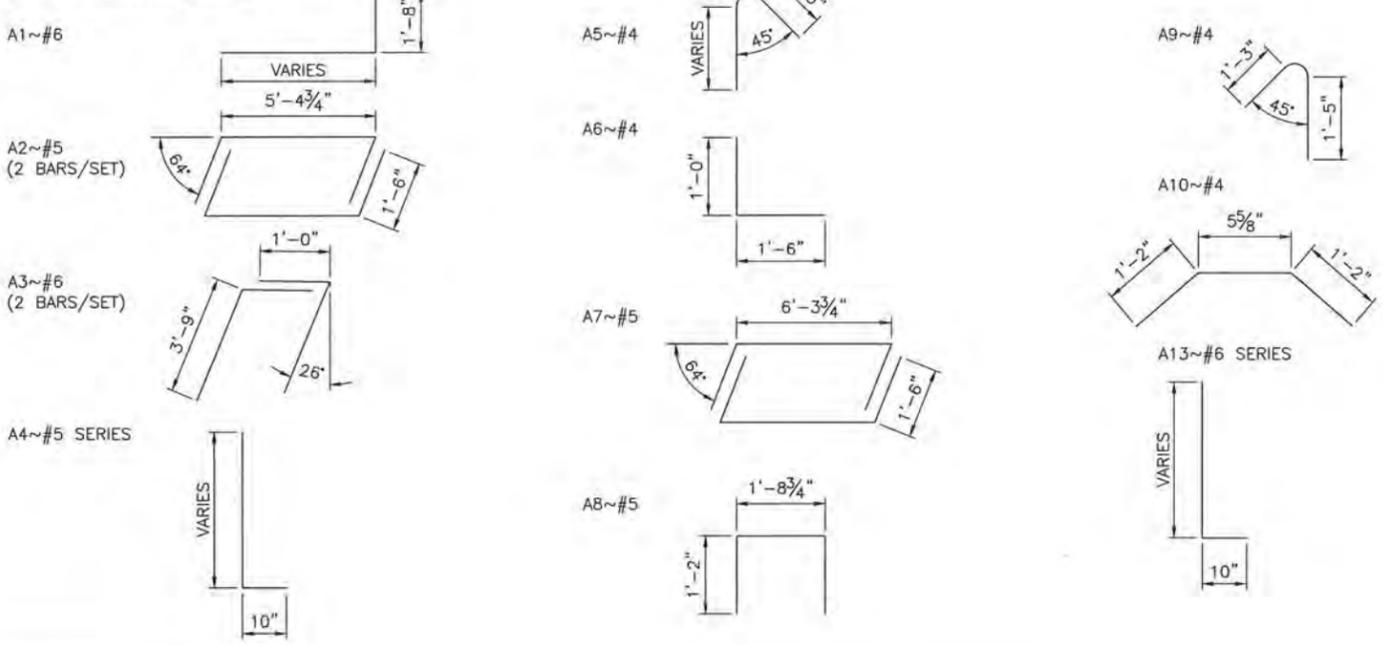
FILE: 10-18-126-57-0  
 JUB PROJ #: 10-18-126  
 DRAWN BY: ARB  
 DESIGN BY: ACC  
 CHECKED BY: BDS  
 ONE INCH = 10 FEET  
 AT FULL SIZE, IF NOT ONE INCH SCALE ACCORDINGLY  
 LAST UPDATED: 1/9/2019

REUSE OF DRAWINGS  
 JUB SHALL RETAIN ALL COMMON LAW, STATUTORY, COPYRIGHT AND OTHER RIGHTS IN THIS DRAWING AND ANY PART THEREOF. NO PART OF THIS DRAWING SHALL BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, WITHOUT WRITTEN CONSENT OF JUB. ANY REUSE WITHOUT WRITTEN CONSENT BY JUB WILL BE AT CLIENT'S SOLE RISK AND WITHOUT LIABILITY OR LEGAL EXPOSURE TO JUB.



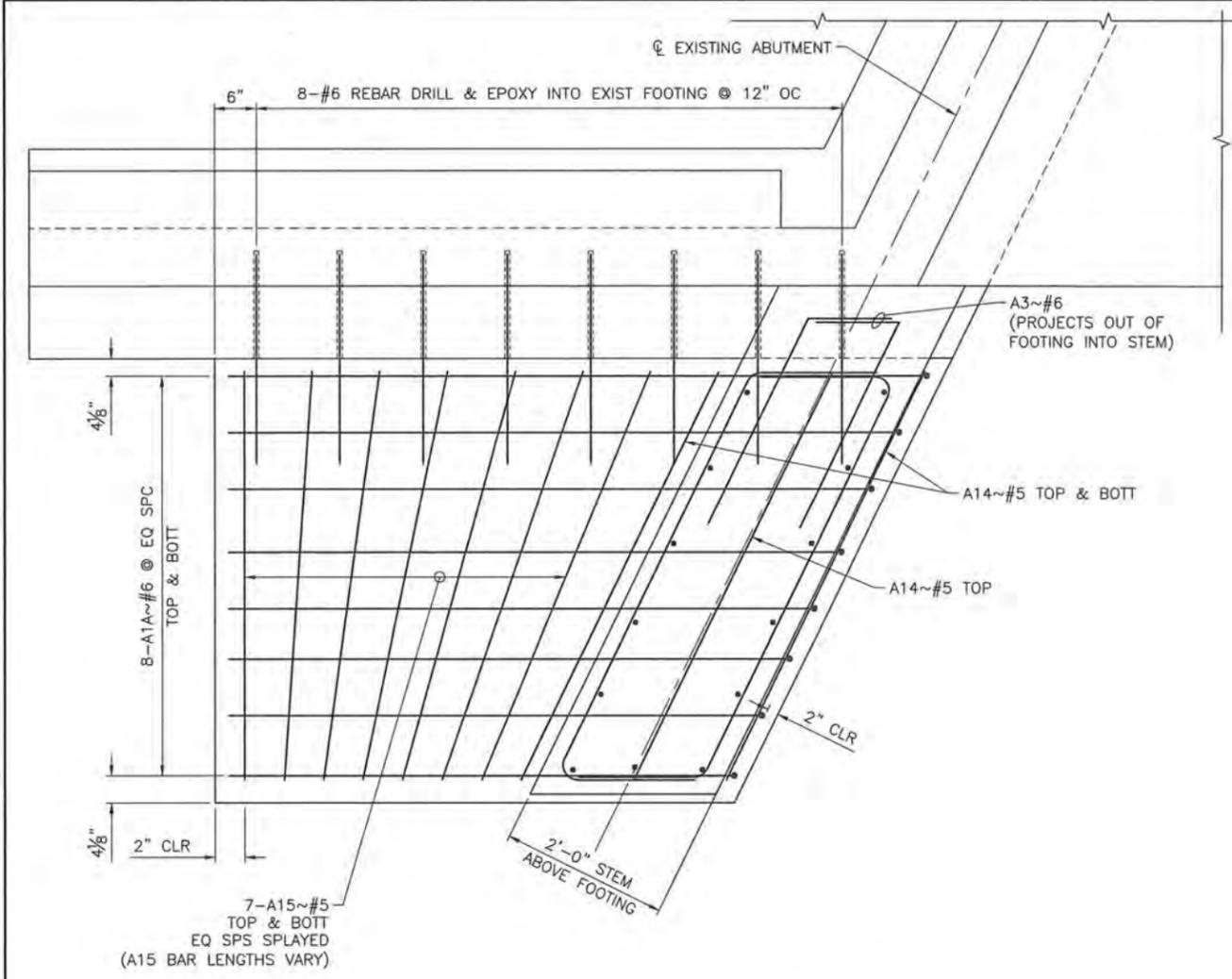
**PLAN - STEM REINFORCEMENT**  
 (SW ABUTMENT SHOWN; OTHERS SIMILAR)

**BENT BAR DETAILS**

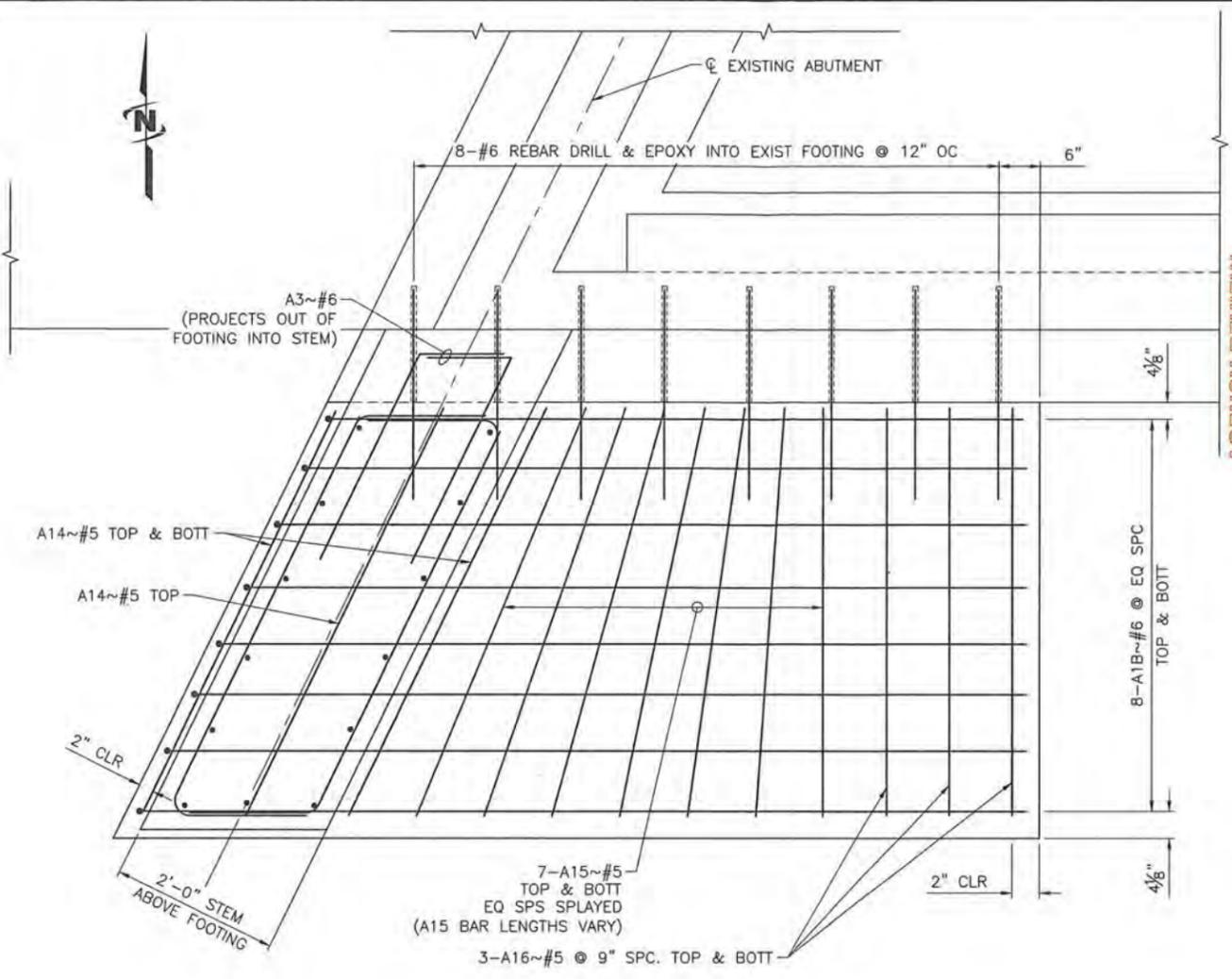


**ELEVATION - STEM REINFORCEMENT**  
 (SW ABUTMENT SHOWN; OTHERS SIMILAR)

P:\10-18-126-57-0\10-18-126-57-0.dwg  
 Date Created: 1/9/2019 12:55 PM  
 Plotted By: Kurt Cox  
 W:\03\FILES\PUBLIC\PROJECTS\10-18-126-57-0.DWG



**PLAN - FOOTING REINFORCEMENT**  
 (SW CORNER SHOWN; NE CORNER SIMILAR)



**PLAN - FOOTING REINFORCEMENT**  
 (SE CORNER SHOWN; NW CORNER SIMILAR)

Plot Date: 1/9/2019 12:56 PM Plotted By: Kurt Cox  
 Date Created: 1/2/2019 10:58:58 AM Project: 100 FALCON CREEK SEWER GRADING/CONCRETE/STRUCTURAL CROSSING/10 - 125\_S8.0.DWG

**JUB**  
 J-U-B ENGINEERS, INC.  
 J-U-B ENGINEERS, INC.  
 250 S. Beechwood Ave.  
 Suite 201  
 Boise, ID 83709-0944  
 Phone: 208.376.7330  
 www.jub.com

AGENCY REVIEW

PROFESSIONAL ENGINEER  
 PRELIMINARY  
 PLAN 444  
 CONSTRUCTION

REUSE OF DRAWINGS  
 JUB SHALL RETAIN ALL COMMON LAW, STATUTORY, COPYRIGHT AND PATENT RIGHTS IN THIS DRAWING. ANY REUSE WITHOUT WRITTEN CONSENT BY JUB WILL BE AT CLIENT'S SOLE RISK AND WITHOUT LIABILITY OR LEGAL EXPOSURE TO JUB.

NO.	REVISION	DESCRIPTION	BY	DATE

**KUNA ROAD SEWER EXTENSION  
 KUNA, ID**

**ABUTMENT REINFORCEMENT - 2**

FILE: 10-18-125-S8.0  
 JUB PROJ #: 10-18-125  
 DRAWN BY: ARB  
 DESIGN BY: ACC  
 CHECKED BY: BDS  
 ONE INCH  
 AT FULL SIZE IF NOT ONE  
 INCH SCALE ACCORDINGLY  
 LAST UPDATED: 1/8/2019  
 SHEET NUMBER:  
**S8.0**



J-U-B ENGINEERS, INC.

J-U-B ENGINEERS, INC.  
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Suite 201  
Boise, ID 83709-0944  
Phone: 208.376.7330  
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AGENCY REVIEW

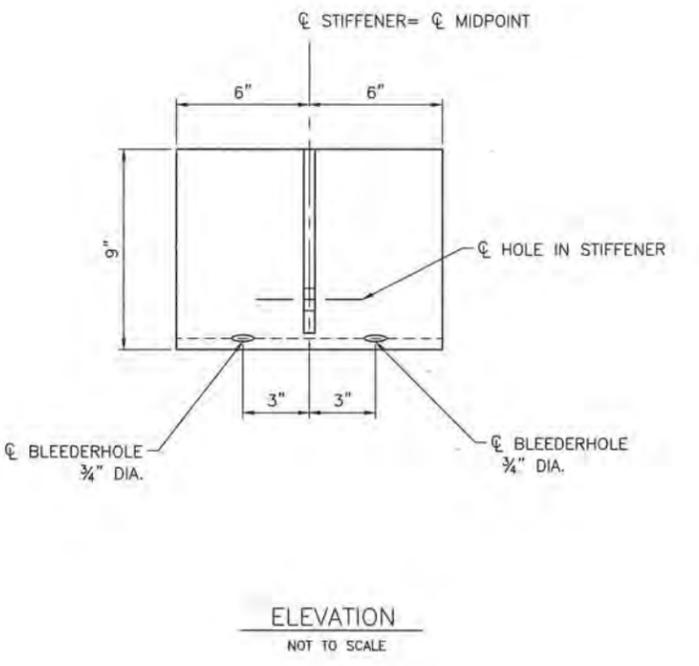
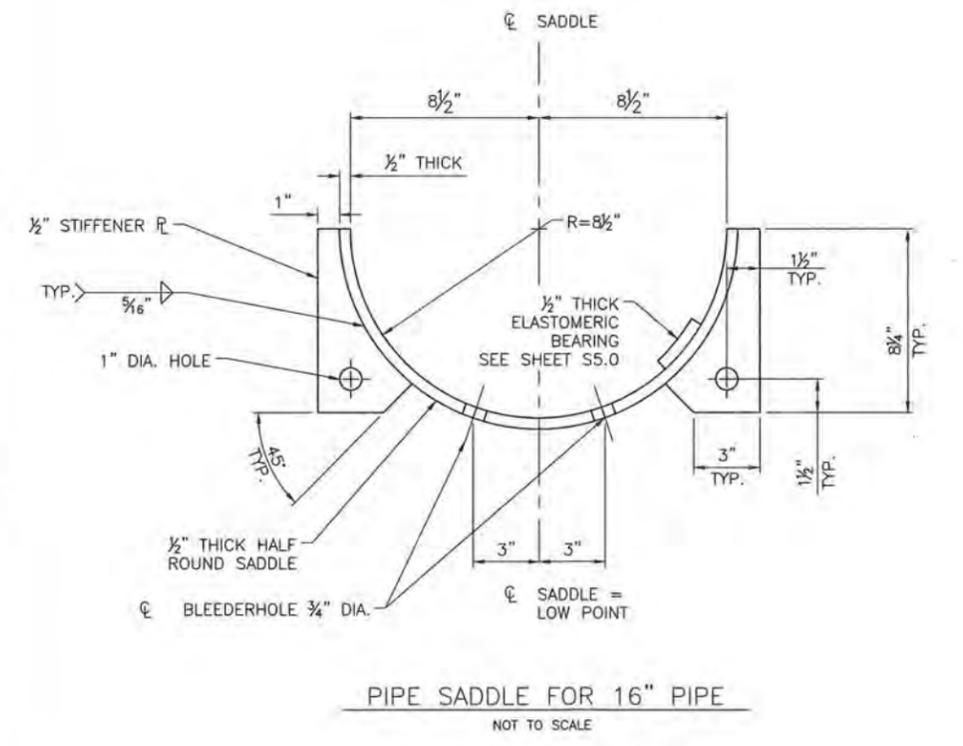
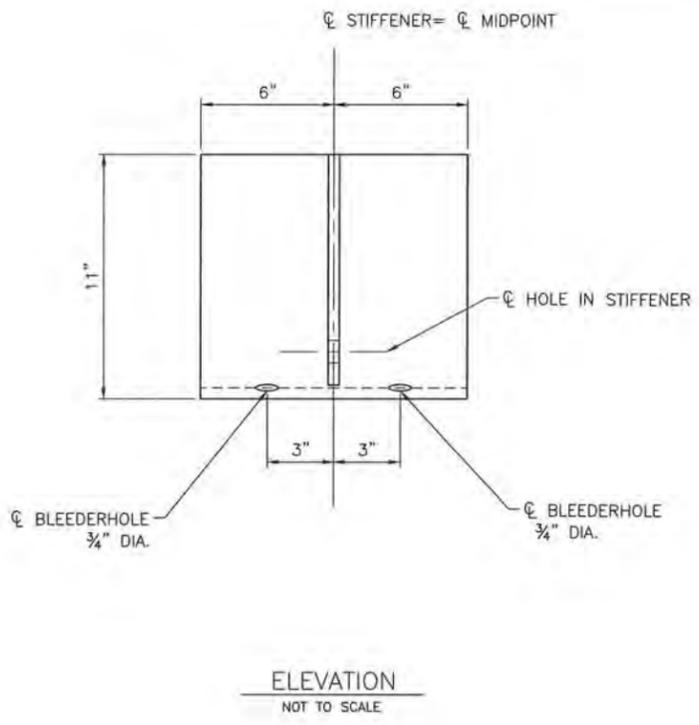
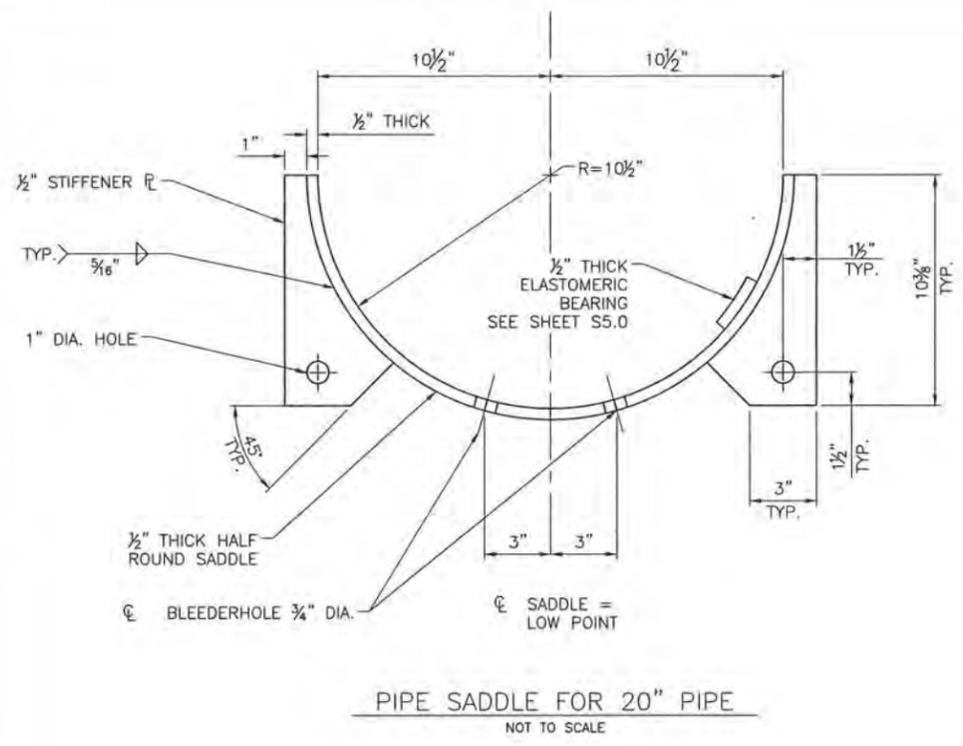
REUSE OF DRAWINGS  
J-U-B SHALL RETAIN ALL COMMON LAW, STATUTORY, COPYRIGHT AND OTHER RESERVED RIGHTS OF THESE DRAWINGS, AND THE SAME. ANY REUSE WITHOUT WRITTEN CONSENT BY J-U-B WILL BE AT CLIENT'S SOLE RISK AND WITHOUT LIABILITY OR LEGAL EXPOSURE TO J-U-B.

NO.	DESCRIPTION	BY	DATE

KUNA ROAD SEWER EXTENSION  
KUNA, ID

ABUTMENT - SUPPORT METAL SADDLE

FILE: 18_18_125_S9-0
AJB PROJ. #: 18-18-125
DRAWN BY: RT
DESIGN BY: AJO
CHECKED BY: BOS
SCALE: ONE INCH = 1 FT
AT FULL SIZE; IF NOT ONE INCH SCALE ACCORDINGLY
LAST UPDATED: 1/8/2019
SHEET NUMBER: <b>S9.0</b>



ABUTMENT SADDLE DETAILS

Plot Date: 1/9/2019 12:56 PM, Plotted By: Kurt Cox, Data Created: 1/9/2019, I:\DISSET\EST\PROJECTS\JUB\18-18-100 FALCON CREST SEWER GRADING\DWG\HRT\SCANA\_CROSSING\18\_125\_S9-0.DWG

**RESOLUTION NO. R19-2019  
CITY OF KUNA, IDAHO**

**A RESOLUTION OF THE CITY COUNCIL OF KUNA, IDAHO AUTHORIZING THE MAYOR TO EXECUTE ALL AGREEMENTS AND CONTRACTS AND THE CLERK TO ATTEST TO THE MAYOR'S SIGNATURE ON ALL SAID DOCUMENTS RELATED TO THE PURCHASE OF THE REAL PROPERTY WITH ADA COUNTY PARCEL NUMBERS S1326121090 AND S1326121015.**

**WHEREAS**, the city of Kuna, Idaho has been in negotiations to purchase the above-described real property; and

**WHEREAS**, the city has obtained a certified appraisal evidencing that the agreed purchase price is less than the fair market value of said real property; and

**WHEREAS**, the purchase of said property is in the best interests of the city and its need to expand its park system to accommodate the growth.

**NOW, THEREFORE, BE IT RESOLVED:**

Section 1. The Mayor is hereby authorized to execute all contracts and agreements related to the purchase of the property with Ada County Parcel Numbers of S1326121090 and S1326121015 on behalf of the City.

Section 2. The City Clerk is hereby authorized to attest to the Mayor's signatures on said contracts and agreements on behalf of the City.

**PASSED BY THE COUNCIL** of Kuna, Idaho this 5<sup>th</sup> day of March 2019.

**APPROVED BY THE MAYOR** of Kuna, Idaho this 5<sup>th</sup> day of March 2019.

\_\_\_\_\_  
Joe L. Stear, Mayor

ATTEST:

\_\_\_\_\_  
Chris Engels, City Clerk



ADDENDUM # 1 (All addendums shall be numbered sequentially.)

JULY 2018 EDITION  
Page 1 of 1

RE-11 ADDENDUM



THIS IS A LEGALLY BINDING CONTRACT, READ THE ENTIRE DOCUMENT, INCLUDING ANY ATTACHMENTS.  
IF YOU HAVE ANY QUESTIONS, CONSULT YOUR ATTORNEY AND/OR ACCOUNTANT BEFORE SIGNING.

1 Today's Date: 02/25/2019

2  
3 This is an **ADDENDUM** to the  Purchase and Sale Agreement  Other \_\_\_\_\_  
4 ("Addendum" means that the information below is added material for the agreement {such as lists or descriptions} and/or means the form is  
5 being used to change, correct or revise the agreement {such as modification, addition or deletion of a term}).

6  
7 **AGREEMENT DATED:** 02/01/2019 **ID #** 2119SW

8  
9 **ADDRESS:** 501 W. Shortline Street Kuna **ID**

10  
11 **BUYER(S):** City of Kuna

12  
13 **SELLER(S):** Jim Zamzow

14  
15 The undersigned parties hereby agree as follows:  
16 **1. See attached Exhibit A for legal description.**  
17 **2. Closing to be on or before March 8, 2019.**  
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48 To the extent the terms of this ADDENDUM modify or conflict with any provisions of the Purchase and Sale Agreement including all prior  
49 Addendums or Counter Offers, these terms shall control. **All other terms of the Purchase and Sale Agreement including all prior**  
50 **Addendums or Counter Offers not modified by this ADDENDUM shall remain the same.** Upon its execution by both parties, this agreement  
51 is made an integral part of the aforementioned Agreement.

52  
53 **BUYER:** \_\_\_\_\_ **Date:** \_\_\_\_\_

54  
55 **BUYER:** \_\_\_\_\_ **Date:** \_\_\_\_\_

56  
57 **SELLER:** \_\_\_\_\_ **Date:** \_\_\_\_\_

58  
59 **SELLER:** \_\_\_\_\_ **Date:** \_\_\_\_\_



ADDENDUM # 1 (All addendums shall be numbered sequentially.)

JULY 2018 EDITION  
Page 1 of 1

RE-11 ADDENDUM

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1 Today's Date: 02/25/2019

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5 being used to change, correct or revise the agreement {such as modification, addition or deletion of a term}).

6  
7 **AGREEMENT DATED:** 02/01/2019 **ID #** 2119SW

8  
9 **ADDRESS:** TBD South Aspen Willow Ave.

10  
11 **BUYER(S):** City of Kuna

12  
13 **SELLER(S):** Rick Zamzow

14  
15 The undersigned parties hereby agree as follows:  
16 **1. See attached Exhibit A for legal description.**  
17 **2. Closing to be on or before March 8, 2019.**  
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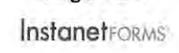
48 To the extent the terms of this ADDENDUM modify or conflict with any provisions of the Purchase and Sale Agreement including all prior  
49 Addendums or Counter Offers, these terms shall control. **All other terms of the Purchase and Sale Agreement including all prior**  
50 **Addendums or Counter Offers not modified by this ADDENDUM shall remain the same.** Upon its execution by both parties, this agreement  
51 is made an integral part of the aforementioned Agreement.  
52

53 **BUYER:** \_\_\_\_\_ **Date:** \_\_\_\_\_

54  
55 **BUYER:** \_\_\_\_\_ **Date:** \_\_\_\_\_

56  
57 **SELLER:** \_\_\_\_\_ **Date:** \_\_\_\_\_

58  
59 **SELLER:** \_\_\_\_\_ **Date:** \_\_\_\_\_





**IR IDAHO REALTORS®** **RE-15 COMPENSATION AGREEMENT WITH BUYER** JULY 2018 EDITION Page 1 of 2

THIS IS A LEGALLY BINDING CONTRACT, READ THE ENTIRE DOCUMENT, INCLUDING ANY ATTACHMENTS. IF YOU HAVE ANY QUESTIONS, CONSULT YOUR ATTORNEY AND/OR ACCOUNTANT BEFORE SIGNING.



1. BUYER Name(s) City of Kuna

BUYER desires to purchase, lease, or option the following real estate: Type of property:  
 Residential,  Residential Income  Commercial  Vacant Land  Custom Build Job  Other \_\_\_\_\_

Applicable area, City(s), County(s), etc. Kuna

Other Description: (ie, geographical area, price, etc.) TBD South Aspen Willow Ave. & 601 W. Shortline St.

and, whereas the undersigned parties desire to enter into this formal agreement expressing their agreement as to the payment/receipt of any real estate commission resulting from the purchase and/or lease of the above described type of property. **THIS IS NOT A BROKER REPRESENTATION AGREEMENT.** This is an agreement for compensation for services to a "customer" as defined by Idaho law. A Buyer or seller is not represented by a brokerage in a regulated real estate transaction unless the buyer or seller and the brokerage agree in a separate written document, to such representation. No type of agency representation may be assumed by a brokerage, buyer or seller or created orally or by implication.

2. AGENCY DISCLOSURE CONFIRMATION: The BUYER has received, has read, and understands the AGENCY DISCLOSURE BROCHURE prepared by the Idaho Real Estate Commission.

3. TERM OF AGREEMENT: The term of this Agreement shall commence on 02/01/2019 and will expire at 11:59 p.m. on date 03/15/2019, or upon closing of escrow of such property purchased through this agreement whichever is sooner.

4. COMPENSATION OF BROKER: Broker shall be compensated in the following ways: Check those that apply.

A. If the property is subject to a listing agreement with the Broker's company or with a cooperating Broker through the Multiple Listing Service (MLS) or otherwise, the Brokerage fee shall be the amount paid by the seller to the aforementioned Brokers but not less than 1.5 % of the gross selling price or \$ \_\_\_\_\_ dollars. BUYER agrees to pay to the Broker any difference between the amount received from the aforementioned Brokers and the stated minimum.

B. If the property is not subject to a Listing Agreement, such as a For Sale By Owner or a Custom Build Job, the BUYER agrees that the Broker will be paid a fee of not less than \_\_\_\_\_ % of selling price or \$ \_\_\_\_\_ dollars. The Broker shall first seek to obtain this fee through the transaction paid by the Seller. If the fee cannot be obtained through the Seller, the BUYER will be responsible for such fee stated above.

C. If the property is leased by BUYER, then BUYER agrees to pay a total brokerage fee of the greater of: \_\_\_\_\_ % of the total Base Rent OR \_\_\_\_\_ month's rent OR a one-time flat fee of \$ \_\_\_\_\_. The Broker shall first seek to obtain this fee through the transaction paid by the lessor. If the fee cannot be obtained through the lessor, then BUYER will be responsible for the fee. 'Base Rent' is defined as the specific minimum rent owed by BUYER to lessor each year. In addition, if the lease is a 'lease to own' contract, or if the BUYER exercises an option to purchase under the lease, then in addition to the compensation for lease procurement specified in this paragraph C, Broker shall also be entitled to any compensation for sale as enumerated in paragraphs A or B above.

D. Retainer Fee. BUYER will pay Broker a non-refundable retainer fee of \$ \_\_\_\_\_ due and payable upon signing of this Agreement. Retainer fee  shall  shall not be credited against any compensation set forth in paragraph A or B above.

E. Hourly rate. BUYER will pay Broker at the rate of \$ \_\_\_\_\_ per hour for the time spent by Broker pursuant to this Agreement to be paid when billed whether or not BUYER acquires or leases property. The fee  shall  shall not be credited against any compensation as set forth in paragraph A, B, or C above.

This compensation shall apply to transactions made for which BUYER enters into a contract during the original term of this Agreement or during any extension of such original or extended term, and shall also apply to transactions for which BUYER enters into a contract within \_\_\_\_\_ calendar days (ninety [90] if left blank) after this Agreement expires or is terminated, if the property acquired or leased by the BUYER was submitted in writing to the BUYER by the Broker pursuant to Section One hereof during the original term or extension of the term of this Agreement. The fee shall be paid at closing unless otherwise designated by the Broker in writing. The closing agent for this transaction is hereby authorized to pay the above mentioned compensation at closing.

5. OTHER TERMS AND CONDITIONS: \_\_\_\_\_

*initial date 2-26-19*

BUYER'S Initials JD ( ) Date: 2-20-19

BUYER'S NAME(S) \_\_\_\_\_

60 6. GENERAL PROVISIONS: In the event either party shall initiate any suit or action or appeal on any matter relating to this Agreement the  
61 defaulting party shall pay the prevailing party all damages and expenses resulting from the default, including all reasonable attorneys' fees  
62 and all court costs, including fees and cost upon appeal, and other expenses incurred by the prevailing party. This Agreement is made in  
63 accordance with and shall be interpreted and governed by the laws of the State of Idaho. All rights and obligations of the parties hereunder  
64 shall be binding upon and inure to the benefit of their heirs, personal representatives, successors and assigns.

65 7. WIRE TRANSFER WARNING: Electronic means of transferring money (i.e. ETF, wire transfer, electronic check, direct deposit, etc...) are  
66 subject to sophisticated cyber fraud attacks. These attacks are even more prevalent in real estate transactions due to the large sums  
67 of money being exchanged. BUYER is advised that Brokerage will not provide electronic transfer instructions by e-mail. Following money  
68 transfer instructions contained in an email from any party is inherently dangerous and should be avoided. BUYER agree that if BUYER  
69 use, or authorize the use of, electronic transfer of funds in a transaction they hereby hold the Brokerages, their agents, and the designated  
70 title and escrow company harmless from any and all claims arising out of inaccurate transfer instructions, fraudulent interception of said  
71 funds and/or any other damage relating to the conduct of third parties influencing the transfer process or stealing funds.

72  
73  
74 8. MERGER AND TIME: TIME IS OF THE ESSENCE IN THIS AGREEMENT The terms hereof, and any addendums or exhibits constitute  
75 the entire agreement and supersede all prior agreements, negotiations and discussions between parties. This agreement may be modified  
76 only by a written agreement signed by each of the parties.

77 9. REAL ESTATE BROKERAGE: \_\_\_\_\_

78		2-20-2019		2-20-2019
79	Buyer Signature	Date	Agent or Broker (on behalf of Brokerage) Signature	Date
80	_____	_____	_____	_____
81	Buyer Signature	Date	Brokerage Address	
82	_____	_____	_____	
83	Address		Brokerage Phone	Brokerage Fax
84	_____		_____	_____
85	Phone	Fax	Brokerage Email	
86	_____	_____	_____	
87	Email		Agent/Broker Email	
88	_____		_____	

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**RESOLUTION NO. R20-2019  
CITY OF KUNA, IDAHO**

**CITY OF KUNA IDAHO'S  
2019 TRANSPORTATION PRIORITY  
REQUEST TO THE ADA COUNTY HIGHWAY DISTRICT**

**A RESOLUTION OF THE CITY COUNCIL FOR KUNA, IDAHO APPROVING THE PROJECT PRIORITY LIST TITLED “CITY OF KUNA IDAHO'S TRANSPORTATION PRIORITY REQUESTS - 2019” AS THE OFFICIAL TRANSPORTATION PROJECT PRIORITY LIST FOR THE CITY OF KUNA, IDAHO FOR THE ADA COUNTY HIGHWAY DISTRICT; AUTHORIZING THE CITY CLERK TO TRANSMIT THE DOCUMENT TO ACHD; AND HEREBY REPEALING ALL PREVIOUS TRANSPORTATION PROJECT PRIORITY LISTS.**

**BE IT HEREBY RESOLVED** by the Mayor and Council of the City of Kuna, Idaho as follows:

Section 1. The project priority list entitled “City of Kuna Transportation Priority Requests - 2019” is hereby approved as the official transportation project priority list for Kuna, Idaho; which said project priority list is attached hereto, and made a part thereof;

Section 2. The City Clerk is hereby authorized to transmit the document to the Ada County Highway District;

Section 3. All previous Transportation Project Priority lists for the city of Kuna, Idaho are hereby repealed.

**PASSED BY THE COUNCIL** of Kuna, Idaho this 5<sup>th</sup> day of March 2019

**APPROVED BY THE MAYOR** of Kuna, Idaho this 5<sup>th</sup> day of March 2019

\_\_\_\_\_  
Joe L. Stear, Mayor

ATTEST:

\_\_\_\_\_  
Chris Engels, City Clerk



## City of Kuna

PO Box 13 ▪ 751 W 4th St. ▪ Kuna, Idaho 83634 ▪ Phone: 208.922.5274

### MEMORANDUM

Date: March 5, 2019  
To: Esteemed Mayor and Council Members  
From: Wendy I. Howell, PCED  
Re: Transportation Priority List, 2019

---

Before the City Council for consideration is Kuna's 2019 Transportation Priority List for approval. ACHD's Integrated Five-Year Work Plan is comprised of prioritized projects submitted by each City within ACHD's jurisdiction boundaries annually for integration into ACHD's Capital Project budget; that budget is then categorized by project cost elements and then organized into sub-programs with each focusing on a particular aspect of the county roadway system. They are as follows:

- Roadways - These projects include widening, rebuilding or preserving of arterial or collector roadways. It also includes the annual allocation to the Corridor Preservation program which allows ACHD to purchase right-of-way from new development on identified roadways prior to the year identified in the Integrated Five Year Work Plan or Capital Improvements Program.
- Intersections - Includes new, rebuild, and/or signalization intersection projects.
- Traffic - Includes projects aimed at signal upgrades, intelligent traffic systems (ITS), ACHD Commuteride projects, including park & ride lots.
- Maintenance - Includes annual programs for overlays, crack seal, cul-de-sac and scrub coat projects on roadways.
- Cooperative - Provides an annual allocation for developer projects that result from cooperative agreements between ACHD and other entities.
- Community Programs - Includes curb, gutter, pedestrian, school safety, neighborhood, bikeway or traffic calming projects not associated with a roadway widening project.

Upon approval, Kuna's transportation project priorities will be forwarded to Ada County Highway District staff for their consideration and additional vetting. Based on ACHD staff recommendations, projects which appear to serve the greatest overall public need are then approved by the ACHD Commission for inclusion into the Integrated Five-Year Work Plan.

**RESOLUTION NO. R\*\*  
CITY OF KUNA, IDAHO**

**CITY OF KUNA IDAHO'S  
2019 TRANSPORTATION PRIORITY  
REQUEST TO THE ADA COUNTY HIGHWAY DISTRICT**

**A RESOLUTION OF THE CITY COUNCIL FOR KUNA, IDAHO APPROVING THE PROJECT PRIORITY LIST TITLED “CITY OF KUNA IDAHO'S TRANSPORTATION PRIORITY REQUESTS- 2019” AS THE OFFICIAL TRANSPORTATION PROJECT PRIORITY LIST FOR THE CITY OF KUNA, IDAHO FOR THE ADA COUNTY HIGHWAY DISTRICT; AUTHORIZING THE CITY CLERK TO TRANSMIT THE DOCUMENT TO ACHD; AND HEREBY REPEALING ALL PREVIOUS TRANSPORTATION PROJECT PRIORITY LISTS.**

**BE IT HEREBY RESOLVED** by the Mayor and Council of the City of Kuna, Idaho as follows:

1. The project priority list entitled “City of Kuna Transportation Priority Requests- 2019” is hereby approved as the official transportation project priority list for Kuna, Idaho; which said project priority list is attached hereto, and made a part thereof;
2. The City Clerk is hereby authorized to transmit the document to the Ada County Highway District;
3. All previous Transportation Project Priority lists for the city of Kuna, Idaho are hereby repealed.

**PASSED BY THE COUNCIL** of Kuna, Idaho this 5<sup>th</sup> day of March 2019

**APPROVED BY THE MAYOR** of Kuna, Idaho this 5<sup>th</sup> day of March 2019

\_\_\_\_\_  
Joe L. Stear, Mayor

ATTEST:

\_\_\_\_\_  
Chris Engels, City Clerk

**City of Kuna Request Status and 2019 Prioritization**

Attachment 2

*Instructions - Please insert your agency's ranking for each project in the column "2019 City Priority Ranking". Please add any additional comments or information in the last column. The remaining columns are for your information to assist you in preparing your 2019 requests. Additional lines can be added to input new project requests as needed. Please contact Ryan Head at (208) 387-6234 with any questions.*

2019 City Priority Ranking	2018 City Priority Ranking	GIS #	ACHD Project Name (City Project Name)	Project Description (Request Details)	Current Programming Status						Additional Project Specific/Miscellaneous City Input
					In IFYWP	DSN YR	ROW YR	CN YR	Total Cost in Millions	Programming Notes	
<b>Road &amp; Intersection</b>											
	1		SH 69 (Avalon Rd) and Kay St	Install a signal and a pedestrian crossing at the intersection of SH69 and Kay St.							The Post Office is moving by summer to the complex that access the Kay St/SH 69 intersection. According to the developer of Merlin Pointe Subdivision, the TIS shows that Kay St. south bound has a LOS F. This is a priority for the Mayor and Council to get completed.
	2	IN211-01	Linder Rd and Deer Flat Rd	Federal aid project to improve intersection of Linder Rd and Deer Flat Rd, including curb, gutter, sidewalk and bike lanes. Right-of-way funding will advance to a year prior to construction as funds become available.	Yes	Done	2019	2020	\$4.15	To be built in FY2020.	
	3	MA215-05	Swan Falls Railroad Overpass (Swan Falls Rd and Shortline St)	Install a new bridge over Indian Creek and Union Pacific Railroad on Swan Falls Rd in accordance with the Kuna Crossing Feasibility and Implementation Plan.	No				\$18.50	Included in ACHD's Master Street Map. Project to occur in accordance with the Kuna Crossing Feasibility and Implementation Plan.	
	4	IN205-44	Ten Mile Rd and Deer Flat Rd	Install an expandable single-lane roundabout in accordance with the Ten Mile Corridor Intersection Analysis and the 2016 CIP.	Yes	PD	PD	PD	\$0.67	Added to IFYWP. Project to advance through subsequent updates.	
	5		Ten Mile Rd and Lake Hazel Rd	Widen intersection to 3/4 lanes on Ten Mile and 3/4 lanes on Lake Hazel in accordance with the 2016 CIP. (City request for a roundabout will be evaluated during the design phased of this project.)	Yes	2023	PD	PD	\$2.71	To begin design in FY2023.	
	6	IN205-127	Ten Mile Rd and Columbia Rd	Install a single-lane expandable roundabout in accordance with the Ten Mile Corridor Intersection Analysis.	Yes	2023	PD	PD	\$1.34	To begin design in FY2023.	
	7		Ten Mile Rd, Hubbard Rd / Columbia Rd	Widen Ten Mile Rd to 5 lanes with curb, gutter, sidewalk, and bike lanes in accordance with the Master Street Map. Construct a roundabout at Ten Mile Rd and Hubbard Rd	No				TBD	Defer. Operating at a LOS D. Not a 2016 CIP project.	
	8		Linder Rd and Columbia Rd	Install a roundabout for continuous traffic flow.	No				TBD	Not a 2016 CIP project. ACHD Traffic Engineering to evaluate intersection operations.	
	9		Kuna Rd / S Cloverdale Rd	Design and construct a wider intersection, turn lanes, and street lights. Determine if a signal is warranted due to traffic flow at the intersection.							SC Beef's trucks (GVW of 80,000 or less) routinely use Cloverdale Rd and Falcon Crest's development will necessitate improvements. A sewer trunk line will be installed in that area in the near future, increasing development opportunities.
	10		Kuna Mora Rd, Cloverdale Rd / Cole Rd	Widen Kuna Mora Rd to 5/7 lanes with curb, gutter, sidewalk, and bike lanes in accordance with the Master Street Map. Intersection improvements to effectively handle freight trucks (GVW of 80,000 lbs or less)	No				TBD	Defer. Operating at a LOS D. Not a 2016 CIP project.	

2019 City Priority Ranking	2018 City Priority Ranking	GIS #	ACHD Project Name (City Project Name)	Project Description (Request Details)	Current Programming Status					Additional Project Specific/Miscellaneous City Input	
					In IFYWP	DSN YR	ROW YR	CN YR	Total Cost in Millions		Programming Notes
	11		Deer Flat Rd and Kay St	Install a roundabout with pedestrian crossing to address continued traffic flow add a left turn lane from Kay onto Deer Flat.	No				TBD	North bound right-turn lane to be installed in fall 2018. ACHD Traffic Engineering to reevaluate intersection operations.	
	12		Deer Flat Rd, Linder Rd to Hwy 69	Design and construct Deer Flat Rd to 5 lanes with curb, gutter, sidewalk between Linder Rd and Hwy 69.							Deer Flat is operating at LOS E during PM Peak Hour, between Linder Rd and Hwy 69.
	13		Kuna Mora Rd / Cole Rd	Improve Kuna Mora Rd and Cole Rd intersection to allow a left turn lane.							The City of Kuna expects to see this area develop with additional industrial activity in the next several years. CS Beef Facility's truck traffic (GVW of 80,000 lbs. or less) has increased the use of this intersection. An immediate fix for this intersection is
	14		Linder Rd / Avalon Rd / Swan Falls Rd	Construct a multi-lane roundabout at intersection per Downtown Corridor Plan (Table 18, I-2)							This intersection is operating at LOS F during PM Peak Hour.
	15		Kuna Rd / S Eagle Rd	Construct wider intersection, turn lanes, and determine if a signal is necessary for traffic flow.							
	16		Kuna Mora Rd	Improve Kuna Mora Rd where it currently curves around a canal bank, making the road easier to move truck traffic and enable travel from the Blacks Creek Exit through to Canyon County.							
	17		Cloverdale Rd / I-84	Design and construct an ingress/egress interchange from Cloverdale Rd and I-84							There is significant truck traffic that uses Cloverdale Rd coming from the CS Beef Facility off of Kuna Mora and Cole Rds. Kuna residents also frequent this route into Boise. The expansion of Falcon Crest and surrounding areas, the traffic will be increasing.
<b>Community Programs</b>											
	1		Main St, Ave C / Ave A	Streetscape improvements, including road reconstruction, wider sidewalks, on-street parking, drainage, and bike markings in accordance with Kuna Downtown Corridor Plan.	No				TBD	ACHD to support a City-led effort. As project moves forward, ACHD to evaluate financial partnership for roadway maintenance upon specific request.	
	2	CM213-64	Linder Rd, Main St / Deer Flat Rd	Complete curb, gutter, detached sidewalk and bike lanes on both sides of Linder Rd, between 4th St (Linder/Main/3rd project limits) to Deer Flat Rd. Project will construct a Rectangular Rapid Flashing Beacon at Porter St.	Yes	2020	2021	2023	\$1.75	To be built in FY2023.	
	3	CM214-10 and IN211-01	Deer Flat Rd, Linder Rd / Kay Ave	Install sidewalk.	Yes	Done	2019	2020	\$4.15	To be built in FY2020 as part of Linder Rd and Deer Flat intersection project.	
	4	CM217-27	Deer Flat Rd and School St Pedestrian Crossing	Install an enhanced pedestrian crossing on Deer Flat Rd at School St.	Yes	2019	2019	2020	\$0.18	To be built in FY2020.	
	5	CM217-09	Ave C, Main St / 4th St	Construct curb, gutter, and sidewalk on both sides of Ave C, between Main St and 4th St in accordance with the Kuna Downtown Corridor Plan.	Yes	2021	2021	2022	\$0.15	To be built in FY2022.	

2019 City Priority Ranking	2018 City Priority Ranking	GIS #	ACHD Project Name (City Project Name)	Project Description (Request Details)	Current Programming Status						Additional Project Specific/Miscellaneous City Input
					In IFYWP	DSN YR	ROW YR	CN YR	Total Cost in Millions	Programming Notes	
	6	CM217-10	Ave D, Main St / 4th St	Construct curb, gutter, and sidewalk on both sides of Ave D, between Main St and 4th St in accordance with the Kuna Downtown Corridor Plan.	Yes	2023	2023	PD	\$0.16	Design to begin in FY2023.	
	7	IN211-01	Deer Flat Rd, Linder Rd / 1,000' E/O Linder Rd	Construct a sidewalk on the south side of Deer Flat Rd to provide a contiguous walkway.	Yes	Done	2019	2020	\$4.15	To be built in FY2020 as part of Linder Rd and Deer Flat intersection project.	
	8	CM217-52	Ten Mile Rd and Crenshaw St	Install an enhanced pedestrian crossing and school zone on Ten Mile Rd at Crenshaw St.	Yes	2019	2020	2021	\$0.17	To be built in FY2021.	
	9		Swan Falls Rd and Indian Creek Pathway Pedestrian Crossing	Install an enhanced pedestrian crossing on Swan Falls Rd at the Indian Creek Pathway.	0				\$0.14	Scoped. To be evaluated and prioritized for possible inclusion into future IFYWP	The greenbelt is planned to continue east since the City now owns the site that previously had blocked further extension.
	10	CM217-11 and CM217-12	Linder Ave/Avalon St, 2nd St / Orchard St	Install an interim asphalt pathway with extruded curb on the north side of Linder Ave/Avalon St within existing right-of-way. Includes an enhanced pedestrian crossing (rectangular rapid flashing beacon) and curb ramps on Avalon St at Orchard (west side).	No				\$0.53	Scoped. To be evaluated and prioritized for possible inclusion into future IFYWP updates.	
	11		Avenue B, south of Main St	185' of sidewalk on the west side of Ave. B, to connect senior center to downtown							
	12		Kuna Middle School Zone	Install flashing school zone beacons near Kuna Middle School.	No				TBD	Scoped in 2018. Final report pending.	
	13		Ten Mile Rd and Deer Flat Rd Pedestrian Crossing	Install an enhanced pedestrian crossing on Ten Mile Rd at Deer Flat Rd.	No				TBD	An enhanced pedestrian crossing is not appropriate in conditions where all traffic must already stop. Any additional enhancement would occur in connection with intersection improvements.	
	14	CM217-08	Ave B, Main St / 4th St	Construct curb, gutter, and sidewalk on both sides of Ave B, between Main St and 4th St in accordance with the Kuna Downtown Corridor Plan.	No				\$0.10	Scoped. To be evaluated and prioritized for possible inclusion into future IFYWP	
	15	CM217-06	Ave A, Main St / 4th St	Construct curb, gutter, and sidewalk on both sides of Ave A, between Main St and 4th St in accordance with the Kuna Downtown Corridor Plan.	No				\$0.15	Scoped. To be evaluated and prioritized for possible inclusion into future IFYWP updates.	
	16	CM217-07	Ave B, 2nd St / Main St	Construct curb, gutter, and sidewalk on both sides of Ave B, between 2nd St and Main St in accordance with the Kuna Downtown Corridor Plan.	No				\$0.16	Scoped. To be evaluated and prioritized for possible inclusion into future IFYWP updates.	
	17	CM217-05	Ave C, 2nd St / Main St	Construct curb, gutter, and sidewalk on both sides of Ave C, between 2nd St and Main St in accordance with the Kuna Downtown Corridor Plan.	No				\$0.09	Scoped. To be evaluated and prioritized for possible inclusion into future IFYWP updates.	
	18		4th St, School St / Linder Ave	Concept design to revitalize evaluate infrastructure, pedestrian, bicycle, lighting and utility improvements.	No				TBD	ACHD to evaluate through future budget development.	
	19		Avalon St, Ten Mile Rd / School Ave	Construct sidewalk on the south side of Avalon St from Ten Mile Rd to School Ave.	No				TBD	Scoped in 2018. Final report pending.	
	20		Deer Flat Rd, Kay St / SH 69 (Meridian Rd)	Install sidewalk on the south side of Deer Flat Rd from Kay St to SH 69 (Meridian Rd).	No				TBD	Scoped in 2018. Final report pending.	

2019 City Priority Ranking	2018 City Priority Ranking	GIS #	ACHD Project Name (City Project Name)	Project Description (Request Details)	Current Programming Status					Additional Project Specific/Miscellaneous City Input	
					In IFYWP	DSN YR	ROW YR	CN YR	Total Cost in Millions		Programming Notes
	21		Ave B and 2nd St Parking Lot	2nd St downtown parking lot design including solar energy options, lighting, electric car charging, impervious asphalt, landscaped islands, and covered area for farmer's market	No				TBD	ACHD to support a City-led effort.	
	22		2nd St, Ave D / Avalon St	Install bike lanes in accordance with the Kuna Downtown Corridor Plan.	No				TBD	Current low traffic volumes make striping a bike lane not appropriate at this time. ACHD to reevaluate as conditions change in the area.	
	23	N/A	Kay St, Deer Flat Rd / Boise St	Construct curb, gutter, sidewalk and on-street parking on the east side of Kay St, between Deer Flat Rd and Boise St.	No				N/A	East side sidewalk to be built through development. Complete sidewalk exists on the west side of Kay St.	
	24	RS203-07	Park and Ride Lot	Park and ride lot. Location to be determined	No				TBD	ACHD to support a City-led effort.	
	25	N/A	Greenbelt Extension (West)	Extend greenbelt west to Deer Flat Rd	No				N/A	ACHD to support a City-led effort.	
	26	N/A	Greenbelt Extension (East)	Extend greenbelt east to Stroebel Rd	No				N/A	ACHD to support a City-led effort.	



# City of Kuna

## City Council Memo

P.O. Box 13  
Kuna, ID 83634  
Phone: (208) 922-5274  
Fax: (208) 922-5989  
Kunacity.id.gov

To: **Kuna City Council**

Case Number: 18-20-FP (Final Plat) – Ashton Estates Subdivision No. 2

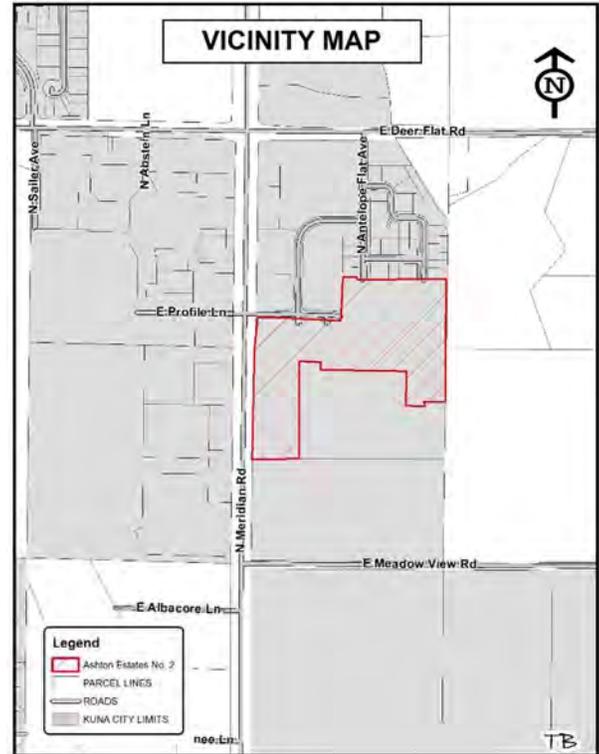
Location: SE corner of Meridian Road and Deer Flat Road, Kuna, Idaho 83634

Planner: Troy Behunin, Planner III

Meeting Date: March 5, 2019

Applicant/  
Owner: Don Newell  
SDN, LLC  
PO Box 1939  
Eagle, ID 83616  
[ashton.homes@hotmail.com](mailto:ashton.homes@hotmail.com)

Representative: KM Engineering, LLP  
9233 W State St  
Boise, ID 83714  
208-639-6939  
[kgrabo@kmengllp.com](mailto:kgrabo@kmengllp.com)



### A. General Project Facts, Staff Analysis:

1. The applicant is requesting final plat approval for *Ashton Estates Subdivision No. 2* which contains fifty-one (51) residential building lots, thirteen (13) common lots, four (4) commercial building lots, over approximately 17.76 acres (Assessor Parcel No. [APN] S1419223155).
2. In accordance with Kuna City Code (KCC) Title 6 Subdivision Regulations, this application seeks final plat approval for the *Ashton Estates Subdivision No. 2*. The proposed final plat is in substantial conformance with the approved preliminary plat.

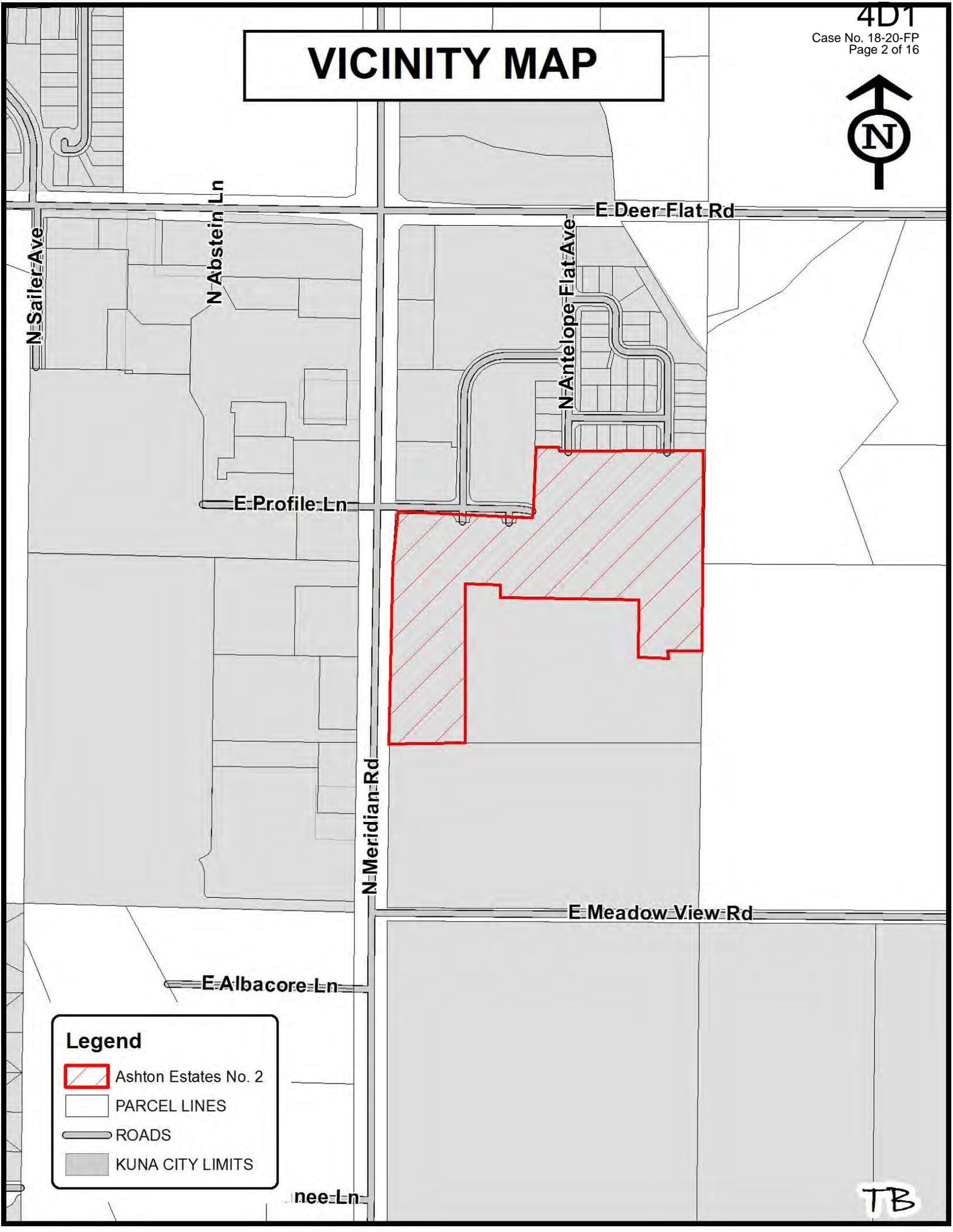
### B. Applicable Standards:

1. Kuna City Code Title 6 Subdivision Regulations.
2. City of Kuna Comprehensive Plan and Future Land Use Map.
3. Idaho Code, Title 50, Chapter 13, Plats and Vacations.

### C. Staff Analysis:

1. Staff has determined that the proposed final plat for the *Ashton Estates Subdivision No. 2* is in substantial conformance with the approved preliminary plat.
2. Applicant shall correct any technical items and make any requested changes to bring the final plat into conformance as recommended by Kuna Public Works Staff.
3. Applicant shall annex the property's water rights into the Kuna Municipal Irrigation District prior to requesting Kuna City Engineer's signature on the final plat Mylar.
4. Applicant shall secure all signatures on the final plat check-off list prior to requesting Kuna City Engineer's signature on the final plat Mylar.

# VICINITY MAP



**Legend**

-  Ashton Estates No. 2
-  PARCEL LINES
-  ROADS
-  KUNA CITY LIMITS

TB

October 25, 2018  
Project No.: 18-053

Mr. Troy Behunin  
Planning & Zoning Department  
City of Kuna  
751 West 4<sup>th</sup> Street  
Kuna, ID 83634

**RE: Ashton Estates Subdivision No. 2 – Kuna, ID  
Final Plat Application**

Dear Mr. Behunin:

On behalf of SDN, LLC, we are pleased to submit the attached application and required supplements for the final plat of Ashton Estates Subdivision No. 2.

The final plat for this phase encompasses approximately 17.76 acres of the overall site and includes 51 buildable single-family residential lots, 4 buildable commercial lots, and 13 common lots for a total of 68 lots. Access for this phase is proposed via stub streets constructed as a part of Phase 1. The construction plans for this phase have been submitted to the applicable regulatory agencies for review, including the City Engineer.

To our knowledge, the final plat for Ashton Estates Subdivision No. 2 is in conformance with the approved preliminary plat and meets the requirements and conditions of approval thereof.

Should you have questions or require further information in order to process this application, please let me know as soon as possible.

Sincerely,  
KM Engineering, LLP



Kirsti Grabo  
Development Coordinator

cc: SDN, LLC



City of Kuna  
Planning & Zoning  
Department  
P.O. Box 13  
Kuna, Idaho 83634  
208.922.5274  
Fax: 208.922.5989  
Website: www.kunacity.id.gov

### Commission & Council Review Application

Note: Engineering fees shall be paid by the applicant if required.

\*Please submit the appropriate checklist (s) with application

**Type of Review (check all that apply):**

- Annexation
- Appeal
- Comprehensive Plan Amendment
- Design Review
- Development Agreement
- Final Planned Unit Development
- Final Plat
- Lot Line Adjustment
- Lot Split
- Planned Unit Development
- Preliminary Plat
- Rezone
- Special Use
- Temporary Business
- Vacation
- Variance

For Office Use Only	
File Number (s)	
Project name	
Date Received	
Date Accepted/Complete	
Cross Reference Files	
Commission Hearing Date	
City Council Hearing Date	

02.11.2019  
 3.5.19  
 M/A  
 Ashton Est.  
 No. 2  
 18-20-FP

### Contact/Applicant Information

Owners of Record: <u>SDN, LLC</u>	Phone Number: <u>208.404.2161</u>
Address: <u>PO Box 1939</u>	E-Mail: <u>ashton.homes@hotmail.com</u>
City, State, Zip: <u>Eagle, Idaho 83616</u>	Fax #: _____
Applicant (Developer): <u>same</u>	Phone Number: _____
Address: _____	E-Mail: _____
City, State, Zip: _____	Fax #: _____
Engineer/Representative: <u>Kirsti Grabo</u>	Phone Number: <u>208.639.6939</u>
Address: <u>9233 W State Street</u>	E-Mail: <u>kgrabo@kmengllp.com</u>
City, State, Zip: <u>Boise, Idaho 83714</u>	Fax #: <u>208.639.6930</u>

### Subject Property Information

Site Address: <u>North Meridian Road</u>	
Site Location (Cross Streets): <u>SE Corner of Meridian &amp; Deer Flat</u>	
Parcel Number (s): <u>S1419223155</u>	
Section, Township, Range: <u>Section 19, T2N, R1E</u>	
Property size : <u>± 17.76 acres</u>	
Current land use: <u>agriculture</u>	Proposed land use: <u>mixed use</u>
Current zoning district: <u>R-6</u>	Proposed zoning district: <u>no change</u>

Exhibit  
 A2a

**Project Description**

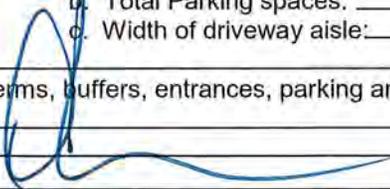
Project / subdivision name: <u>Ashton Estates Subdivision No. 2</u>
General description of proposed project / request: <u>68 total lots consisting of 51 single - family residential lots, 4 commercial lots, and 13 common lots</u>
Type of use proposed (check all that apply):
<input checked="" type="checkbox"/> Residential <u>single-family homes</u>
<input checked="" type="checkbox"/> Commercial _____
<input type="checkbox"/> Office _____
<input type="checkbox"/> Industrial _____
<input type="checkbox"/> Other _____
Amenities provided with this development (if applicable): _____

**Residential Project Summary (if applicable)**

Are there existing buildings? <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No
Please describe the existing buildings: _____
Any existing buildings to remain? <input type="checkbox"/> Yes <input type="checkbox"/> No
Number of residential units: <u>51</u> Number of building lots: <u>51</u>
Number of common and/or other lots: <u>13</u>
Type of dwellings proposed:
<input checked="" type="checkbox"/> Single-Family _____
<input type="checkbox"/> Townhouses _____
<input type="checkbox"/> Duplexes _____
<input type="checkbox"/> Multi-Family _____
<input type="checkbox"/> Other _____
Minimum Square footage of structure (s): _____
Gross density (DU/acre-total property): <u>3.9</u> Net density (DU/acre-excluding roads): <u>5.9</u>
Percentage of open space provided: <u>13.2%</u> Acreage of open space: <u>1.72</u>
Type of open space provided (i.e. landscaping, public, common, etc.): <u>landscape &amp; common lots</u>

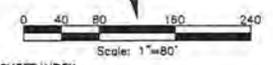
**Non-Residential Project Summary (if applicable)**

Number of building lots: <u>4</u> Other lots: _____
Gross floor area square footage: _____ Existing (if applicable): _____
Hours of operation (days & hours): _____ Building height: _____
Total number of employees: _____ Max. number of employees at one time: _____
Number and ages of students/children: _____ Seating capacity: _____
Fencing type, size & location (proposed or existing to remain): _____
Proposed Parking:
a. Handicapped spaces: _____ Dimensions: _____
b. Total Parking spaces: _____ Dimensions: _____
c. Width of driveway aisle: _____
Proposed Lighting: _____
Proposed Landscaping (berms, buffers, entrances, parking areas, common areas, etc.): <u>buffers</u>

Applicant's Signature:  Date: 10/25/18

PLAT OF  
**ASHTON ESTATES SUBDIVISION No. 2**  
A PARCEL OF LAND SITUATED IN A PORTION OF GOVERNMENT LOTS 1 AND 2  
OF SECTION 19, TOWNSHIP 2 NORTH, RANGE 1 EAST, BOISE MERIDIAN,  
CITY OF KUNA, ADA COUNTY, IDAHO.

2018



**SHEET INDEX**

- SHEET 1 - SUBDIVISION PLAT MAP
- SHEET 2 - NOTES, DETAILS AND LINE AND CURVE TABLES
- SHEET 3 - CERTIFICATE OF OWNERS
- SHEET 4 - CERTIFICATES AND APPROVALS

**LEGEND**

- SUBDIVISION BOUNDARY LINE
- SUBDIVISION LOT LINE
- - - SECTION LINE
- - - ADJACENT BOUNDARY LINE
- - - ROAD CENTERLINE
- - - EASEMENT LINE AS NOTED
- - - SURVEY TIE LINE
- ⊙ FOUND BRASS CAP MONUMENT
- ⊙ FOUND ALUMINUM CAP MONUMENT
- ⊙ FOUND 5/8" REBAR WITH PLASTIC CAP MARKED "KSK 16662" UNLESS OTHERWISE NOTED
- ⊙ FOUND 1/2" REBAR WITH PLASTIC CAP MARKED "KSK 16662" UNLESS OTHERWISE NOTED
- ⊙ SET 5/8" X 24" REBAR WITH PLASTIC CAP MARKED "KSK 16662"
- ⊙ SET BRASS PILE MONUMENT WITH MAGNET MARKED "KSK 16662"
- ⊙ SET 1/2" X 24" REBAR WITH PLASTIC CAP MARKED "KSK 16662"
- ⊙ CALCULATED POINT
- ⊙ LOT NUMBER
- ⊙ BLOCK NUMBER
- 1 ADJACENT SUBDIVISION LOT NUMBER
- ▨ CITY OF KUNA PRESSURIZED IRRIGATION EASEMENT. SEE NOTE 18
- ▨ EASEMENT AREA AS DEFINED IN DETAILS ON SHEET 2

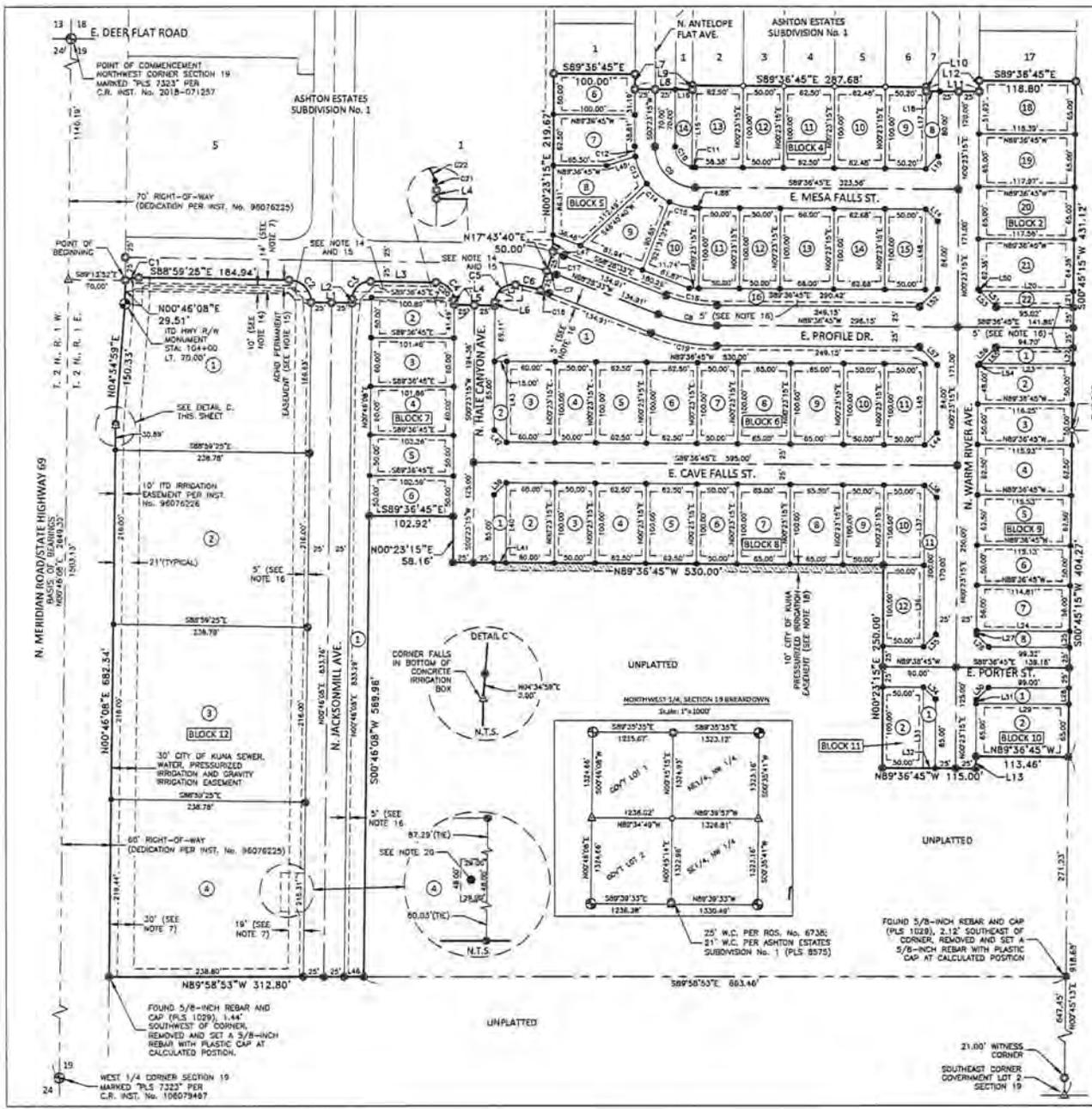
**REFERENCES**

- R1. RECORD OF SURVEY No. 2499, RECORDS OF ADA COUNTY, IDAHO.
- R2. RECORD OF SURVEY No. 8887, RECORDS OF ADA COUNTY, IDAHO.
- R3. RECORD OF SURVEY No. 6738, RECORDS OF ADA COUNTY, IDAHO.
- R4. FEDERAL AID PROJECT STP-378(101) RECORDS OF IDAHO TRANSPORTATION DEPARTMENT, DISTRICT 3.
- R5. PLAT OF PROLEY'S FAMILY CENTER SUBDIVISION No. 1, BOOK 107 OF PLATS AT PAGES 14,820-14,822, RECORDS OF ADA COUNTY, IDAHO.
- R6. PLAT OF ASHTON ESTATES SUBDIVISION No. 1, BOOK 114 OF PLATS AT PAGES 17,055-17,059, RECORDS OF ADA COUNTY, IDAHO.



DEVELOPER  
**SDN, LLC**  
BOISE, IDAHO

16-23-2018



A4  
EXHIBIT

PLAT OF  
ASHTON ESTATES SUBDIVISION No. 2

CURVE TABLE					
CURVE	RADIUS	LENGTH	DELTA	CHORD BRG	CHORD
C1	38.50'	16.89'	24°35'20"	N78°42'55"E	16.82'
C2	27.50'	43.08'	89°45'33"	S44°06'38"E	38.81'
C3	22.50'	35.44'	90°14'27"	N45°53'22"E	31.89'
C4	22.50'	35.10'	89°22'40"	S44°18'05"E	31.65'
C5	22.50'	38.11'	97°02'57"	N48°54'43"E	33.72'
C6	175.00'	32.06'	10°29'45"	S77°18'56"E	32.01'
C7	200.00'	12.63'	3°37'03"	N70°17'04"W	12.63'
C8	200.00'	73.78'	21°08'12"	N79°02'39"W	73.36'
C9	50.00'	78.54'	90°00'00"	S44°36'45"E	70.71'
C10	25.00'	35.13'	80°30'36"	S39°52'03"E	32.31'
C11	25.00'	4.14'	9°29'24"	S84°52'03"E	4.14'
C12	75.00'	12.43'	9°29'53"	S42°14'1"E	12.42'
C13	75.00'	36.35'	27°46'09"	S22°59'42"E	36.00'
C14	75.00'	36.19'	27°38'54"	S50°42'13"E	35.84'
C15	75.00'	32.84'	25°05'05"	S77°04'13"E	32.57'
C16	175.00'	64.56'	21°08'10"	S79°02'38"E	64.19'
C17	225.00'	14.28'	3°38'14"	N70°17'40"W	14.28'
C18	175.00'	10.97'	3°35'31"	N70°16'18"W	10.97'
C19	225.00'	83.00'	21°08'10"	S79°02'38"E	82.53'
C20	22.50'	31.82'	81°02'16"	S48°28'17"E	29.24'
C21	22.50'	3.28'	8°20'24"	N3°48'57"W	3.27'

CURVE TABLE					
CURVE	RADIUS	LENGTH	DELTA	CHORD BRG	CHORD
C22	22.50'	31.82'	81°02'16"	S48°28'17"E	29.24'
C23	39.50'	5.40'	7°49'51"	N70°20'11"E	5.39'
C24	39.50'	11.55'	16°45'29"	N82°37'51"E	11.51'
C25	27.50'	44.08'	91°50'35"	S43°04'07"E	39.51'
C26	22.50'	35.44'	90°14'27"	N45°53'22"E	31.89'
C27	22.50'	23.09'	58°48'32"	S59°38'09"E	22.09'
C28	22.50'	24.47'	62°18'02"	N66°17'11"E	23.28'
C29	175.00'	32.43'	10°37'07"	S77°15'15"E	32.39'
C30	225.00'	0.38'	0°05'44"	N72°03'55"W	0.38'
C31	230.00'	1.94'	0°28'56"	S72°15'51"E	1.94'
C32	39.50'	8.53'	12°22'09"	S72°38'20"W	8.51'
C33	27.50'	3.50'	7°17'15"	S2°52'29"E	3.50'
C34	22.50'	24.44'	82°14'49"	S57°52'01"E	23.26'
C35	175.00'	23.73'	7°46'06"	N75°57'06"W	23.71'

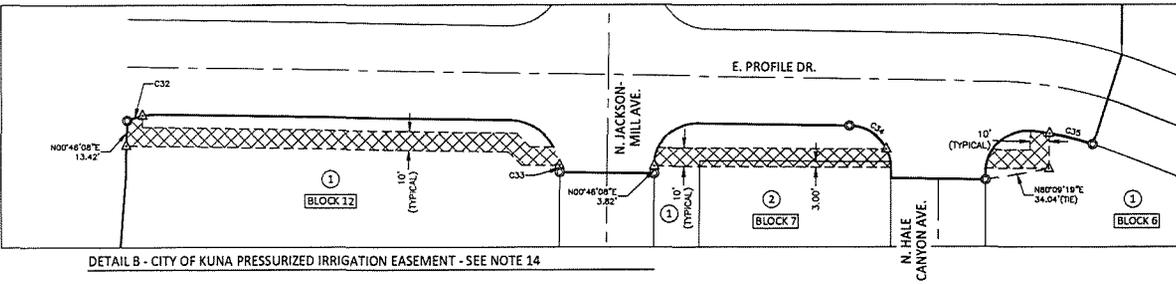
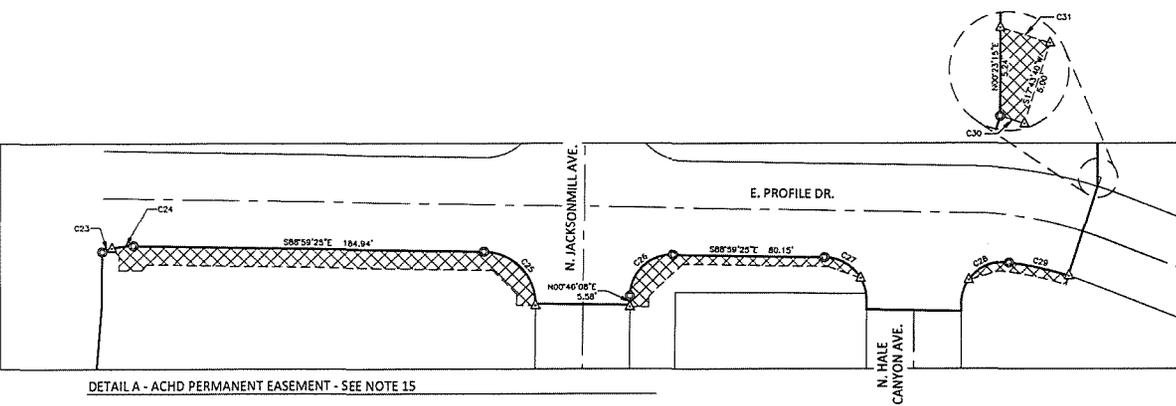
LINE TABLE		
LINE#	LENGTH	DIRECTION
L1	50.00	S89°13'52"E
L2	4.58	N0°46'08"E
L3	80.15	S88°59'25"E
L4	5.24	S0°23'15"W
L5	50.00	S88°59'25"E
L6	3.60	S0°23'15"W
L7	18.81	S0°23'15"W
L8	70.88	S89°36'45"E
L9	5.00	N0°23'15"E
L10	5.00	S0°23'15"W
L11	65.00	S89°36'45"E
L12	13.38	N0°23'15"E
L13	15.00	S0°23'15"W
L14	21.21	S44°36'45"E
L15	94.88	N0°23'15"E
L16	20.88	S89°36'45"E
L17	95.00	N0°23'15"E
L18	15.00	S89°36'45"E
L19	21.21	S45°23'15"W

LINE TABLE		
LINE#	LENGTH	DIRECTION
L20	115.14	N89°36'45"W
L21	20.00	S0°45'15"W
L22	20.00	S0°45'15"W
L23	114.57	N89°36'45"W
L24	114.45	N89°36'45"W
L25	20.00	S0°45'15"W
L26	21.21	N44°36'45"W
L27	5.00	N0°23'15"E
L28	20.00	S0°45'15"W
L29	113.87	N89°36'45"W
L30	21.21	S45°23'15"W
L31	5.00	N0°23'15"E
L32	15.00	N89°36'45"W
L33	100.00	N0°23'15"E
L34	21.21	S44°36'45"E
L35	21.21	S45°23'15"W
L36	100.00	N0°23'15"E
L37	100.00	N0°23'15"E
L38	21.21	S44°36'45"E

LINE TABLE		
LINE#	LENGTH	DIRECTION
L39	21.21	S45°23'15"W
L40	100.00	N0°23'15"E
L41	15.00	N89°36'45"W
L42	21.21	S44°36'45"E
L43	100.00	N0°23'15"E
L44	21.21	S45°23'15"W
L45	100.00	N0°23'15"E
L46	24.00	N89°58'53"W
L47	20.86	S0°23'15"W
L48	100.00	N0°23'15"E
L49	37.28	S72°43'32"W
L50	2.83	S44°36'45"E
L51	28.28	S44°36'45"E
L52	31.11	N45°23'15"E
L53	31.11	N44°36'45"W
L54	2.83	N45°23'15"E
L55	28.28	N45°23'15"E
L56	31.11	S45°23'15"W
L57	31.11	N44°36'45"W

NOTES

- MINIMUM BUILDING SETBACK LINES SHALL CONFORM TO THE APPLICABLE ZONING REGULATIONS OF THE CITY OF KUNA AT THE TIME OF ISSUANCE OF A BUILDING PERMIT.
- ANY RESUBDIVISION OF THIS PLAT SHALL COMPLY WITH THE APPLICABLE ZONING REGULATIONS OF THE CITY OF KUNA IN EFFECT AT THE TIME OF RESUBDIVISION.
- IRRIGATION WATER WILL BE PROVIDED BY THE CITY OF KUNA IN COMPLIANCE WITH IDAHO CODE SECTION 31-3805(1)(B). ALL LOTS WITHIN THIS SUBDIVISION WILL BE ENTITLED TO IRRIGATION RIGHTS AND WILL BE OBLIGATED FOR ASSESSMENTS FROM THE CITY OF KUNA.
- MAINTENANCE OF ANY IRRIGATION AND DRAINAGE PIPES OR DITCHES CROSSING A LOT IS THE RESPONSIBILITY OF THE LOT OWNER UNLESS SUCH RESPONSIBILITY IS ASSUMED BY AN IRRIGATION/DRAINAGE ENTITY. SUCH LOTS MUST REMAIN FREE OF ENCROACHMENTS AND OBSTRUCTIONS TO SAID IRRIGATION/DRAINAGE FACILITIES.
- THIS DEVELOPMENT RECOGNIZES SECTION 22-4503 OF THE IDAHO CODE, RIGHT TO FARM ACT, WHICH STATES: "NO AGRICULTURAL OPERATION, AGRICULTURAL FACILITY OR EXPANSION THEREOF SHALL BE OR BECOME A NUISANCE, PRIVATE OR PUBLIC, BY ANY CHANGED CONDITIONS IN OR ABOUT THE SURROUNDING NONAGRICULTURAL ACTIVITIES AFTER IT HAS BEEN IN OPERATION FOR MORE THAN ONE (1) YEAR, WHEN THE OPERATION, FACILITY OR EXPANSION WAS NOT A NUISANCE AT THE TIME IT BEGAN OR WAS CONSTRUCTED. THE PROVISIONS OF THIS SECTION SHALL NOT APPLY WHEN A NUISANCE RESULTS FROM THE IMPROPER OR NEGLIGENT OPERATION OF AN AGRICULTURAL OPERATION, AGRICULTURAL FACILITY OR EXPANSION THEREOF."
- LOT 22, BLOCK 2, LOTS 8 AND 14, BLOCK 4, LOT 16, BLOCK 5, LOTS 1 AND 2, BLOCK 6, LOT 1, BLOCK 7, LOTS 1 AND 11, BLOCK 8, LOTS 1 AND 8, BLOCK 9, LOT 1, BLOCK 10 AND 11, BLOCK 11 ARE COMMON LOTS AND SHALL BE OWNED AND MAINTAINED BY THE ASHTON ESTATES HOMEOWNERS' ASSOCIATION, OR ASSIGNS. THESE COMMON LOTS ARE SUBJECT TO BLANKET EASEMENTS FOR PUBLIC UTILITIES AND CITY OF KUNA IRRIGATION.
- A PUBLIC UTILITY EASEMENT IS HEREBY RESERVED AS FOLLOWS (UNLESS OTHERWISE DIMENSIONED):  
+10'-FEET WIDE ALONG PUBLIC RIGHTS-OF-WAYS.
- WATER, SEWER, DRAINAGE AND IRRIGATION EASEMENTS ARE HEREBY RESERVED FOR THE CITY OF KUNA FOR THE INSTALLATION AND MAINTENANCE OF LINES AS SHOWN HEREON (UNLESS OTHERWISE DIMENSIONED).  
+10'-FEET WIDE ALONG PUBLIC RIGHTS-OF-WAYS, REAR LOT LINES AND THE EXTERIOR BOUNDARY.  
+10'-FEET WIDE CENTERED ON INTERIOR LOT LINES.
- LOTS SHALL NOT BE REDUCED IN SIZE WITHOUT PRIOR APPROVAL FROM THE HEALTH AUTHORITY AND THE CITY OF KUNA.
- NO ADDITIONAL DOMESTIC WATER SUPPLIES SHALL BE INSTALLED BEYOND THE WATER SYSTEM APPROVED IN THE SANITARY RESTRICTION RELEASE.
- THIS SUBDIVISION IS SUBJECT TO THE COVENANTS, CONDITIONS AND RESTRICTIONS FOR ASHTON ESTATES SUBDIVISION PER INSTRUMENT NO. 2018-089358 RECORDS OF ADA COUNTY, IDAHO AND ANY FUTURE AMENDMENTS.
- THE HOMEOWNERS' ASSOCIATION (HOA), ITS OWNERSHIP AND MAINTENANCE COMMITMENTS CANNOT BE DISSOLVED WITHOUT THE EXPRESS WRITTEN CONSENT OF THE CITY OF KUNA, IDAHO. ALL IMPROVED INDIVIDUAL LOTS ARE SUBJECT TO A FRACTIONAL SHARE OF THE IRRIGATION ASSESSMENT FOR EACH HOA COMMON LOT(S) THAT RECEIVES MUNICIPAL IRRIGATION. IF THE ASSESSMENT IS NOT PAID BY THE HOA, THE INDIVIDUAL IMPROVED LOTS ARE SUBJECT TO A LIEN FOR NON-PAYMENT.
- LOTS 8 AND 14, BLOCK 4, LOT 1, BLOCK 6, LOT 1, BLOCK 7 AND LOT 11, BLOCK 8, LOT 1, BLOCK 11 ARE SERVIENT TO AND CONTAIN THE ACHD STORM WATER DRAINAGE SYSTEM. THESE LOTS ARE ENCUMBERED BY THAT CERTAIN FIRST AMENDED MASTER PERPETUAL STORM WATER DRAINAGE EASEMENT RECORDED ON NOVEMBER 10, 2015, AS INSTRUMENT NO. 2015-103256, OFFICIAL RECORDS OF ADA COUNTY, AND INCORPORATED HEREIN BY THIS REFERENCE AS IF SET FORTH IN FULL (THE MASTER EASEMENT). THE MASTER EASEMENT AND THE STORM WATER DRAINAGE SYSTEM ARE DEDICATED TO ACHD PURSUANT TO SECTION 40-2302, IDAHO CODE. THE MASTER EASEMENT IS FOR THE OPERATION AND MAINTENANCE OF THE STORM WATER DRAINAGE SYSTEM.
- LOT 1, BLOCK 6, LOTS 1 AND 2, BLOCK 7 AND LOT 1, BLOCK 12 ARE SUBJECT TO A CITY OF KUNA PRESSURIZED IRRIGATION EASEMENT PER INSTRUMENT NO. 2018-018088, RECORDS OF ADA COUNTY, IDAHO.
- LOT 1 BLOCK 6, LOT 1 BLOCK 7 AND LOT 1 BLOCK 12 ARE SUBJECT TO AN ACHD PERMANENT EASEMENT PER INSTRUMENT NO. 2018-018088, RECORDS OF ADA COUNTY, IDAHO.
- LOT 22, BLOCK 2, LOT 16, BLOCK 5, LOT 1, BLOCK 6, LOT 1, BLOCK 7, LOT 1, BLOCK 9 AND LOTS 1, 2, 3 AND 4, BLOCK 12, ARE SUBJECT TO AN ACHD PERMANENT EASEMENT PER INSTRUMENT NO. 2018-\_\_\_\_\_, RECORDS OF ADA COUNTY, IDAHO.
- THIS PLAT IS SUBJECT TO AN ACHD LANDSCAPE LICENSE AGREEMENT PER INSTRUMENT NO. 2018-\_\_\_\_\_, RECORDS OF ADA COUNTY, IDAHO.
- CITY OF KUNA PRESSURIZED IRRIGATION EASEMENT PER INSTRUMENT NO. 2018-\_\_\_\_\_, RECORDS OF ADA COUNTY, IDAHO.
- DIRECT RESIDENTIAL LOT ACCESS TO N. JACKSONMILL AVENUE IS PROHIBITED.
- LOT 4, BLOCK 12 IS SUBJECT TO A TEMPORARY ACHD EASEMENT PER INST. NO. 2018-\_\_\_\_\_, RECORDS OF ADA COUNTY, IDAHO.



DEVELOPER  
SDN, LLC  
BOISE, IDAHO

10-23-2018

**km**  
ENGINEERING  
ENGINEERS, SURVEYORS, PLANNERS  
9233 WEST STATE STREET  
BOISE, IDAHO 83714  
PHONE (208) 629-9379  
FAX (208) 629-6930

PLAT OF  
ASHTON ESTATES SUBDIVISION No. 2

**CERTIFICATE OF OWNERS**

KNOW ALL MEN/WOMEN BY THESE PRESENTS: THAT THE UNDERSIGNED IS THE OWNER OF THE REAL PROPERTY HEREAFTER DESCRIBED.

A PARCEL OF LAND SITUATED IN A PORTION OF GOVERNMENT LOTS 1 AND 2 OF SECTION 19, TOWNSHIP 2 NORTH, RANGE 1 EAST, BOISE MERIDIAN, CITY OF KUNA, ADA COUNTY, IDAHO AND BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT A FOUND ALUMINUM CAP MARKING THE NORTHWEST CORNER OF SAID SECTION 19, WHICH BEARS N00°46'08"E A DISTANCE OF 2,649.32 FEET FROM A FOUND ALUMINUM CAP MARKING THE WEST 1/4 CORNER OF SAID SECTION 19, THENCE FOLLOWING THE WESTERLY LINE OF SAID GOVERNMENT LOT 1, S00°46'08"W A DISTANCE OF 1,146.19 FEET; THENCE LEAVING SAID WESTERLY LINE, S89°13'52"E A DISTANCE OF 70.00 FEET TO A FOUND 5/8-INCH REBAR MARKING THE SOUTHWEST CORNER OF ASHTON ESTATES SUBDIVISION NO. 1 (BOOK \_\_\_\_\_ OF PLATS AT PAGES \_\_\_\_\_ RECORDS OF ADA COUNTY, IDAHO), ON THE EASTERLY RIGHT-OF-WAY LINE OF NORTH MERIDIAN ROAD/STATE HIGHWAY 89 AND BEING THE POINT OF BEGINNING;

THENCE FOLLOWING THE SOUTHERLY BOUNDARY LINE OF SAID ASHTON ESTATES SUBDIVISION NO. 1, THE FOLLOWING TWENTY-FOUR (24) COURSES:

1. 16.95 FEET ALONG THE ARC OF A CIRCULAR CURVE TO THE RIGHT, SAID CURVE HAVING A RADIUS OF 39.50 FEET, A DELTA ANGLE OF 24°35'20", A CHORD BEARING OF N78°42'55"E AND A CHORD DISTANCE OF 16.82 FEET TO A FOUND 5/8-INCH REBAR;
2. S88°59'25"E A DISTANCE OF 184.94 FEET TO A FOUND 5/8-INCH REBAR;
3. 43.08 FEET ALONG THE ARC OF A CIRCULAR CURVE TO THE RIGHT, SAID CURVE HAVING A RADIUS OF 27.50 FEET, A DELTA ANGLE OF 89°45'33", A CHORD BEARING OF S44°06'38"E AND A CHORD DISTANCE OF 36.81 FEET TO A FOUND 5/8-INCH REBAR;
4. S89°13'52"E A DISTANCE OF 50.00 FEET TO A FOUND 5/8-INCH REBAR;
5. N00°46'08"E A DISTANCE OF 4.58 FEET TO A FOUND 5/8-INCH REBAR;
6. 35.44 FEET ALONG THE ARC OF A CIRCULAR CURVE TO THE RIGHT, SAID CURVE HAVING A RADIUS OF 22.50 FEET, A DELTA ANGLE OF 90°14'27", A CHORD BEARING OF N45°53'22"E AND A CHORD DISTANCE OF 31.89 FEET TO A FOUND 5/8-INCH REBAR;
7. S88°59'25"E A DISTANCE OF 80.15 FEET TO A FOUND 5/8-INCH REBAR;
8. 35.10 FEET ALONG THE ARC OF A CIRCULAR CURVE TO THE RIGHT, SAID CURVE HAVING A RADIUS OF 22.50 FEET, A DELTA ANGLE OF 89°22'40", A CHORD BEARING OF S44°18'05"E AND A CHORD DISTANCE OF 31.65 FEET TO A FOUND 5/8-INCH REBAR;
9. S00°23'15"W A DISTANCE OF 5.24 FEET TO A FOUND 5/8-INCH REBAR;
10. S88°59'25"E A DISTANCE OF 50.00 FEET TO A FOUND 5/8-INCH REBAR;
11. N00°23'15"E A DISTANCE OF 3.80 FEET TO A FOUND 5/8-INCH REBAR;
12. 38.11 FEET ALONG THE ARC OF A CIRCULAR CURVE TO THE RIGHT, SAID CURVE HAVING A RADIUS OF 22.50 FEET, A DELTA ANGLE OF 97°02'57", A CHORD BEARING OF N48°54'43"E AND A CHORD DISTANCE OF 33.72 FEET TO A FOUND 5/8-INCH REBAR;
13. 32.06 FEET ALONG THE ARC OF A CIRCULAR CURVE TO THE RIGHT, SAID CURVE HAVING A RADIUS OF 175.00 FEET, A DELTA ANGLE OF 10°28'45", A CHORD BEARING OF S77°18'56"E AND A CHORD DISTANCE OF 32.01 FEET TO A FOUND 5/8-INCH REBAR;
14. N174°43'40"E A DISTANCE OF 50.00 FEET TO A FOUND 5/8-INCH REBAR;
15. N00°23'15"E A DISTANCE OF 219.67 FEET TO A FOUND 5/8-INCH REBAR;
16. S89°36'45"E A DISTANCE OF 100.00 FEET TO A FOUND 5/8-INCH REBAR;
17. S00°23'15"W A DISTANCE OF 18.81 FEET TO A FOUND 5/8-INCH REBAR;
18. S89°36'45"E A DISTANCE OF 70.88 FEET TO A FOUND 5/8-INCH REBAR;
19. N00°23'15"E A DISTANCE OF 5.00 FEET TO A FOUND 5/8-INCH REBAR;
20. S89°36'45"E A DISTANCE OF 287.68 FEET TO A FOUND 5/8-INCH REBAR;
21. S00°23'15"W A DISTANCE OF 5.00 FEET TO A FOUND 5/8-INCH REBAR;
22. S89°36'45"E A DISTANCE OF 65.00 FEET TO A FOUND 5/8-INCH REBAR;
23. N00°23'15"E A DISTANCE OF 13.38 FEET TO A FOUND 5/8-INCH REBAR;
24. S89°36'45"E A DISTANCE OF 118.80 FEET TO A FOUND 5/8-INCH REBAR ON THE EASTERLY LINE OF SAID GOVERNMENT LOT;

THENCE LEAVING SAID SOUTHERLY BOUNDARY LINE AND FOLLOWING SAID EASTERLY LINE, S00°45'15"W A DISTANCE OF 431.12 FEET TO A FOUND 1/2-INCH REBAR MARKING THE NORTHEAST CORNER OF SAID GOVERNMENT LOT 2; THENCE LEAVING SAID EASTERLY LINE OF GOVERNMENT LOT 1 AND FOLLOWING THE EASTERLY LINE OF GOVERNMENT LOT 2, S00°45'15"W A DISTANCE OF 404.27 FEET TO A SET 5/8-INCH REBAR;

THENCE LEAVING SAID EASTERLY LINE, N89°36'45"W A DISTANCE OF 113.46 FEET TO A SET 5/8-INCH REBAR;  
THENCE S00°23'15"W A DISTANCE OF 15.00 FEET TO A SET 5/8-INCH REBAR;  
THENCE N89°36'45"W A DISTANCE OF 115.00 FEET TO A SET 5/8-INCH REBAR;  
THENCE N00°23'15"E A DISTANCE OF 250.00 FEET TO A SET 5/8-INCH REBAR;  
THENCE N89°36'45"W A DISTANCE OF 530.00 FEET TO A SET 5/8-INCH REBAR;  
THENCE N00°23'15"E A DISTANCE OF 58.16 FEET TO A SET 5/8-INCH REBAR;  
THENCE N89°36'45"W A DISTANCE OF 102.92 FEET TO A SET 5/8-INCH REBAR;  
THENCE S00°46'08"W A DISTANCE OF 569.96 FEET TO A SET 5/8-INCH REBAR;  
THENCE N89°58'53"W A DISTANCE OF 312.80 FEET TO A SET 5/8-INCH REBAR ON THE EASTERLY RIGHT-OF-WAY LINE OF SAID NORTH MERIDIAN ROAD/STATE HIGHWAY 89;  
THENCE FOLLOWING SAID EASTERLY RIGHT-OF-WAY LINE, N00°46'08"E A DISTANCE OF 682.34 FEET TO A POINT BEING WITNESSED BY A SET 5/8-INCH REBAR BEARING N04°34'59"E A DISTANCE OF 2.00 FEET;  
THENCE N04°34'59"E A DISTANCE OF 150.33 FEET TO A FOUND BRASS CAP HIGHWAY MONUMENT;  
THENCE N00°46'08"E A DISTANCE OF 29.51 FEET TO THE POINT OF BEGINNING.

SAID PARCEL CONTAINS 17.760 ACRES, MORE OR LESS.

IT IS THE INTENTION OF THE UNDERSIGNED TO HEREBY INCLUDE SAID LAND IN THIS PLAT. THE EASEMENTS SHOWN ON THIS PLAT ARE NOT DEDICATED TO THE PUBLIC BUT THE RIGHTS TO USE SAID EASEMENTS ARE HEREBY PERPETUALLY RESERVED FOR PUBLIC UTILITIES AND SUCH OTHER USES AS SHOWN ON THIS PLAT. NO STRUCTURES OTHER THAN FOR SUCH UTILITY AND OTHER DESIGNATED PUBLIC USES ARE TO BE ERRECTED WITHIN THE LIMITS OF SAID EASEMENTS UNLESS NOTED OTHERWISE ON THIS PLAT. THE UNDERSIGNED, BY THESE PRESENTS, DEDICATES TO THE PUBLIC ALL PUBLIC STREETS AS SHOWN ON THIS PLAT. ALL LOTS WITHIN THIS PLAT WILL RECEIVE WATER SERVICE FROM THE CITY OF KUNA AND SAID CITY HAS AGREED IN WRITING TO SERVE ALL OF THESE LOTS.

\_\_\_\_\_  
DONALD G NEWELL, MANAGER  
SDN, LLC

**ACKNOWLEDGMENT**

STATE OF IDAHO }  
                          } SS  
COUNTY OF ADA }

ON THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, IN THE YEAR \_\_\_\_\_ A.D., BEFORE ME, THE UNDERSIGNED, A NOTARY PUBLIC IN AND FOR SAID STATE, PERSONALLY APPEARED DONALD G. NEWELL, KNOWN OR IDENTIFIED TO ME TO BE THE MANAGER OF SDN, LLC, AND THE PERSON WHO EXECUTED THIS INSTRUMENT ON BEHALF OF SAID LIMITED LIABILITY CORPORATION AND ACKNOWLEDGED TO ME THAT SUCH LIMITED LIABILITY CORPORATION EXECUTED THE SAME.

IN WITNESS WHEREOF, I HAVE HERUNTO SET MY HAND AND AFFIXED MY OFFICIAL SEAL THE DAY AND YEAR IN THIS CERTIFICATE FIRST ABOVE WRITTEN.

\_\_\_\_\_  
NOTARY PUBLIC FOR THE STATE OF

RESIDING AT \_\_\_\_\_

MY COMMISSION EXPIRES \_\_\_\_\_

**CERTIFICATE OF SURVEYOR**

I, KELLY KEHRER, DO HEREBY CERTIFY THAT I AM A REGISTERED PROFESSIONAL LAND SURVEYOR LICENSED BY THE STATE OF IDAHO, AND THAT THIS PLAT OF ASHTON ESTATES SUBDIVISION No. 2 AS DESCRIBED IN THE "CERTIFICATE OF OWNERS" AND AS SHOWN ON THE ATTACHED PLAT, WAS DRAWN FROM AN ACTUAL SURVEY MADE ON THE GROUND UNDER MY DIRECT SUPERVISION AND ACCURATELY REPRESENTS THE POINTS PLATTED THEREON, AND IS IN CONFORMITY WITH THE STATE OF IDAHO CODE RELATING TO PLATS AND SURVEYS.



\_\_\_\_\_  
KELLY KEHRER, P.L.S. 16662



DEVELOPER  
SDN, LLC  
BOISE, IDAHO

9233 WEST STATE STREET  
BOISE, IDAHO 83714  
PHONE (208) 639-4939  
FAX (208) 639-4930

PLAT OF  
ASHTON ESTATES SUBDIVISION No. 2

**APPROVAL OF ADA COUNTY HIGHWAY DISTRICT**  
THE FOREGOING PLAT WAS ACCEPTED AND APPROVED BY THE BOARD OF ADA COUNTY HIGHWAY DISTRICT COMMISSIONERS ON THE \_\_\_\_\_ DAY OF \_\_\_\_\_, 20\_\_\_\_

\_\_\_\_\_  
PRESIDENT  
ADA COUNTY HIGHWAY DISTRICT

**APPROVAL OF KUNA CITY ENGINEER**  
THIS PLAT IS ACCEPTED AND APPROVED THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, AD 20\_\_\_\_, BY THE CITY ENGINEER OF THE CITY OF KUNA, ADA COUNTY, IDAHO.

\_\_\_\_\_  
CITY ENGINEER

**APPROVAL OF CITY COUNCIL**  
I, \_\_\_\_\_, CITY CLERK IN AND FOR THE CITY OF KUNA, ADA COUNTY, IDAHO, DO HEREBY CERTIFY THAT IN A REGULAR MEETING OF THE CITY COUNCIL HELD ON THE DAY OF \_\_\_\_\_, A.D. 20\_\_\_\_, THIS PLAT WAS DULY ACCEPTED AND APPROVED.

\_\_\_\_\_  
KUNA CITY CLERK

**CERTIFICATE OF COUNTY SURVEYOR**  
I, THE UNDERSIGNED, PROFESSIONAL SURVEYOR FOR ADA COUNTY, IDAHO, DO HEREBY CERTIFY THAT I HAVE CHECKED THIS PLAT AND FIND THAT IT COMPLIES WITH THE STATE OF IDAHO CODE RELATING TO PLATS AND SURVEYS.

\_\_\_\_\_  
COUNTY SURVEYOR

\_\_\_\_\_  
DATE

**HEALTH CERTIFICATE**  
SANITARY RESTRICTIONS AS REQUIRED BY IDAHO CODE, TITLE 50, CHAPTER 13 HAVE BEEN SATISFIED ACCORDING TO THE LETTER TO BE READ ON FILE WITH THE COUNTY RECORDER OR HIS AGENT LISTING THE CONDITIONS OF APPROVAL. SANITARY RESTRICTIONS MAY BE REIMPOSED, IN ACCORDANCE WITH SECTION 50-1326, IDAHO CODE, BY THE ISSUANCE OF A CERTIFICATE OF DISAPPROVAL.

\_\_\_\_\_  
HEALTH OFFICER

\_\_\_\_\_  
DATE

**CERTIFICATE OF ADA COUNTY TREASURER**  
I, THE UNDERSIGNED, COUNTY TREASURER IN AND FOR THE COUNTY OF ADA, STATE OF IDAHO, PER THE REQUIREMENTS OF I.C. 50-1308, DO HEREBY CERTIFY THAT ANY AND ALL CURRENT AND OR DELINQUENT COUNTY PROPERTY TAXES FOR THE PROPERTY INCLUDED IN THIS SUBDIVISION HAVE BEEN PAID IN FULL. THIS CERTIFICATION IS VALID FOR THE NEXT THIRTY (30) DAYS ONLY.

\_\_\_\_\_  
COUNTY TREASURER

\_\_\_\_\_  
DATE

**CERTIFICATE OF COUNTY RECORDER**  
STATE OF IDAHO }  
ADA COUNTY } SS  
I HEREBY CERTIFY THAT THIS PLAT OF ASHTON ESTATES SUBDIVISION No. 2 WAS FILED AT THE REQUEST OF \_\_\_\_\_ AT \_\_\_\_\_ MINUTES PAST \_\_\_\_\_ O'CLOCK \_\_\_\_M., THIS \_\_\_\_ DAY OF \_\_\_\_\_, A.D., IN MY OFFICE AND WAS DULY RECORDED AS BOOK \_\_\_\_\_ OF PLATS AT PAGES \_\_\_\_\_ THRU \_\_\_\_\_.

INSTRUMENT NUMBER \_\_\_\_\_

\_\_\_\_\_  
DEPUTY

\_\_\_\_\_  
EX-OFFICIO RECORDER

FEE:

DEVELOPER  
SDN, LLC  
BOISE, IDAHO





CITY OF KUNA  
P.O. BOX 13  
KUNA, ID 83634  
[www.kunacity.id.gov](http://www.kunacity.id.gov)  
(208) 287-1727

Paul A. Stevens, P.E.  
Kuna City Engineer

## FINAL PLAT MEMORANDUM

**To:** Wendy Howell – Director of Planning and Zoning  
**From:** Paul Stevens – City Engineer  
**RE:** Final Plat – **Ashton Estates Subdivision No. 2 – Kuna, ID**  
**Date:** 27 February 2019

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A qualified licensed professional engineer (QLPE) letter was issued for Ashton Estates Subdivision No. 2 on 28 November 2018. A preconstruction meeting followed and construction work is in progress.

In regards to the final plat, we recommend proceeding with approval of the final plat with the understanding that any issues and conditions raised in association with the following comments be addressed during final plat & plan review.

- 1. Record Drawings**  
This projects Record Drawings (As-builts) have not been submitted to the City of Kuna Engineering Department for review because work is in progress and the Record Drawings are not complete. A complete set of Record Drawings must be received before final approval can be provided.
- 2. Final Plat**  
The Final Plat provided with the Construction Drawings appears to have appropriate easements, rights-of-way, easement language and similar items common to final plats.
- 3. Public Works Inspection Fees and Plan Review Fees**  
The inspection fees and/or the plan review fees have been paid on this project in full.

The Final Plat will be signed by the City Engineer upon completion of the project and verification of the integrity of the final plat



**CITY OF KUNA**  
P.O. BOX 13  
KUNA, ID 83634  
[www.kunacity.id.gov](http://www.kunacity.id.gov)

Paul A. Stevens, P.E.  
Kuna City Engineer  
208-287-1727

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## Letter of Transmittal

TO: Mr. Kevin P. McCarthy, P.E.  
KM Engineering, LLP

FROM: Paul A. Stevens, P.E. *PS*  
Kuna City Engineer

RE: Ashton 2  
Final Review. QLPE and Will Serve

DATE: 28 November 2018

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Dear Mr. McCarthy:

Please find enclosed Ashton Estates No. 2 QLPE letter, Will Serve letter, and an itemized invoice for inspections and plan review.

There were some minor changes to street light locations and numbering. Separation distances between pipe crossings are not called out. Minimum separation distances apply. Please make the minor edits and return a clean set of construction drawings for signature.

Please note that we are now requiring material submittals (on paper) at or before the preconstruction meetings. Please make sure that the item submitted for approval is clearly marked.

Respectfully,

*Paul A. Stevens, P.E.*  
Kuna City Engineer



CITY OF KUNA  
P.O. BOX 13  
KUNA, ID 83634  
[www.kunacity.id.gov](http://www.kunacity.id.gov)

PAUL A. STEVENS, P.E.  
CITY ENGINEER  
(208) 287-1727

28 November 2018

To: Todd Crutcher, P.E.  
Department of Environmental Quality  
Southwest Idaho Regional Office  
1445 North Orchard  
Boise, ID 83706

Cc: Kevin McCarthy, P.E. (KM Engineering, LLP)  
Planning & Zoning (City of Kuna)  
Ashton Estates No. 2 File

RE: CITY OF KUNA Plan Review QLPE and Will Serve Approval for Ashton Estates Subdivision No. 2



**PLAN REVIEW**

Paul A. Stevens, P.E. is the Kuna City Engineer and qualified licensed professional engineer representing the City of Kuna. The City Engineer has reviewed the attached **Ashton Estates Subdivision No. 2** sewer and water construction plans, dated October 2018, for conformance with the engineering standards of care and City standards as of the above date. The proposed development is an extension of sewer and water lines that conform to the current City sewer and water master plans. The sewer and water facilities presented in the attached plans are in general conformance with applicable engineering standards of care and City standards.

Once the City Engineer has accepted the construction and the developer has certified that the sewer and water facilities were constructed to City standards, the City will own and operate the sewer and water facilities excluding facilities which are located on private property without easements, service lines, or any temporary pressure sewer systems.

**WATER SYSTEM CAPACITY**

Conditional "will-serve" commitments become firm commitments upon recordation of final plats in most instances and the development is then eligible to connect to the system upon payment of connection fees. There are adequate water connections available for this development and it is recommended that sanitary water restrictions for this project are lifted according to the following terms:

1. A conditional "will-serve" commitment for this development is issued in connection with this QLPE Letter
2. A firm "will-serve" commitment will be issued upon recordation of the final plat
3. If construction of this project is not commenced within one year of the date of this letter and pursued vigorously to completion thereafter, this QLPE Letter and associated commitments may be withdrawn

**TREATMENT PLANT CAPACITY**

Conditional "will-serve" commitments become firm commitments upon recordation of final plats in most instances and the development is then eligible to connect to the system upon payment of connection fees. There are adequate sewer connections available for this development and it is recommended that sanitary sewer restrictions for this project are lifted according to the following terms:

1. A conditional "will-serve" commitment for this development is issued in connection with this QLPE Letter
2. A firm "will-serve" commitment will be issued upon recordation of the final plat
3. If construction of this project is not commenced within one year of the date of this letter and pursued vigorously to completion thereafter, this QLPE Letter and associated commitments may be withdrawn

**SEWER/WATER APPROVAL**

In summary, the following apply:

1. **The City hereby issues Ashton Estates Subdivision No. 2 a "Will Serve" letter with sanitary sewer and water restrictions removed for fifty five (55) residential lots and four (4) commercial lots listed below in compliance with conditions noted above:**

Block 2, Lots 18-21  
 Block 4, Lots 9-13  
 Block 5, Lots 6-15  
 Block 6, Lots 3-11  
 Block 7, Lots 2-6

Block 8, Lots 2-10, 12  
 Block 9, Lots 2-7  
 Block 10, Lot 2  
 Block 11, Lot 2  
 Block 12, Lots 1-4

2. **The plat for Ashton Estates Subdivision No. 2 also includes sixteen (16) common lots for which sanitary sewer and water restrictions are not released and which are listed below:**

Block 2, Lot 22  
 Block 4, Lot 8, 14  
 Block 5, Lot 16  
 Block 6, Lots 1, 2

Block 8, Lots 1, 11  
 Block 9, Lots 1, 8  
 Block 10, Lot 1  
 Block 11, Lot 1

**HOLD HARMLESS COMMITMENT**

The Developer may proceed with its water and sewer line construction with the approval of DEQ at his/her own risk with the understanding that water and sewer permits can only be issued when there is remaining available capacity for permits in the water and sewer systems. The developer, in choosing to proceed with construction, will hold the City of Kuna and DEQ harmless from any damages that may be caused by a delay in receiving EDU sewer or water permits for any of its subdivision lots.

Respectfully,



Paul A. Stevens, P.E.  
Kuna City Engineer



CITY OF KUNA  
P.O. BOX 13  
KUNA, ID 83634  
[www.kunacity.id.gov](http://www.kunacity.id.gov)  
(208) 287-1727  
pstevens@kunaid.gov

PAUL A. STEVENS, PE  
CITY ENGINEER

28 November 2018

To: Mr. Kevin McCarthy, P.E.  
KM Engineering, LLP  
9233 W. State St.  
Eagle, Idaho 83714  
208-639-6939

Cc : Ashton Estates No 2 File  
Planning & Zoning (City of Kuna)

RE: **Ashton Estates Subdivision No. 2**  
**FINAL CONSTRUCTION PLAN APPROVAL**

---

#### QLPE Letter

The City Engineer has reviewed the Final Construction Drawings (dated 10-24-18) for **Ashton Estates Subdivision No. 2**. As stated in the attached QLPE Letter, sewer, water and pressure irrigation facilities presented in the construction plans substantially conform to engineering standards of care and City standards.

When finalizing the project construction drawings please make the red line edits as shown on the enclosed plan set. The edits generally pertain to:

1. Street Light #'s and locations.
2. Call out separation distances of lateral pipes on the appropriate profile sheets. Verify that minimum separation distances are maintained.

The submitted construction drawings are approved and eligible for a City commitment to serve with water and sewer. The Street Light Plan, Landscape Plan and Fire Hydrant Plan are approved by others and are not included in the scope of this approval. Please see the attached QLPE Letter for conditions of the "Will Serve" commitment. This construction plan approval is issued contingent upon completing items 1 and 2 listed above, the commencement of construction in a substantial way within one year of the date of this letter, and the expeditious completion of the work. Failure to meet these conditions may result in cancellation of the construction drawing approval and the termination of any "Will Serve" commitments provided in the attached QLPE Letter.

#### Reimbursable Improvements

As required by City Ordinance, before the commencement of construction, submit an itemized list, with detailed cost estimates of improvements for which you will request reimbursement in accordance with the City's current Reimbursement Policy.

A copy of the current reimbursement policies for water, sewer, and pressure irrigation can be obtained from the City. Once your reimbursement request and cost estimates are received, they will be reviewed by the City to determine reimbursement eligibility. Once construction is complete, a reimbursement agreement will be executed between yourself and the City of Kuna. **Please note, if you proceed with construction prior to the City approving eligibility of reimbursable improvements and cost estimates, it may jeopardize your eligibility for reimbursement.**

### Inspection Fees

Pursuant to Ordinance #708, an inspection fee of one dollar (\$1.00) for each lineal foot of water, sewer, and pressurized irrigation pipe is due upon the City's approval of final construction plans. Based on our review of the approved construction plans, there are 4370 feet of water line, 3730 feet of sewer line, 4390 feet of pressure irrigation line improvements, a Plan Review Fee of \$200 plus \$10 per lot, and a Record Drawing Review Fee of \$200. **Consequently, inspection and other fees of \$13,480 are now due and payable to the City of Kuna. The City does not proceed with pre-construction conferences until these fees are paid but will accept delivery of payment at the conference.**

### Next Steps

Please proceed with the following steps:

1. Obtain stamped approval from ACHD for the Street Plans.
2. Provide a list of improvements for which you will seek reimbursement with a detailed cost estimate.
3. Submit clean construction plans for the stamped approval of the City Engineer.
4. Provide copies of the plans on yellow paper for the City (3 – 24" x 36" & 2- 11"x17"), ACHD, contractors and for you.
5. Submit a copy of the stamped plans and the QLPE Letter to IDEQ.
6. Submit an electronic copy of stamped and approved plans to the City in PDF format.
7. Submit payment of \$13,480 for the Plan Review Fees, Inspection Fees, and Record Drawing Review Fee.
8. Schedule the Pre-Construction Conference.
9. Provide all material submittals to the City at or before the Pre-Construction Conference.

Please phone 287-1727 if you have any questions.

Respectfully,



Paul A. Stevens, P.E.  
Kuna City Engineer



**CITY OF KUNA**  
**P.O. BOX 13**  
**KUNA, ID 83634**  
 PH. (208) 922 - 5546 FAX (208) 922 - 5989

Invoice No.

**INVOICE**

**Customer**

Name KM Engineering  
 Address 9233 W State Street  
 City Eagle State ID ZIP 83714  
 Phone 208-639-6939

**Misc**

Date 11/28/2018  
 Code1 \_\_\_\_\_  
 Code2 \_\_\_\_\_  
 FOB \_\_\_\_\_

Qty	Description	Unit Price	TOTAL
4370	Ashton Estates No. 2 Inspection Fee - Acct 20-4550	\$ 1.00	\$ 4,370.00
3730	Ashton Estates No. 2 Inspection Fee - Acct 21-4550	\$ 1.00	\$ 3,730.00
4390	Ashton Estates No. 2 Inspection Fee - Acct 25-4550	\$ 1.00	\$ 4,390.00
200	Record Drawing Review Fee- Acct 20-4358	\$ 0.42	\$ 84.00
200	Record Drawing Review Fee- Acct 21-4358	\$ 0.42	\$ 84.00
200	Record Drawing Review Fee- Acct 25-4358	\$ 0.16	\$ 32.00
1	Base Plan Review Fee	\$ 200.00	\$ 200.00
59	Additional Plan Review Fee*	\$ 10.00	\$ 590.00
*Per Resolution R27-2008; charge \$10 per buildable lot			
DELINQUENT AFTER:			

SubTotal	\$ 13,480.00
Shipping	
Tax Rate(s)	0.00%
TOTAL	\$ 13,480.00

**Payment**

Check

Tax Rate(s)

Comments Ashton Estates No. 2 - Insp Fees

Name \_\_\_\_\_

CC # \_\_\_\_\_

Expires \_\_\_\_\_

**TERMS: NET 20th of month**

Late payments may be subject to interest at 18% annum.

Please reference the invoice number on your remittance.

Thank You!



# City of Kuna

City Council  
Staff Memo

P.O. Box 13  
Phone: (208) 922-5274  
Fax: (208) 922-5989  
www.Kunacity.id.gov

**To:** City Council

**Case Numbers:** 18-06-AN (Annexation) & 18-08-S (Preliminary Plat) **Ledgestone Subdivision**

**Site Location:** East Hubbard Road and North Locust Grove Road, Kuna, ID 83634

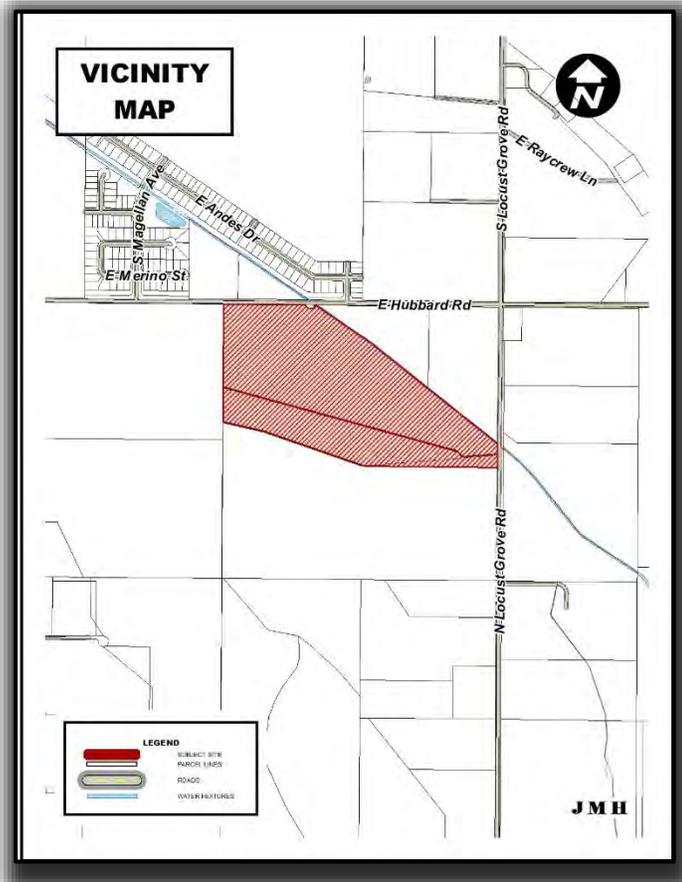
**Planner:** Jace Hellman, Planner II

**Hearing Date:** March 5, 2019

**Owner:** TJ Johnson  
2425 N Locust Grove Road  
Kuna, ID 83634

**Applicant:** Trilog Development, Inc.  
9839 Cable Car Street, Suite 101  
Boise, ID 83709  
208.895.8858

**Representative:** Jane Suggs, WHPacific  
2141 W. Airport Way, Suite 104  
Boise, ID 83705  
208.275.8729  
[jsuggs@whpacific.com](mailto:jsuggs@whpacific.com)



**Table of Contents:**

- A. Process and Noticing
- B. Applicants Request
- C. Exhibit Maps
- D. Site History
- E. General Project Facts
- F. Staff Analysis
- G. Applicable Standards
- H. Proposed Comprehensive Plan Analysis
- I. Proposed Kuna City Code Analysis
- J. Commission’s Recommendation
- K. Council’s Proposed Order of Decision

**A. Process and Noticing:**

1. Kuna City Code (KCC), Title 1, Chapter 14, Section 3, states that design reviews are designated as *public meetings*, with the Planning and Zoning Commission (acting as the Design Review Board) as the decision-making body; and that preliminary plats and annexations are designated as public hearings, with the Planning and Zoning Commission as a recommending body and City Council as the decision-making body. These land use applications were given proper public notice and followed the requirements set forth in Idaho Code, Chapter 65, Local Planning Act.

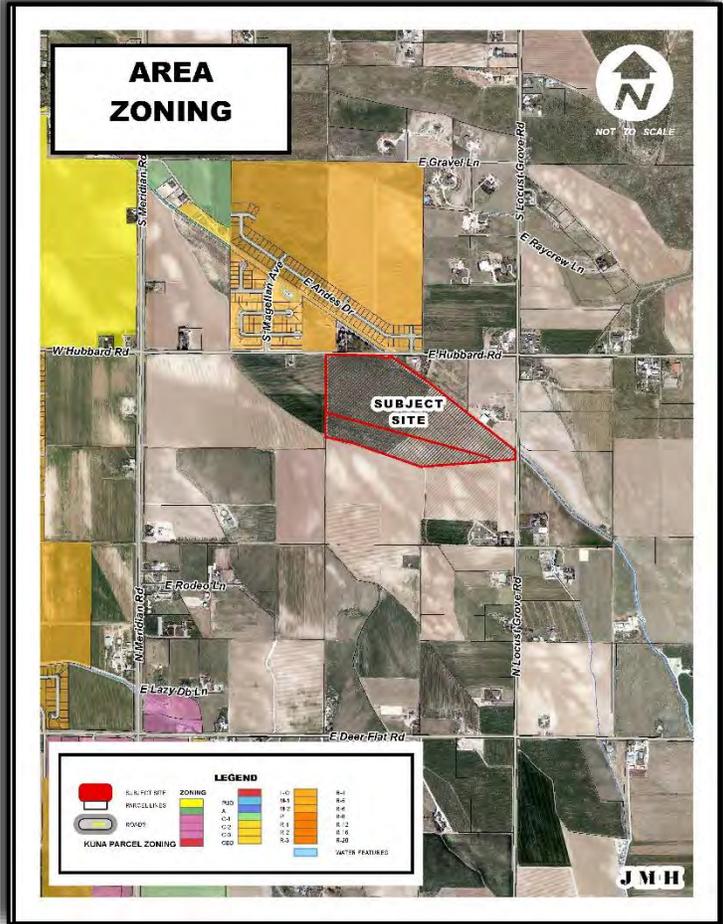
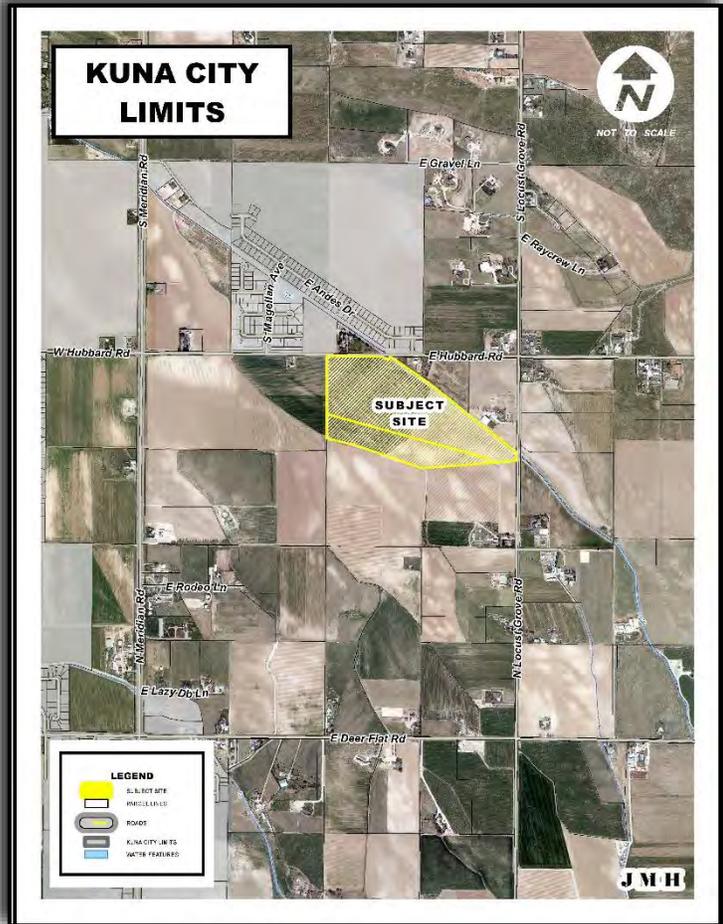
**a. Notifications**

- |                                  |                                  |
|----------------------------------|----------------------------------|
| i. Neighborhood Meeting          | July 9, 2018 (6 people attended) |
| ii. Agency Comment Request       | October 22, 2018                 |
| iii. 400' Property Owners Notice | February 13, 2019                |
| iv. Kuna Melba Newspaper         | February 13, 2019                |
| v. Site Posted                   | February 21, 2019                |

**B. Applicant's Request:**

On behalf of Trilogy Development, Inc., Jane Suggs, with WHPacific requests to annex two parcels comprising of approximately 60.85 acres on East Hubbard Road and North Locust Grove Road into Kuna City Limits with an R-8 zoning designation; and to subdivide the 60.85-acres into 298 total lots, consisting of 253 single family lots, and 45 common lots. The subject sites are adjacent to the southwest corner of East Hubbard Road and North Locust Grove Road, Kuna, ID 83634, within Section 18, Township 2 North, Range 1 East; (APN's S1418121126 & S1418123400).

**C. Exhibit Maps:**

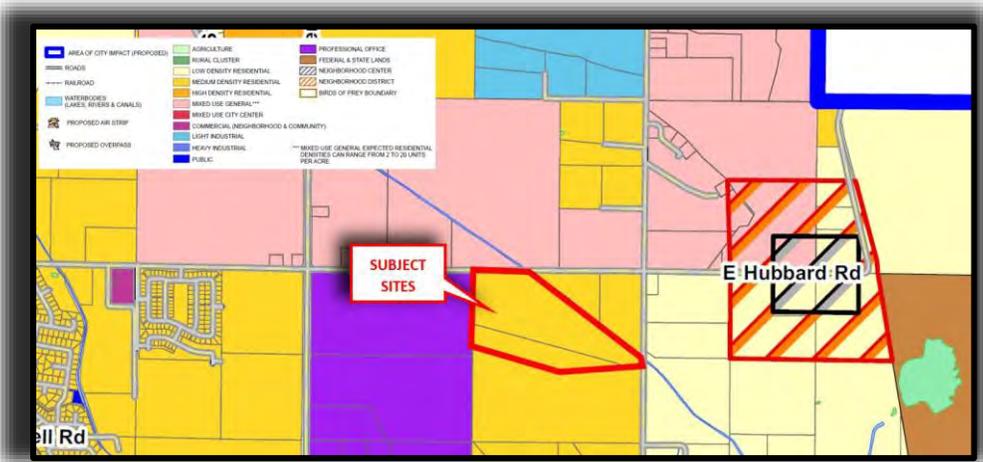


**D. Site History:**

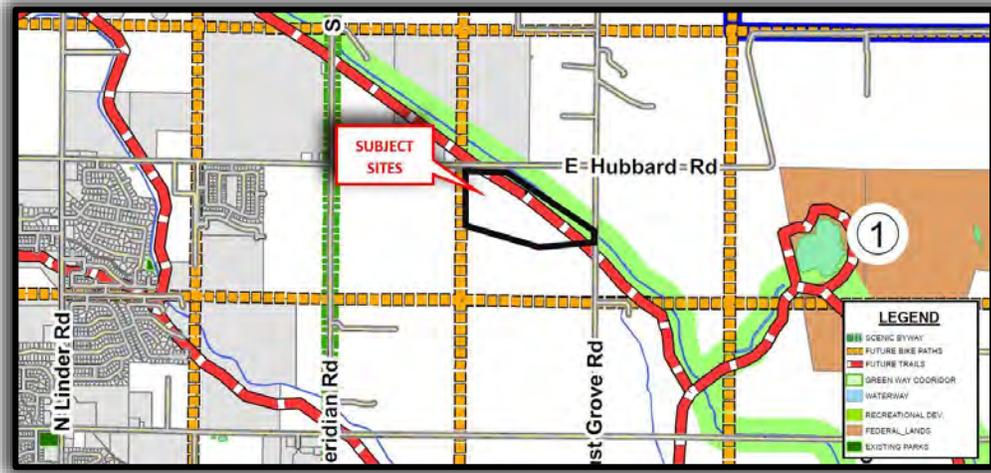
Both parcels are currently zoned Rural Residential (RR) within unincorporated Ada County. Historically these parcels have served as farmland.

**E. General Projects Facts:**

1. **Comprehensive Plan Map:** The Future Land Use Map (Comp Plan Map) is intended to serve as a *guide* for the decision-making body for the City. The Future Land Use Map indicates land use designations generally speaking, it is not the actual zone. The Future Land Use Map identifies the subject site as having a Medium Density Residential designation.



2. **Recreation and Pathways Map:** The Recreation and Pathways Master Plan Map indicates a future trail through the northeast edge of the site situated along the Mason Creek Feeder. Accordingly, it is the City's goal and desire to increase the number of trails and pathways in Kuna. Therefore, developers need to design and construct trails and pathways along frontages of canals and ditches abutting to, or within the subject site, to comply with the Master Plan.



3. **Surrounding Land Uses:**

North	R-6	Medium Density Residential – Kuna City
	RR	Rural Residential – Ada County
South	RR	Rural Residential – Ada County
East	RR	Rural Residential – Ada County
West	RR	Rural Residential – Ada County

4. **Parcel Sizes, Current Zoning, Parcel Numbers:**

Property Owner	Parcel Size	Current Zone:	Parcel Numbers
TJ Johnson	60.85 acres	RR (Rural Residential)	S1418121126 & S1418123400

5. **Services:**

- Sanitary Sewer– City of Kuna
- Potable Water – City of Kuna
- Pressurized Irrigation – City of Kuna (KMIS)
- Fire Protection – Kuna Rural Fire District
- Police Protection – Kuna Police (Ada County Sheriff’s office)
- Sanitation Services – J & M Sanitation

6. **Existing Structures, Vegetation and Natural Features:**

The project sites are vacant of any structures with the exception of a few outbuildings. vegetation on-site is consistent with that of crop fields. The site is relatively flat with an estimated average slope of 0% to 2%. Bedrock depth is estimated to be greater than sixty inches according to the USDA Soil Survey for Ada County.

7. **Transportation / Connectivity:**

The applicant proposes connections to public streets in two locations, which includes a new section of the mid mile collector Stroebel Road and existing Locust Grove Road. Additionally, a temporary access onto Hubbard Road has been proposed, which will be relinquished upon a later phase of development. A continuation of the regional pathway along the Mason Creek Feeder has been proposed along the northeastern property line of the proposed project site.

8. **Environmental Issues:**

The subject sites are within the nitrate priority area. Idaho Department of Environmental Quality (DEQ) has provided recommendations for surface and groundwater protection practices and requirements for development of the site.

9. **Agency Responses:** The following agency comments are included as exhibits with this case file:

- Department of Environmental Quality ..... Exhibit C-2
- Central District Health Department ..... Exhibit C-3
- Community Planning Association of Southwest Idaho (Compass) ..... Exhibit C-4
- Nampa & Meridian Irrigation District ..... Exhibit C-5
- Boise Project Board of Control ..... Exhibit C-6
- Kuna City Engineer ..... Exhibit C-7
- Idaho Transportation Department ..... Exhibit C-8
- Ada County Highway District ..... Exhibit C-9

F. **Staff Analysis:**

A pre-application meeting was held for this project on May 23, 2018. The applicant, Planning and Zoning staff, Public Works staff and Kuna Rural Fire District staff were in attendance to discuss the proposed project. A neighborhood meeting was held by the applicant for residents within the vicinity of the proposed project on July 9, 2018. A recap of the neighborhood meeting can be found within the applicant’s letter of intent (Exhibit A2b).

The applicant proposes to annex two contiguous parcels comprising of approximately 60.85 acres on East Hubbard Road and North Locust Grove Road into Kuna City Limits with an R-8 zoning designation; and to subdivide the 60.85-acres into 298 total lots, consisting of 253 single family buildable lots, and 45 Common lots.

The applicant has proposed a zoning designation of R-8 (medium-density residential) zone. However, the applicant has proposed an actual density of 4.15 dwelling units per acre. Lots are proposed to be larger than the minimum of 3,300 square feet required by Kuna City Code, with lot sizes ranging from approximately 5,400 square feet to 10,600 square feet. However, the dimensional lot standards of an R-8 designation allow the applicant to provide a new type of home product to the City of Kuna. 53 buildable lots are proposed as alley loaded homes. These single-family homes will face the street; however, driveway and garage access will be available from the rear of the house via a public alley way. This product will fit into the current dimensional lot standards of an R-8 zone except for a proposed lot coverage of 52%. Kuna City Code 5-3-3 allows for a maximum lot coverage of 40%. However, the City of Kuna encourages creativity in the design of its subdivisions and Kuna City Code 5-3-3-B allows for the Planning and Zoning director to modify some dimensional standards with the Planning and Zoning Commissions' concurrence. The remaining 200 buildable lots will be standard single-family home lots.

Applicant is constructing a new section of Stroebel Road along the east property line, which will serve as the main entrance into the Project. Stroebel Road is listed as a north-south collector according to Kuna's Street Circulation Map. Staff would recommend the applicant work with Staff and Ada County Highway District to ensure the new collector street is constructed to each agency's standards. A secondary access has been proposed onto existing Locust Grove Road. However, this access point is not projected to be constructed until a later phase of site development. In the meantime, the applicant proposes a temporary emergency access point on to Hubbard Road. Several stub streets have been provided as a part of this project, two (2) to the south, including the stub of the future collector Stroebel Road, and one (1) to the northeast, across the Mason Creek Feeder. The applicant will be required to install a sign at the terminus of each of the roads stating these roads will continue in the future. Staff will defer the applicant to comments provided by ACHD for preferred language.

The installation of streetlights are a required public improvement (Kuna City Code 6-4-2). The applicant has not identified streetlight locations on the preliminary plat. Applicant will be required to work with staff in order to comply with Kuna City code and install street lights a maximum spacing of two hundred fifty (250) feet along the site's frontage. The locations of street lights will be approved at the time of construction document review. Please note that these street lights must be designed and installed according to "Dark skies" standards.

Applicant proposes to install an eight (8) foot sidewalk within the proposed buffer areas along Hubbard, Stroebel and Locust Grove Roads. Road sections of Hubbard Road and Locust Grove Road that abut the site are designated as arterials, while the new proposed section of Stroebel Road is designated as a collector. Kuna City Code 5-17 requires the construction of detached eight (8) foot sidewalks along classified roadways. This sidewalk needs to be located within the public right-of-way and separated from the public vertical curb by a four to eight-foot-wide irrigated, landscaped planter strip, which is in addition to the required landscape buffers. Staff would recommend the applicant be required to install curb and gutter along all roadways, as required by Kuna City Code 6-4-2. Correspondence received from the applicant's representative on January 28, 2019 (Exhibit A4), indicates the applicant will install curb and gutter along Hubbard Road and Locust Grove Road. Furthermore, the applicant is required to conform to Kuna City Code and install sidewalk within the right-of-way separated from the vertical curb by a four to eight-foot-wide irrigated and landscaped planter strip.

Kuna's Comprehensive Plan (Comp Plan), encourages a variety of housing types for all income levels numerous times throughout the document. Pertinent sections of the Comp Plan that address housing types are included below, in Section K (Comp Plan Analysis) of this report. The City aims to balance all housing types. The Comprehensive Plan's Future Land Use Map has identified this site as Medium Density Residential. Staff has reviewed the proposed preliminary plat for technical compliance with Kuna City Code (KCC), and finds the preliminary plat and landscape plan are in compliance with applicable codes. Staff would recommend that the applicant work with staff, ACHD, and Kuna Rural Fire District (KRFD) to conform to each agency's requirements.

The Recreation and Pathway Master Plan Map identifies a regional green way corridor pathway along Mason Creek. The applicant proposes the construction of over 2,000 linear feet of regional public pathway along the south side of Mason Creek along the northeastern edge of the subject site's property line. As a proposed green

way corridor pathway, Kuna City Code 5-17-14 requires pathway of a minimum of ten (10) feet wide and it must be located within a thirty-foot wide public easement. Staff has determined that the proposed pathway complies with Kuna City Code. Additionally, several internal pathways have been proposed throughout to promote pedestrian connectivity throughout the project.

Applicant is hereby notified that this project is subject to design review inspection fees. Required inspections (post construction), are to verify landscaping compliance prior to signature on the final plat.

Staff has determined the preliminary plat is in technical compliance with Kuna City Code, Title 5 and Title 6, Idaho Statute § 67-6511; and complies with the goals and policies set in Kuna's Comprehensive Plan. The Planning and Zoning Commission voted 3-0 at their regularly scheduled January 22, 2019 meeting to recommend approval case nos. 18-06-AN and 18-08-S to the City Council, subject to conditions of approval as stated in section "K" of this report.

#### **G. Applicable Standards:**

1. City of Kuna Zoning Ordinance Title 5.
2. City of Kuna Subdivision Ordinance Title 6.
3. City of Kuna Comprehensive Plan.
4. Idaho Code, Title 67, Chapter 65- the Local Land Use Planning Act.

#### **H. Proposed Comprehensive Plan Analysis:**

The Kuna City Council may accept or reject the Comprehensive Plan components, and has determined the proposed annexation and preliminary plat requests for the site *are/are not* consistent with the following Comprehensive Plan components as described below:

##### **2.0 – Property Rights**

***Goal 1: Ensure that the City of Kuna land use policies, restrictions, conditions and fees do not violate private property rights. Establish an orderly, consistent review process for the City of Kuna to evaluate whether proposed actions may result in a private property "takings".***

*Policy: As part of a land use action review, the staff shall evaluate with guidance from the City's attorney; The Idaho Attorney General's six criteria established to determine the potential for property taking.*

##### **6.0 – Land Use**

*Policy: Provide a variety of housing densities and types to accommodate various lifestyles, ages and economic groups.*

**Goal 2: Encourage a balance of land uses to ensure that Kuna remains desirable, stable and a self-sufficient community.**

**Goal 3: Protect the quality of existing residential neighborhoods and ensure new residential development is sustainable. Provide a variety of housing opportunities to meet the needs of all Kuna residents.**

*Objective 3.1: Encourage and plan for the development of cohesive neighborhood units that incorporate a variety of housing densities and styles.*

##### **9.0 – Transportation**

**Goal 1: Promote and encourage bicycling and walking as transportation modes.**

##### **10.0 – Recreation**

**Goal 2: Integrate trails, pathways, bike lanes and greenway corridor systems into community life and development patterns.**

*Objective 2.1: Ensure that neighborhoods have easy access to open green space, pathways, trails and bike lanes.*

## **12.0 – Housing**

**Goal 1: Provide a wide-range of housing to meet the needs of the current and future population. Ensure that housing is available throughout the community for people of all income levels and for those with special needs.**

*Objective 1.1: Encourage the construction of housing that is safe, affordable and designed to accommodate a range of income levels and lifestyles.*

*Policy: Encourage the development community to provide a variety of lot sizes, dwelling types, densities and price points.*

**Goal 3: Encourage high-quality residential development.**

### **I. Proposed Kuna City Code Analysis:**

1. This request appears to be consistent and in compliance with all Kuna City Code (KCC).

**Comment:** *The proposed applications adhere/do not adhere to the applicable requirements of Title 5 and Title 6 of KCC.*

2. The City Council feels the site *is/is not* physically suitable for the proposed development.

**Comment:** *The 60.85-acre (approximate) site does/does not appear to be suitable for the proposed development.*

3. The preliminary plat request is not likely to cause substantial environmental damage or avoidable injury to wildlife or their habitat.

**Comment:** *The land to be developed is not used as wildlife habitat. Roads, driveways, family units and open spaces are planned for construction according the City and ACHD requirements and best practices and will therefore not cause environmental damage or loss of habitat.*

4. These applications *are/are not* likely to cause adverse public health problems.

**Comment:** *The project would connect to public sewer and potable water systems, therefore eliminating the occurrence of adverse public health problems.*

5. The application appears to avoid detriment to the present and potential surrounding uses; to the health, safety, and general welfare of the public taking into account the physical features of the site, public facilities and existing adjacent uses.

**Comment:** *The preliminary plat request considers the location of the property and adjacent uses. The adjacent uses are medium density residential (Kuna City) and rural residential (Ada County).*

6. The existing and proposed street and utility services in proximity to the site are suitable *or* adequate for a commercial development.

**Comment:** *Correspondence from ACHD and Kuna Public Works confirms that the streets and utility services are suitable and adequate for this project.*

**J. Commission's Recommendation:**

Based on the facts outlined in staff's report and public testimony as presented, the Planning and Zoning Commission of Kuna, Idaho, hereby recommends *approval* of Case Nos. *18-06-AN (Annexation) and 18-08-S (Preliminary Plat)*, a subdivision request from Jane Suggs with WHPacific, on behalf to Trilogy Development to annex and subdivide approximately 60.85 acres into 298 total lots (253 buildable commercial lots and 45 common lots), subject to the conditions of approval as listed in the staff report including two additional conditions:

- Applicant shall work with Ada County Highway District and City Staff regarding curb, gutter and sidewalk along Hubbard Road and Locust Grove Road. (#14)
- Applicant shall place sod wherever the landscape plan identifies "Lawn". Additionally, the applicant shall provide staff with an updated landscaping plan accommodating the required change. (#15)

**K. Council's Proposed Order of Decision:**

*Note: This motion is for approval/conditional approval/denial of the annexation and preliminary plat applications. However, if the City Council wishes to approve or deny specific parts of these requests as detailed in the report, those changes must be specified.*

Based on the facts outlined in staff's report and public testimony as presented, the City Council of Kuna, Idaho, hereby *approves/conditionally approves/denies* Case Nos. *18-06-AN (Annexation) and 18-08-S (Preliminary Plat)*, a subdivision request from Jane Suggs with WHPacific, on behalf to Trilogy Development to annex and subdivide approximately 60.85 acres into 298 total lots (253 buildable commercial lots and 45 common lots) subject to the following conditions of approval:

1. The applicant and/or owner shall obtain written approval on letterhead or may be written/stamped on the approved plans of the construction plans from the agencies noted below. All submittals are required to include the lighting, landscaping, drainage, and development plans. All site improvements are prohibited prior to approval of the following agencies:
  - a. The City Engineer shall approve the sewer hook-ups.
  - b. The City Engineer shall approve all civil plans. No construction, grading, filling, clearing or excavation of any kind shall be initiated until the applicant has received approval of the drainage plan.
  - c. Central District Health Department recommends the plan be designed and constructed in conformance with standards contained in, "Catalog for Best Management Practices for Idaho Cities and Counties".
  - d. The Kuna Fire District shall approve fire flow requirements and/or building plans. Installation of fire protection facilities as required by Kuna Fire District are required.
  - e. The Kuna Municipal Irrigation District and Boise Project Board of Control shall approve any modifications to the existing irrigation system.
  - f. Approval from Ada County Highway District (ACHD) shall be obtained and Impact Fees must be paid prior to *issuance* of any building permit(s).
  - g. All public rights-of-way shall be dedicated and constructed to standards of the City and Ada County Highway District. No public street construction may commence without the approval and permit from Ada County Highway District.
2. Installation of service facilities shall comply with the requirements of the public utility or irrigation district providing the services. All utilities shall be installed underground, see **KCC 6-4-2-W**.
3. Compliance with Idaho Code, Section §31-3805 pertaining to irrigation waters is required. Irrigation/drainage waters shall not be impeded by any construction on site. Compliance with the requirements of the Boise Project Board of Control is required.

4. When required, submit a petition to the City (as necessary, confirmed with the City engineer) consenting to the pooling of irrigation surface water rights for delivery purposes and request to annex the irrigation surface water rights appurtenant to the property over to the Kuna Municipal Pressure Irrigation system of the City (KMID).
5. Connection to City Services (Sewer, Water, Pressurized Irrigation) is required. The applicant shall conform to all corresponding Master Plans.
6. Curb, gutter and sidewalk (attached and detached) shall be installed in accordance with Kuna City Code on the interior of the development and along the site's frontage on Stroebel Road.
7. Applicant shall work with staff in order to provide locations of street lights as required by Kuna City Code.
8. Street lights for the site shall be LED lighting and must comply with Kuna City Code and established Dark Skies practices.
9. Fencing within and around the site shall comply with Kuna City Code (Unless specifically approved otherwise and permitted).
10. All signage within/for the project shall comply with Kuna City Code and shall be approved through the design review process.
11. All required landscaping shall be permanently maintained in a healthy growing condition. The property owner shall remove and replace unhealthy or dead plant material within 3 days or as the planting season permits as required to meet the standards of these requirements. Maintenance and planting within public rights-of-way shall be with approval from the public entities owning the property.
12. If any revisions are made, the applicant shall provide the Planning and Zoning Staff with a revised copy of the preliminary plat.
13. The land owner/applicant/developer, and any future assigns having an interest in the subject property, shall fully comply with all conditions of development as approved by the City Council, or seek amending them through public hearing processes.
14. Applicant shall work with Ada County Highway District and City Staff regarding curb, gutter and sidewalk along Hubbard Road and Locust Grove Road.
15. Applicant shall place sod wherever the landscape plan identifies "Lawn". Additionally, the applicant shall provide staff with an updated landscaping plan accommodating the required change.
16. Developer/owner/applicant shall follow staff, City engineers and other agency recommended requirements as applicable.
17. Developer/owner/applicant shall comply with all local, state and federal laws.

**DATED** this 5<sup>th</sup> day of March, 2019.



# City of Kuna

Kuna City Council

## Proposed Findings of Fact and Conclusions of Law

Public Hearing Case No. 18-06-AN and 18-08-S  
P.O. Box 10  
Phone: (208) 922-5274  
Fax: (208) 922-5989  
www.Kunacity.id.gov

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Based upon the record contained in Case Nos. 18-06-AN and 18-08-S including the Comprehensive Plan, Kuna City Code, Staff's Memorandums, including the exhibits, and the testimony during the public hearing, the Kuna City Council hereby *approves* or *denies* the Findings of Fact and Conclusions of Law, and conditions of approval for Case Nos. 18-06-AN and 18-08-S, a request from WHPacific to annex two contiguous parcels comprising of approximately 60.85 acres on East Hubbard Road and North Locust Grove Road into Kuna City Limits with an R-8 zoning designation; and to subdivide the 60.85-acres into 298 total lots, consisting of 253 single family buildable lots, and 45 Common lots.

*If the City Council wishes to approve, deny or modify specific parts of the Findings of Facts and Conclusions of Law as detailed below, those changes must be specified.*

1. *Based on the evidence contained in Case Nos. 18-06-AN and 18-08-S this proposal does generally comply with the City Code.*

**Staff Finding:** *The applicant has submitted a complete application, and following staff review for technical compliance the application appears to be in general compliance with the design requirements, public improvement requirements, objectives and considerations listed in Kuna City Code Title 5 and Title 6.*

2. *The public notice requirements have been met and the neighborhood meeting was conducted within the guidelines of applicable Idaho Code and City Ordinances.*

**Staff Finding:** *Neighborhood Notices were mailed out to residents within 400-FT of the proposed project site on February 13, 2019 and a legal notice was published in the Kuna Melba Newspaper on February 13, 2019. The applicant posted sign on the property on February 21, 2019.*

3. *Based on the evidence contained in Case Nos. 18-06-AN and 18-08-S, this proposal does generally comply with the Comprehensive Plan.*

**Staff Finding:** *The Comp Plan has listed numerous goals for providing a variety of housing densities and types to accommodate various lifestyles, ages and economic group in Kuna. The proposed zoning designation is R-8 (Medium Density Residential). The Comp Plan Map designates this property as Medium Density.*

4. *The contents of the proposed preliminary plat application does contain all of the necessary requirements as listed in Kuna City Code 6-2-3: - Preliminary Plat.*

**Staff Finding:** *Review by Staff of the proposed preliminary plat confirms all technical requirements listed in KCC 6-2-3 were provided.*

5. *The availability of existing and proposed public services and streets can accommodate the proposed development.*

**Staff Finding:** *Correspondence from ACHD and Kuna Public Works confirms that the streets and utility services and suitable and adequate to accommodate the proposed project.*

6. *The proposed development is continuous with Master Utility Plans (Sewer/Water/Pressurized Irrigation).*

**Staff Finding:** *Correspondence from Kuna Public Works recommends the applicant be required to conform to the Master Sewer Plan, Master Water Plan and Master Pressurized Irrigation Plan, therefore satisfying this requirement.*

7. The public *does* have the financial capability to provide supporting services to the proposed development.

**Staff Finding:** *Throughout the development of the project and beyond, connection fees, impact fees (Fire, Park and Ada County Highway District), and property taxes will be collected, therefore satisfying the financial capability to provide supporting services.*

8. The proposed project *does* consider health and safety of the public and the surrounding area's environment.

**Staff Finding:** *Connection to City services, as well as other public improvements such as streetlights, fire hydrants, sidewalks, bike lanes, etc. are required to be implemented as a part of this project. No major wildlife habitats will be impacted by the proposed development.*

9. All private landowners *have* consented to annexation.

**Staff Finding:** *An affidavit of legal interest was signed by TJ and Elaine Johnson allowing WHPacific to act on their behalf of this project, therefore consenting to the annexation of the proposed project site.*

10. The proposed project lands *are* contiguous or adjacent to property within Kuna City Limits.

**Staff Finding:** *The parcel is contiguous with City limits to the north.*

**DATED** this 5<sup>th</sup> day of March, 2019.

# SURROUNDING PROJECTS MAPS

**Patagonia Subdivision**  
 Acreage: 150.35  
 Zoning: R-6 (Medium-Density Residential)  
 Project Summary: 470 residential lots & 18 common lots  
 Proposed to be completed in seven phases  
 (Currently three phases have been recorded).

**Merino Cove**  
 Acreage: 6.8 Acres  
 Zoning: R-6 (Medium-Density Residential)  
 Project Summary 25 residential lots  
 Proposed to be completed in one phase  
 (Subdivision has not recorded)

**Greyhawk Subdivision**  
 Acreage: 80.18 Acres  
 Zoning: R-6 (Medium-Density Residential)  
 Project Summary 317 residential lots and 18 Common Lots  
 Proposed to be completed in eight phases  
 (Subdivision has recorded its final phase)

**SUBJECT SITE**

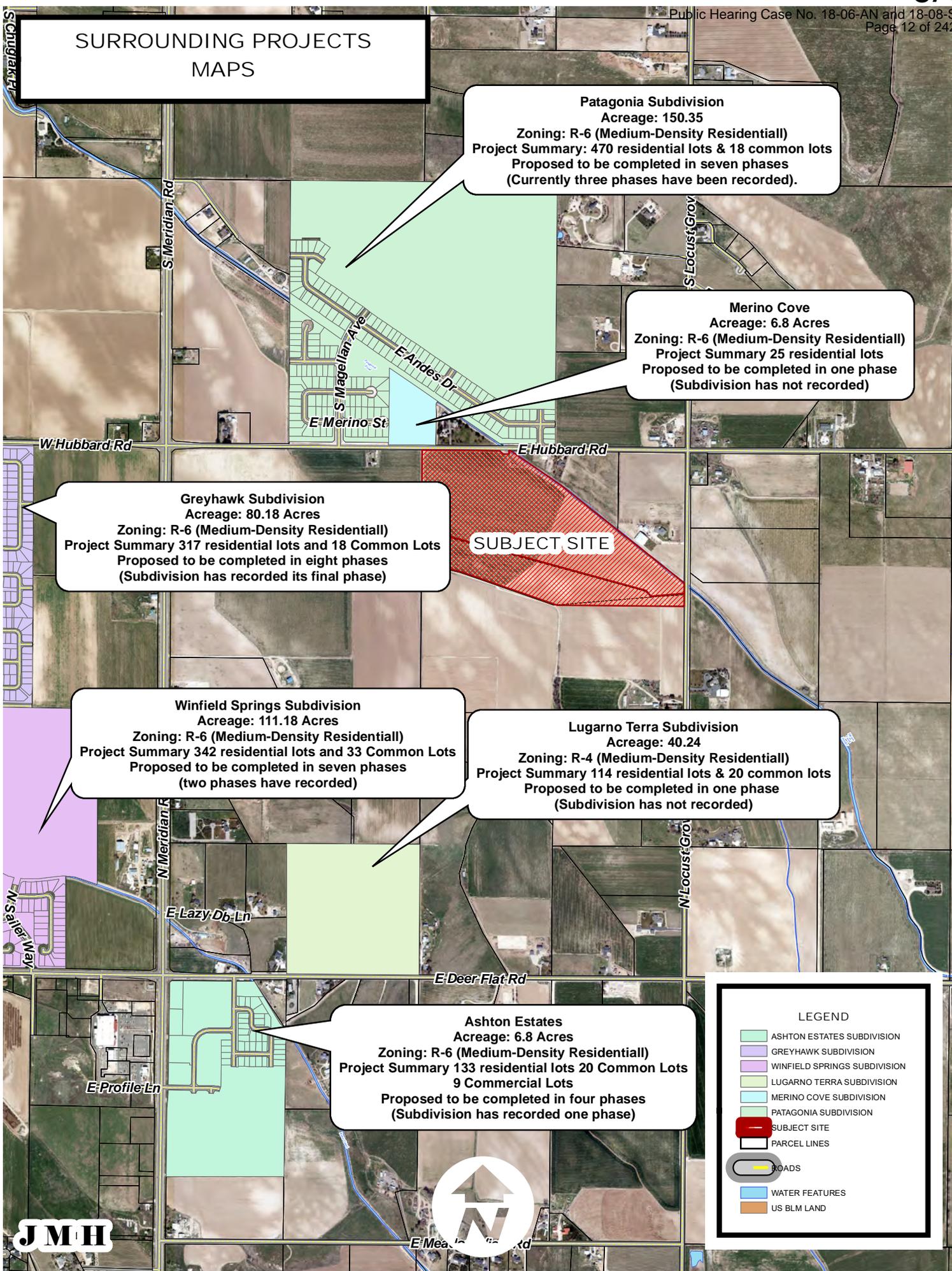
**Winfield Springs Subdivision**  
 Acreage: 111.18 Acres  
 Zoning: R-6 (Medium-Density Residential)  
 Project Summary 342 residential lots and 33 Common Lots  
 Proposed to be completed in seven phases  
 (two phases have recorded)

**Lugarno Terra Subdivision**  
 Acreage: 40.24  
 Zoning: R-4 (Medium-Density Residential)  
 Project Summary 114 residential lots & 20 common lots  
 Proposed to be completed in one phase  
 (Subdivision has not recorded)

**Ashton Estates**  
 Acreage: 6.8 Acres  
 Zoning: R-6 (Medium-Density Residential)  
 Project Summary 133 residential lots 20 Common Lots  
 9 Commercial Lots  
 Proposed to be completed in four phases  
 (Subdivision has recorded one phase)

**LEGEND**

- ASHTON ESTATES SUBDIVISION
- GREYHAWK SUBDIVISION
- WINFIELD SPRINGS SUBDIVISION
- LUGARNO TERRA SUBDIVISION
- MERINO COVE SUBDIVISION
- PATAGONIA SUBDIVISION
- SUBJECT SITE
- PARCEL LINES
- ROADS
- WATER FEATURES
- US BLM LAND



**CITY OF KUNA  
PLANNING & ZONING COMMISSION**

**MEETING MINUTES  
Tuesday, January 22, 2019**

PZ COMMISSION MEMBER	PRESENT	CITY STAFF PRESENT:	PRESENT
Chairman Lee Young	X	Wendy Howell, Planning Director	X
Commissioner Dana Hennis	X	Troy Behunin, Senior Planner	Absent
Commissioner Cathy Gealy	X	Jace Hellman, Planner II	X
Commissioner Stephen Damron	Absent	Sam Weiger, Planner I	X
Commissioner John Laraway	X		

**6:00 pm – COMMISSION MEETING & PUBLIC HEARING**

Chairman Young called the meeting to order at **6:00 pm**.

**Call to Order and Roll Call**

**1. CONSENT AGENDA**

Meeting Minutes for January 8, 2019.

**Findings of Fact and Conclusions of Law** For 18-07-AN (Annexation) & 18-05-ZC (Rezone) – Bodahl-Stiner Annexation and Rezone.

*Commissioner Hennis Motions to approve the consent agenda; Commissioner Gealy Seconds, all aye and motion carried 3-0.*

**2. PUBLIC HEARING**

**18-06-AN (Annexation), 18-08-S (Preliminary Plat) & 18-35-DR (Design Review)** – Ledgestone Subdivision; On behalf of Trilogy development, Inc., Jane Suggs, with WHPacific requests to annex two contiguous parcels comprising of approximately 60.85 acres on East Hubbard Road and North Locust Grove Road into Kuna City Limits with an R-8 zoning designation; and to subdivide the 60.85-acres into 298 total lots, consisting of 253 single family buildable lots, and 45 Common lots. The subject sites are adjacent to the southwest corner of East Hubbard Road and North Locust Grove Road, Kuna, ID 83634, within Section 18, Township 2 North, Range 1 East; (APN's S1418121126 & S1418123400).

**Jane Suggs:** Jane Suggs, WH Pacific, 2141 Airport Way. I represent Trilogy Development and Ledgestone Subdivision. We agree with all conditions in the staff report. We are asking for an annexation into Kuna of R-8. We have a property just over 60 acres and a density of just over 4 units per acre. We are below the Mason Creek Feeder. This density is approved in the Comprehensive Plan and the future Comprehensive Plan. The preliminary plat drawing shows alley loads. We are on the south portion of this site introducing alley-loaded products. The garages are accessed from the alley and the rear. That kind of design allows us to build lots that are 40 feet wide. The 40-foot-wide lot doesn't have a garage in the front like you normally see. You're probably familiar with alley loads in other communities. The lots are typically 4400 square feet. The can provide a garage and a 20-foot apron behind the garage on the property. R-8 is the only type of zoning that allows you to do the alley-load lot. I'll point out that R-8 seems like a high density, but we're tying that density right to that plat. Unlike the rezones that you've approved recently without a plat, which can be difficult, this one is tied directly to that plat. This home design is very desirable, it promotes a very nice streetscape so when you go down the street, you see the front houses and front porches. There is very little yard maintenance. There will be side yard separation. You will have side yard and a little front yard. The backyard will just be the side and the apron. There is no parking in the alleyways. The alley loaded homes make up 20 percent of all the homes there. The average lot size of the standard lots is 6800 square

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feet. The landscape plan shows that 14 percent of the site is landscaped. This includes some of the buffers that are along the streets. If you take out the streets which are basically landscaped areas, we still have over 10 percent of usable open space. The most prominent feature of our landscaping of course is the regional pathway along the Mason Creek Feeder. The developer is going to build a pathway that's going to be matching the city's recreation and pathways master plan. We will build over 2,000 feet of your master plan pathway along Mason Creek. Once it's completed, it will be dedicated to the City. There are other landscaped areas throughout the subdivision. We have a centrally located park that will have a top lot, a gazebo, and a half-basketball court as well as open areas for playing. There are five other additional green spaces around. They will be passive landscaping areas. When you want to meet a neighbor, there's a place to meet. Stroebel Road will be the main entrance, and there will be open space there. We are extending all utilities to and through the site. We did a traffic impact study and it was reviewed by ACHD and ITD, we agree with all of the conditions they have set. One condition from ITD is to fund a right-turn lane as you're going north on Highway 69 turning right onto Hubbard eventually. We have a phase map that shows it's done in five phases. There will be a four-way stop sign at Hubbard and Locust Grove. There will be a couple letters in your package from neighbors. We are building this development at the low end of the Comprehensive Plan for medium density. This goes up to seven to eight units per acre. We have a true mix of homes and sizes. **C/Laraway:** I noticed on this plat that you're requesting R-8 because of the specialty type construction. You only have six blocks with this alleyway. Will others be R-8 also? **Jane Suggs:** We have a few blocks with the R-8 size lots, the rest will be R-6 and R-4 lots. **C/Laraway:** The whole plat will be rezoned as R-8, correct? **Jane Suggs:** We're not zoning and coming back with a change, we will tie that condition of approval and rezone this with that plat. **C/Laraway:** I noticed the staff report there was discussion about street lights that will be later talked about. Will there be lights in the alley? **Jane Suggs:** There will generally not be lights in the alley. The lights generally stay on the street and on intersections. I've seen this in the past, where sometimes people put lights on the garages so that whenever they drive up, there are some lights. Typically, there are no street lights in the alley. **C/Laraway:** Will there be any driveways in the alley, or will it be a property line garage? **Jane Suggs:** It will be 20 feet. It will be a 20-foot alley, a 20-foot apron, and then a garage. You have not only garage parking but 20 feet in an apron. When you don't have garages on the street, you have a lot of parking. **C/Laraway:** Who will do the snow removal in the alleys? They will be public alleys, and we're building this to public alley standards. **Jace Hellman:** Chairman, Commissioner, for the record, Jace Hellman, Kuna Planning and Zoning Staff 751 W. 4<sup>th</sup> St. The application before you this evening is for an annexation Preliminary Plat and Design Review. The applicant requests to annex two contiguous parcels comprising of approximately 60.85 acres on East Hubbard Road and North Locust Grove Road into Kuna City Limits, with an R-8 zoning designation, and to subdivide the 60.85 acres into 298 total lots consisting of 253 single family buildable lots and 45 common lots. The project is proposed to be completed in five phases. Although the applicant has proposed R-8 as the zoning classification, the proposed density is actually only 4.15 dwelling units per acre. However, the dimensional lot standards of an R-8 designation allow the applicant to provide a new type of home product to the City. 53 buildable lots are proposed as alley loaded homes. These are single family homes, however the driveway and garage access will be available from the rear of the house via a public alley way. Applicant is constructing a new section of Stroebel Road along the east property line, which will serve as the main entrance into the Project. Stroebel Road is listed as a north-south collector according to Kuna's Street Circulation Map. Staff would recommend the applicant work with Staff and Ada County Highway District to ensure the new collector street is constructed to each agency's standards. A secondary access has been proposed onto existing Locust Grove Road. However, this access point is not projected to be constructed until a later phase of site development. In the meantime, the applicant proposes a temporary emergency access point on to Hubbard Road. Several stub streets have been provided as a part of this project, two (2) to the south, including the stub of future collector Stroebel Road, and one (1) to the northeast, across the Mason Creek Feeder. The applicant will be required to install a sign at the terminus of each of the roads stating these roads will continue in the future. Staff will defer the applicant to comments provided by ACHD for preferred language. A design review application for common area landscaping and open space was included as a part of the overall application. Applicant proposes several dedicated landscaping and buffer areas throughout the project.

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Approximately 14%, or 8.51 acres of the project has been dedicated to common area; and 10.2%, or 6.2 acres of the project have been determined as "usable open space". Staff find the proposed landscaping, buffers and common space to be in compliance with Kuna City Code. However, staff would recommend the applicant be conditioned to place sod wherever the landscape plan identifies "Lawn", and provide staff an updated landscaping plan accommodating requested change. The Recreation and Pathway Master Plan Map identifies a regional green way corridor pathway along Mason Creek on the South, which would be the applicant's property. The applicant proposes the construction of over 2,000 linear feet of regional public pathway along the south side of Mason Creek along the northeastern edge of the subject site's property line, which will connect with the pathway installed by Patagonia to the North. The property was posted and 400-foot notices were sent out to all people within 400 feet of the property. An additional courtesy notice was sent after this was tabled prior. And with that I will stand for any questions you have. **C/Young:** We will open the public testimony at 6:18. **Jane Suggs:** I don't have anything to rebut, but I want to respectfully request your recommended approval of the annexation, rezone and the design review for the landscaping. **C/Gealy:** Ms. Suggs, can you address the concerns in the letters that were sent in? **Jane Suggs:** Mr. Browning lives across Hubbard Road. He had some questions about landscaping. We are doing our landscaping to meet code. The only difference I see is that for Patagonia, ACHD asked us to put our landscaping in but not put curb and gutter in, because of future widening of Hubbard Road. It looks like Patagonia chose to put curb and gutter in, which will likely have to come out whenever Hubbard is improved. We put in the widened street, a little bit of the shoulder. We will then put in our sidewalk and have a 30-foot buffer like Patagonia has. That is the only difference that I see. He asked for limited access to Hubbard Road and we will only have one temporary access. Our main access is off of Stroebel Road, which is a new road that we will be building. You'll see there at the top of the project. There is a temporary access, because we will build likely more than 30 buildings with one access. If you go to the park area to Hubbard Road, lot 14 will actually be an emergency access until we get the road connecting to Locust Grove. He was concerned about accesses off of Hubbard Road, and that's a temporary access. He talked about the walking path matching and we will meet the requirements for the pathway just like Patagonia did. We will not have a pool and clubhouse. When I looked up Patagonia, they have over 400 homes in there. The pool and clubhouse thing is one of those things that depends on how many units that you're building. The type of HOA fees that you want to homeowners to take to pay. We have found that the walking path and the smaller parks and places where people can gather are more popular. The Garretts live right across the canal from us. They have that nice piece of property right there on the corner of Locust Grove and Hubbard. They are in the southwest corner of Locust Grove and Hubbard. They are right across the canal. They are concerned mostly because we are coming in. They are in the RR zone which is in the county. Patagonia is here, that area is changing. We do have the infrastructure to sustain this. He comments that he has issues with that. We do have the sewer and water irrigation capacity to sustain this. We will be building oversized pipes through the property to deal with expansion east of us. The road coming off of Locust Grove is right on the property boundary. The next person can actually use that road. Those are the forwarding thinking things that the staff asked us to do. We're very much in keeping with the Comprehensive Plan, yes things are changing a bit. I left a message for Mr. Browning and didn't have a phone number for the Garrett's, but I'd be happy to chat with him too. **C/Young:** I will close the public testimony at 6:25. **Jace Hellman:** Regarding the curb and gutter, there is proposed Condition No. 6 that requires that it is installed per Kuna City Code, so if the body would like to try and enforce that. **C/Hennis:** I like the amenities provided in the center along with the path. **C/Young:** For the site, the overall density is closer to an R-4 than R-8, which for the alleyway product is not just a 20-foot little slot. **C/Gealy:** I appreciate the variety of lot sizes and the variety of housing product that can be made available. There are some larger and smaller lots. I appreciate the open space and the pathway along the canal. Another concern I have is transitional lots, but I think with the alley-loaded product where it is, it makes sense to have smaller single family lots around that area on that end of the development and then have the larger lots on the other end. I do have a question regarding the curb and gutter. It is included as a condition of approval that curb, gutter and sidewalk shall be installed in accordance with Kuna City Code. You did say that you were accepting all of the conditions of approval. **Jane Suggs:** Commissioners, when I read that I was thinking about the curb, gutter and sidewalk being inside the property and not that along

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the arterials. Again, because that curb and gutter hasn't been set and designed by ACHD for any kind of specific elevations and things, it often comes out. That's why ACHD doesn't ask us to do the curb, gutter and sidewalk. I can only ask that the condition be adjusted to say interior, but I did not say that at the beginning. We don't do a lot of curb, gutter and sidewalk. We were looking at that as just being in the interior, so we would ask that this condition be changed to curb, gutter and sidewalk inside the subdivision. **C/Hennis:** You said that ACHD has this in the work plan? **Jane Suggs:** I don't know that they do, I haven't looked. The elevations of what the curb and gutter would look like for drainage. You put that in, and then you prevent drainage issues on the corners. They typically don't like to put that in bits and pieces. Once you pick up that drainage, it goes on and there's no system to take care of it. They wait until on arterials, a whole half-mile of improvements. I'm sorry I don't have the five-year work plan in front of me. I don't think that we've got elevations on curb, gutter and sidewalk. ACHD did not require it. **C/Gealy:** You are planning to put curb, gutter and sidewalk along Stroebel, correct? **Jane Suggs:** yes, we are going on Stroebel as a mid-mile collector. **C/Gealy:** The other condition that was requested from staff was that you have sod in all cases. **Jane Suggs:** Absolutely, we'll resubmit a plan showing where it says lawn, we'll put sod in. They're just asking to be sodded instead of seeded, because sod takes a little faster. We agree to that. I would ask that this be changed so that the interior streets and Stroebel will be considered the curb, gutter, and sidewalk. **Wendy Howell:** This very scenario with curb and gutter has come up in the past, where they do not have the elevations yet. ACHD has worked with the City and the developer so that curb and gutter could be put in. Maybe change the condition that developers work with the City and ACHD regarding curb and gutter, so that come to the best solution that we can. Our direction from Council is if it's not in the five-year work plan, they need to put it in as our ordinance states. **Jane Suggs:** I can accept a work-with condition. **C/Young:** As far as the curb and gutter goes, we've not done that when we know that Wendy said the five-year plan is coming and we're not tearing in brand new stuff, knowing what's developing to our west and to our east. It's required for City Code. **C/Hennis:** Well yes, but then again also if you don't have a road section plan as to where to put it, then it's going to be kind of difficult too. The condition to work with the City and ACHD would give a viable solution in the end. **C/Gealy:** We'll know more in five years than we do now since it's phased.

*Commissioner Gealy motions to recommend approval of Case No. 18-06-AN & 18-08-S to the City Council with the conditions as outlined in the staff report; With an modification to Condition No. 6 that the developer will provide curb, gutter and sidewalk on the interior of the development and along Stroebel Road, and the developer will work with ACHD and City staff regarding the curb and gutter along Locust Grove and Hubbard Road; And an additional condition that on the landscaping plan in cases where "lawn" is mentioned, the developer would use sod; Commissioner Hennis seconds, all aye and motion carried 3-0. Commissioner Gealy motions to approve 18-35-DR with the conditions as outlined in the staff report; With an modification to Condition No. 6 that the developer will provide curb, gutter and sidewalk on the interior of the development and along Stroebel Road, and the developer will work with ACHD and City staff regarding the curb and gutter along Locust Grove and Hubbard Road; And an additional condition that on the landscaping plan in cases where "lawn" is mentioned, the developer would use sod. Commissioner Hennis seconds, all aye and motion carried 3-0.*

**18-07-S (Preliminary Plat)** – Arbor Ridge Villas Subdivision; On behalf of Endurance Holdings, LLC., B&A Engineers requests to subdivide approximately 2.1-acres into eleven (11) total lots, consisting of eight (8) buildable multi-family lots, and three (3) common lots and has reserved the name Arbor Ridge Villas Subdivision. The subject site is located on the northwest corner of West Ardell Road and North School Avenue, Kuna, ID 83634, within Section 14, Township 2 North, Range 1 West; (APN# S1314244201).

**David Crawford:** I'm David Crawford with B&A Engineers, here representing the applicant for Arbor Ridge Villas Subdivision. Arbor Ridge has been completing developments over the past few years. We're up to phase four, and we're currently working on Arbor Ridge No. 5 and 6 concurrently. Arbor Ridge Villas will be a re-subdivision of three commercial lots in Arbor Ridge Subdivision No. 5. Arbor Ridge Subdivision No. 5 is currently beginning

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construction and will complete the road improvements for the full widening of School Avenue to the north boundary and Ardell Road clear over to Walnut Creek Subdivision. That will complete the widening of Ardell Road. Ardell Road has been a partial widening for many years. This would complete that widening. Ardell Road has been fully completed east of the site through Ardell Estates Subdivision. Ardell Estates Subdivision No. 3 is going to be approved and going to construction this year. That will complete a full travel width road section all the way to Linder Road. Arbor Ridge Villas contemplates taking the three lots that were created in Arbor Ridge Subdivision No. 5. Placing lots around the buildings that were approved by the Planning and Zoning Commission in October. We're not really expanding the use, we're just allowing the buildings to exist on their own lot rather than having multiple buildings on a total of three lots. It's a little complicated, we're doing work up front and then just platting around what we're doing. It allows for individual ownership of the buildings rather than having multiple buildings on one lot. There was a letter that we received from the Planning Staff dated January 14 from the Ewings. Some of the developments like Mineral Springs that are kind of close and cut through Ardell Road and then on out to Ten Mile. With the completion of Ardell Road and Linder Road we leave a lot of that alleviated. The connection of School Street to the Arbor Ridge first phase of development will also allow traffic to go out to Hubbard Road. I believe that a lot will be alleviated to some regard, but we can't say that nobody is going to go through there. We will allow at least two connection points for the residential then for these developments to occur. Arbor Ridge Subdivision was a Planned Unit Development that was approved several years ago. It dedicated a five-acre park to the City with the first phase of development directly south of Hubbard Road. With that development, we are extremely lucky to also work with the Boise Project Board of Control to allow a twelve-foot pathway to connect all the way from Ardell Road through phases of Arbor Ridge subdivision. Arbor Ridge Phase 6 will complete that pathway system to connect all the way from Ardell Road to Kuna City Park that is located in Arbor Ridge Subdivision No. 1. Arbor Ridge Subdivision No. 5 is a very large lot subdivision. Most of the lots exceed 12-14,000 square feet. It was part of the original PUD. Down at the intersection, on the northwest corner of Ardell and School Street, this was slated for commercial. There was also a few years ago, a development with the re-subdivided Mineral Springs Subdivision. This created Mineral Springs Subdivision No. 2. That was also slated for commercial construction but found not to be viable. They made it into a single-family home development and rezoned that. This is the last portion of commercial land that existed within the approvals. I'll stand for any questions you may have, but we respectfully request your approval for the Arbor Ridge Villas tonight. **C/Gealy:** Is it your intention then to construct the pathway along the canal that runs to the Northwest portion of the commercial piece? **David Crawford:** It was not our intent to install the pathway along there, we are actually creating a fencing boundary that is required to maintain the access from the Boise Project Board of Control. I don't know exactly what the pathway system would look like, that hasn't been discussed as a condition of approval. I do know that currently, the Boise Project Board of Control doesn't allow construction of pathways within the right of way. I think it will be the state lateral. The only thing that we'll be able to do is put a gravel pathway down. It wouldn't allow the construction of asphalt. If that condition were imposed in this development, it would be a condition that we couldn't comply with. This is because they simply wouldn't allow it. It's not that we don't want to do it, it's just that we can't get their permission to do it. **Jace Hellman:** Chairman, Commissioners, for the record, Jace Hellman, Kuna Planning and Zoning Staff 751 W. 4<sup>th</sup> St. The application before you this evening is a request for a preliminary plat recommendation to the City Council of approval or denial. The applicant is proposed to subdivide approximately 2.1-acres into eleven total lots, consisting of 8 buildable multi-family lots and three common lots. The project has reserved the name Arbor Ridge Villas. The site of the project is located on the northwest corner of Ardell Road and School Avenue. The proposed project site was identified in original Arbor Ridge Villas Subdivision Planned Unit Development (PUD), which was approved in 2003. The subject site is currently zoned R-4, and is identified as medium-density residential on the Comprehensive Plan Future Land Use Map. However, following the original approval of the Arbor Ridge PUD, a development agreement was signed, which guaranteed the proposed project site area the zoning designation of C-1 (neighborhood commercial). If approved, the applicant will have to record the separation of the parcel, and staff will provide a rezone ordinance reflecting the approvals laid out in the signed development agreement. Access to the site is proposed via two full access driveways. The first is situated on West Ardell Road and the second is

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situated on North School Avenue. Additionally, the applicant will be constructing the remain half of the street sections of West Ardell Road and North School Avenue that fronts the proposed project site as a part of the required public improvements listed in Kuna City Code. Currently, there is no curb, gutter or sidewalk along the property's frontage. And both Ardell and School Road are unimproved roadways. Staff would note that both West Ardell Road and North School Avenue are listed as Major Collectors according to the City of Kuna's Street Circulation Map. Kuna City Code requires the right-of-way improvements such as street widening, curb, gutter and sidewalk along a property's frontage as a part of the subdivision process. Following email correspondence with the project's engineer on January 16, 2019 (exhibit D3) confirms all required right-of-way improvements will be installed with the installation of Arbor Ridge phases five and six prior to the re-subdivision and development of the proposed project. The Recreation and Pathways Master Plan Map indicates a future trail along the eastern boundary of the proposed project site along the State Lateral. However, the preliminary plat does not identify the location of a trail on site. It is the City's goal and desire to increase the number of trails and pathways in Kuna. Staff highly recommends the developers be conditioned to comply with the Master Plan's goals and design and construct the identified pathway along the State Lateral to the standards set in Kuna City Code 6-4-2 (Required Public Improvements). Notices were sent out to people that were within 400 feet of the property as mandated by state code which requires 300 feet. The property was noticed and an ad was run in the Kuna Melba News. With that I will stand for any questions you may have. **C/Hennis:** With regards to the applicant, he discussed having a pathway along the canal, what's the city's stance on that with being able to allow anywhere along that easement, do you know otherwise a method to where we can provide a pathway along there? **Jace Hellman:** Typically, it's been done in other subdivisions and it's been done on where they've allowed it with it on their own private property rather than not on their property. That might be an option to explore. This is why we could do a condition that they work with staff and the Boise Project to get that goal of getting a pathway in there. **C/Young:** We'll open the public testimony at 6:48. **Tim Donka:** I live at 2332 North Corktree Way. First and foremost, I'd like to thank you for taking the time to hear me out. I did take the time to look at the vision of Kuna. I notice that it's been updated several times throughout the last couple years back in 2003 and as current as 2015. I do understand that there must a balance made between affordable housing and apartment complexes or lower income. The vision I saw that stood out in my mind was building stronger communities. I oppose apartment complexes. I know that these won't be a concern for Planning and Zoning. I live in Arbor Ridge Subdivision and I enjoy the pathway that's mostly completed. The opposition that we've had in the past is people that have come forth and said I don't want an apartment close to my house. Those don't build stronger neighborhoods. I do appreciate the fact that they will finish Ardell all the way through. If you guys do submit this and approve it. Right now, it's a one lane road. Multiple times, I've seen vehicles coming both directions and they've got nowhere to go. That is a plus for that. The only thing concerning with this was that it's two acres and they're dividing it into 11 lots, three of them being open space which I assume is parking. You can't put eight apartments in there and then tell me that there's going to be a park there. There is a park that's fairly close, and it's within maybe five or six blocks. It would be nice to have more green space inside there, besides just the huge asphalt section. I don't have a plat of what they have or what it's going to look like. **Ian Gordon:** I live at 2246 N Citrine, which is on the corner of Ardell and Citrine. I'm opposed to the zoning being changed on that property. I'm opposed to the construction of apartments. It is zoned for multifamily right now rather than single-family, which I had initially thought was the planned use. Was this changed recently? **C/Young:** As staff indicated as a part of the PUD approved in 2002, that parcel would be used as a C-1 commercial property. **Ian Gordon:** I moved here recently like many of the people behind me. Many of us came from high-density cities with a lot of problems and we moved here for a different experience. We moved to support the community. I spent \$500,000 on my house with the expectation that other houses would certainly be built. As you're planning another community, you would take into account keeping comparable property values when you build a new subdivision. The most important thing for new home owners is to build equity in their house. We have a lot of senior citizens up here who brought their retirement. We've got a house that we want to appreciate as much as possible. The construction of our apartment building is going to depreciate our property. We're going right back to where we just left. We left 900 miles away to come here and be faced with the social

## CITY OF KUNA PLANNING & ZONING COMMISSION

### MEETING MINUTES Tuesday, January 22, 2019

problems that will be brought by having apartments in this area. This town has plenty of area to build apartments. Have an apartment section where all the apartments are. I don't see where there's good planning if you take a low value property, something that's turning people over month after month in our neighborhood. If you look at national statistics by the FBI, we will be faced by not only this traffic problem. The fact that this developer plans to finish Ardell Road is not really finishing it. Until Ardell is completed, there won't be a solution to that. **John Hoyt:** I live at 2251 N Citrine Avenue at the corner of Ardell and Citrine. My comments will echo Ian's. I do appreciate the fact that road improvement is part of this development. High density rental housing, rental housing in particular is incongruous with the adjacent properties. As you look around, there are new homes being built across School Ave. Mineral Springs has \$4-500,000 houses popping up. That property would be far better suited to the density that's already there. I don't know if we're talking about fourplexes, eightplexes, I don't know what the ultimate number of people that would be living in these eight buildings. If they're eightplexes, that's 64 families. That's totally incongruous to have 64 families living in two acres across from one guy who lives on half of an acre. I really think that regardless of how this was platted or thought of, in 2002 which is 16 years ago. There's no way that you thought this town was going to look like it does today. You have to change the plan as you go. We're talking about hopscotching two acres at a time. That doesn't make sense. This is not the type of property, not the type of development that needs to go on that property. **Zack Ricketts:** I live at 2597 N Snowbell. My concerns will centralize around safety, there are issues in this area. A lot of this has to do with traffic. This development at this location is putting the cart ahead of the horse. Infrastructure needs to be completed before such development should be considered. All collector roadways to this location. School Avenue from Ardell is not full width. School Avenue north of Ardell is a dead end. Ardell running east and west of School are dead ends. Access to this location from West, North and East requires cutting through subdivisions. Accessing this location via Arbor Ridge Subdivision creates significant concerns with existing driver behaviors. Speeding with the further development of the subdivision, there has been an increase on Quaking Aspen off of Linder and Kelson off of Hubbard. As you heard earlier, the developer plans on using Kelson as one of the cut-throughs for this subdivision or development to go North to Hubbard. That's already a road that's being used significantly to bypass the incomplete roads. This subdivision has a lot of on-street parking. That reduces the visibility to children and animals along the side of the road, significantly increasing the risk of an accident. With the on-street parking, the width of the roadways is reduced and, in some situations, there is not room for two-way traffic. The observation of stop signs near my residence and witnessing other behavior by other stop signs in the subdivision. There are three different behaviors. First, there are drivers that obey the law and stop. Second, there are drivers that roll through the intersection. Third, there are drivers that fail to observe the signs at all. I have personally been almost been involved in three accidents near my house, because people ignore stop signs in the subdivisions. With the addition of eight multifamily units in an area that does not have adequate roads or access, this would add traffic to roads that are not designed or completed as collector roadways as of yet. The Envision Kuna proposed plan does not include this location as a high-density residential area. I ask that you take these into consideration as you make the decision on this. **Tom Matus:** I live at 2164 N Star Garnet Drive, and I am on the Board of Directors of the Mineral Springs HOA and I'm a six-year veteran of the US Navy. I have a number of concerns that the previous opposition has been presented. I have outlined mine here with a few bullet points. Traffic on School Street is a major concern, and my concerns encompass neighborhood safety, increase flow on School Street through Walnut Creek Subdivision as well as Arbor Ridge Subdivisions. I don't mean to exclude the home values with an apartment complex going in. As a vested member of the community, I believe that structure, progress and growth are important. However, I believe that progress and growth should be monitored and thorough impact studies should occur prior to approving this type of project. The future development of other areas in Falcon Crest, which was a subdivision south of the tracks that was approved last week, leads me to believe that we have enough development going on. I think we want the community to stand out as well as a well thought out community with strong leaders and members backing it. We need to protect our community through its growth phase over the next decade and beyond. The area that the development is requesting does not need to be commercial and we do not need multifamily housing going in there. Do we really need to continue urban sprawl, hopscotching small areas

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without good planning and fore thought? The number of multifamily housing within Kuna now is pretty substantial. I believe inserting this project in the middle of three established subdivisions with housing prices between \$250-500,000 is a poor plan and at the least, will have a negative impact on all surrounding subdivisions. I urge you to make detailed observations when generating any changes or approvals to this request. How will this affect our school density. We're already at full capacity. My recommendation is plain and simple. At the very least, make this single-family homes and finish Ardell east and west. Go both directions before we start anything in that area. **Ben Steinbaugh:** I live at 2424 N Sunset Farm Avenue in Walnut Creek Subdivision. The widening of Ardell over to Walnut Creek is going to encourage cut-through traffic. This development is not consistent with all the other homes around it. As I understand it, the previous agreement in 2002 had a stipulation for some commercial lots, this wouldn't even be considered. This is because it's not consistent with everything that's changed and is going on forward. Mineral Springs lost a C-1 status and went to single-family. I believe that this piece of property should be developed as a single-family property and be consistent with Mineral Springs. One of the gentlemen and others up here I'm sure live in Mineral Springs. I don't have an issue with apartments, but an apartment complex should be on an arterial road such as Deer Flat or Ten Mile to support the amount of traffic and the amount of flow as a result of those people living there. This property would not be developed if it were started today. The only reason that's it's in consideration is because 16 years ago, someone thought well maybe we could develop this as a commercial. Maybe we could put something there. This development should be single-family. They wouldn't even consider making this a commercial property. This is dead-center in the middle of three subdivisions, please consider denying this. **John Sanders:** I live at 2217 N Citrine. First of all, the sign that was put up on Ardell, could we get that moved, because that's our passing lane. We can't get through if there's a car coming. I came from a place with apartments, and it brings crime and I don't have the facts to support that. There is no pride of ownership, I bought a house for \$360,000. You put apartments next door with 60-70 families, will they care about how the community looks? Will we have more police and fire to support that. Schools were mentioned, my wife is a school teacher. Are the schools going to be able to handle the additional children coming in? They're striking because they're overcrowded. Has that been considered? **Nicole Corney:** I live at 1340 W Oak Tree Drive in the neighboring Walnut Creek neighborhood. I am hoping that you will hold off this decision and vote no against it today. My first concern is the traffic impact. We have already seen already a giant increase in traffic cutting through our neighborhood in Walnut Creek from Mineral Springs or the southwest area of Arbor Ridge just to get to the Ten Mile area. If you're considering approving these, at least extend Ardell all the way to Ten Mile and all the way to Linder on both directions. Until roads are extended, I don't believe these should be even considered and I'm sure that these units will use our subdivision and Arbor Ridge as a cut-through to neighboring major roadways. These do not fit the characteristics of the surrounding area. Multi-family housing right in the middle of the developed neighborhood is awkward. This type of housing should be done closer to arterial roadways. Mineral Springs has half-acre custom homes right next to this valuing at almost \$500,000. This does not fit with the area, and this will surely bring down home prices close to it. Single-family homes are a much better fit for this specific piece of property in my opinion. Third, I'm very concerned about our already bursting schools. I would assume these would fall under Silver Trail Elementary boundaries. My kids have attended there for the last five years, and every year class sizes seem to get bigger and bigger. This year in particular, my son has been suffering greatly because of it. He is a fifth grader, but because of lack of budget, he is stuck in a class of half fourth and half fifth grade. The school tried to make it look appealing by putting a less kids in it to help with teachers. The principal said there is no money and nothing he can do. Something needs to be done about schools before we keep approving housing developments left and right. I feel that our children are suffering for it. Please consider these issues before approving this. **David Crawford:** I would like to thank everybody for coming out tonight and expressing their concerns with the growing pains. I would like to point out that we had two neighborhood meetings on this project. For the first meeting, two people came and expressed their approval for the project. For the second neighborhood meeting, nobody attended. I didn't get a copy of the letter, but if there was anybody who could get a hold of that, I might be able to address those concerns. Ardell and School Road have been built in bits and pieces. That's because the developer that improves the roads, such as in Mineral Springs Subdivision and in Walnut Creek Subdivision, we

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are the engineers of record. All those were built in pieces and parts as the lands developed. Arbor Ridge was a phased development. It was thought out with the comprehensive plan at the time, a traffic study was done. Part of a planned unit development is that a mix of housing types is brought. Arbor Ridge Subdivision did that. There was a park placed there and a pathway all the way along from Ardell Road to School will be completed and paved with Arbor Ridge Subdivision No. 6. It's interesting that the highway district doesn't fund the overall construction of roads that are adjacent to development. The developer builds these roadways as the phases are required. They are connected up, and eventually they'll connect. Those roads are dedicated to the public at each platted phase of the development. It should be noted that schools were primarily funded through taxes on housing. The more houses that come in and people get to enjoy the great City of Kuna from other states are actually helping to pay for schools. There was talk about high density housing and this not being the right area. This was part of the overall desire and one of the Comprehensive Plan goals from the City of Kuna to bring a mix of housing types. We have half-acre lots, R-4 and R-6 densities within this direct area. Arbor Ridge has a mix of housing types that range from R-4 designations and into Arbor Ridge Subdivision No. 5 where it's huge lots that are more consistent with the Mineral Springs. We always have a mix of housing types, and it's one of the objectives of the Comprehensive Plan. I think this fits it really well. Currently, School Street on-street parking was discussed as it reduces travel ways. There is no on-street parking allowed on any collector or arterial roadways. This project has all of it's on-street parking, and it was approved with the number of parking units that are required for the development. This is part of Arbor Ridge Subdivision No. 5. It's going to create the three lots where these buildings will be built, the Arbor Ridge Villas Subdivision plat is here just to create lots around those buildings. The development has been ongoing for quite some time. It's a test to the developer that's funding the project. He's been able to hang in there during the economic downturn that starved the housing in the community in about 2006. Much of this sat vacant and dormant until about 2013. We subsequently completed a phase of development annually as required, and this year alone we will complete two phases of Arbor Ridge No.5 and 6, to continue to well-planned and thought out community of Arbor Ridge Subdivision. There was enough urban development in the area and we have lots of urban sprawl. Arbor Ridge is pretty close to the city center now. This isn't really urban sprawl, we're filling in the middle. The comprehensive plan, the city's impact area, has grown remarkably over the years. Police, fire, and schools are all funded primarily through taxes that are brought by people who come here to enjoy these communities in our area. We're not putting the cart before the horse, the roadways of Ardell and School will be completed to the extent practical within the boundaries of the development to complete them to where they end. There's another property owner and when that property develops, those will be connected. With Arbor Ridge's Phases No. 5 and 6, that will create the connections through to Hubbard Road, Walnut Creek and over to Linder Road. School Street was slated as being under width, it's actually got full trap lanes all the way from Deer Flat to Ardell Road. It was constructed with more than 24 feet of paving, which allows two vehicles to pass each other. Ardell Road wasn't built quite to that same standard. It will be fully completed with these two projects, Arbor Ridge No. 5 and 6 and Ardell No. 3. All those are slated to be completed this year. I'm also aware of another development that's occurring west of Mineral Springs on the south side of Ardell that will build the rest of it out to Ten Mile. Development is what builds our roads, and those are dedicated to the highway district at the end of it. I've got to talk a little bit about our pathway before we run out of time. We're not opposed to putting in a pathway. The reality of it is, we end up in a situation where I was able to broker a deal with the Boise Project Board of Control with Hubbard where we were allowed to put in a pathway. When the gentleman that allowed to occur at the time is no longer there, and the Bureau of Reclamation Boise Project Board of Control, being the federal entity that it is, changed its mind on what we're allowed to do within the right of way to secure their approval. We can only get their approval to install gravel pathways. We're not opposed to doing that. The minute we start talking about asphalt, it's not going to happen. We had the same discussion with the Mineral Springs development to the state lateral. We created a large open space lot, we piped the thing the whole way through, almost 2,000 feet of piping. We created a large lot, and the only thing we were allowed to do was put grass in it. They absolutely denied having any pathway through there. The Boise Project actually abdicated its responsibility for maintenance to others. It's kind of an interesting scenario, and we're not opposed to putting in a pathway. I'm just concerned

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about what that pathway needs to look like. I'll stand for any questions you may have. **C/Hennis:** You're applying for eight buildable lots and three common lots, but to my understanding, this will be overseen by an HOA that has all that common area taken care of. This will not be up to each individual ownership to maintain their landscaping and common areas, correct? **David Crawford:** The developer that's doing this is very savvy and well-known. He does a lot of these, and he is very concerned about how his developments look. CC&Rs are being prepared so that these will be managed and the landscaping will be professionally maintained. **C/Laraway:** Here's a follow-up question, these apartment complexes are going to be for rent and not for sale, correct? **David Crawford:** Commissioner, Chairman, they are not condos, so there are eight buildings that are proposed on three lots in Arbor Ridge Subdivision No. 5. We are allowing ownership of those buildings to be determined. The developer will own them, but it's possible that he could sell those to other people. They'll have to tie it on to the HOA and CC&R's as well. **C/Laraway:** Could there be eight different owners running an HOA? **David Crawford:** It's possible. The CC&Rs will dictate how that operates. The CC&Rs dictate the maintenance obligations and the controls for building, maintenance and all that kind of thing. That will be handled by a professional management company. Somebody else could own a particular lot within there. **C/Hennis:** You'll have eight buildings, each which will have multiple units, which you have not determined at this point. That building will have an owner, but not each individual unit. Those will be a rental, but you'll have eight ownerships that are still tied to a common CC&R and HOA. **David Crawford:** Absolutely. **C/Laraway:** You mentioned that your development will have access to Hubbard Road, is that going to be through School Street, that they will extend up past the PI Stem school, or are you talking about running it back to the subdivision? **David Crawford:** Arbor Ridge Subdivision No. 5 and 6 are going to complete School up to the project's northerly most boundary. Where it will connect with another road that I think is Pin Cherry Street, which goes east and ends up over at Kelson up to Hubbard Road through Arbor Ridge Subdivision. **C/Young:** I'll close the public testimony at 7:28. Now is the Commission discussion. The police department, fire department and school district have all been noticed and received copies of the application and have had their chance to review and comment on that? **Jace Hellman:** Correct. The police department and fire department typically attend pre-ap meetings. That's usually when they provide comments. No comments were provided on this project. **C/Young:** Has ACHD also received application and had no comment on the application as well. I was trying to address some of the other points addressed in public testimony. **C/Hennis:** The multi-family housing currently is in coordination with that. The applicant tried to clarify it to, with respect to the streets being fixed and widened prior to the development going in, it's kind of hard for a developer to put that in until he's putting the development. My understanding from the applicant is that those roads will be put in as this is developed. You can't just go in and build the streets without the development going in. You can't build streets on something the applicant doesn't own. **C/Hennis:** He can only take it up as far as he can go. Concerns have been heard yes. **C/Young:** We did the design review on this not too long ago, which is why we have the preliminary plat before us today. **C/Hennis:** The audience has seen a lot of the preliminary plat. It doesn't appear. The one's that we've seen lately for this multi-family housing, I think it's laid out well with open space. We're not going to have a line of apartment buildings, it will be separated into little building pods with a lot of open space and green in between it. Internal parking will exist. Based on the sentiment that the audience has brought forth, you look at the actual plat, the applicant and the owner has tried to keep a lot of your concerns in mind. It's not by any means a boxy development. **C/Young:** We can't just change the zoning. It's an allowed use in this zone. **C/Laraway:** I think that we've asked for this in other multifamily complexes is that they have the open space. It would be nice if they had the amenities to put in those spaces to keep the kids in these areas. I'm assuming the park is a part of this, but that is still five blocks away. I don't think parents will want their kids running off down that far if they have some amenities in the complex itself. There seem to be a couple of larger areas that would easily hold something that would keep the kids there to entertain them. I don't know if that's unreasonable. **C/Young:** No, it's something we've discussed in the past. The canal along the side will really help as well. **C/Laraway:** I think that once these arteries are completed on North School and Ardell to the west, traffic will most likely funnel out three different ways rather than straight through the subdivision. I understand that this is probably a short-term problem, but I think the end result is that the traffic will disperse three other directions. **C/Gealy:** I do think that affordable

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housing is important and that we provide a variety of housing type and product. We need to mix it and not have whole areas that are one kind of house. It's important to have diversity in our housing areas. I do think these corners tend to get busier and busier, and because of that they lend themselves more to commercial or multifamily units. They've done a nice job with the two access points, so it's not like a dead end. I do think it would be nice if there were some amenities and some of that green space there. I'd like for them to work on a pathway along that canal.

*Commissioner Hennis motions to recommend approval to the City Council of Case Nos. 18-07-S with the conditions as outlined in the staff report; with an additional condition that the applicant work with the City to arrive at an amicable solution for the pathway that the City has in its Comprehensive Plan; and an additional condition to work with the City to provide at least an amenity for a gathering place for children like a tot lot or gazebo; Commissioner Gealy seconds, all aye and motion carried 3-0.*

**3. COMMISSION REPORTS**

**C/Young:** We have the Planning and Zoning Elections for Chairman and Vice Chairman for the year of 2019.

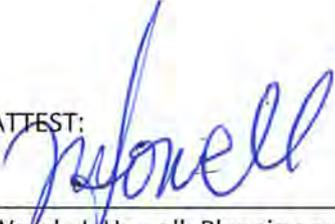
*Commissioner Gealy nominates Chairman Young and Commissioner Hennis to serve as Chairman and Vice Chairman of the Planning and Zoning Commission for the year of 2019. Commissioner Laraway seconds, all aye and motion carried 3-0.*

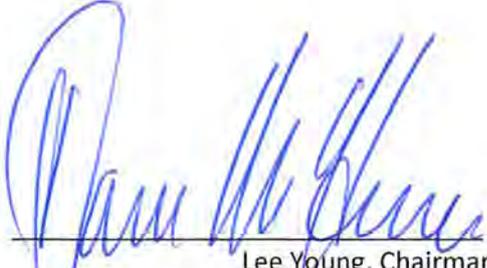
**Wendy Howell:** I hope you all are looking at the new Comprehensive Plan. **C/Laraway:** Do you know when our changes that we talked about last meeting will go before the City Council? **Wendy Howell:** They will go February 5<sup>th</sup>.

**4. ADJOURNMENT**

*Commissioner Gealy motions to adjourn; Commissioner Hennis Seconds, all aye and motion carried 3-0.*

ATTEST:

  
Wendy I. Howell, Planning and Zoning Director  
Kuna Planning and Zoning Department

  
Lee Young, Chairman  
Kuna Planning and Zoning Commission



**City of Kuna  
Planning & Zoning  
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### Preliminary Plat Checklist

Preliminary Plats require public hearings with both the Planning & Zoning Commission and City Council. Public hearing signs will be required to be posted by the applicant for both meetings. Sign posting regulations are available online.

**Project name:** Ledgestone Subdivision      **Applicant:** Jane Suggs / WHPacific

**All applications are required to contain one copy of the following:**

Applicant (✓)	Description	Staff (✓)
✓	Completed and signed Commission & Council Review Application.	X
✓	Vicinity map showing relationship of the proposed plat to the surrounding area with a 2-mile radius.	X
✓	Homeowner's maintenance agreement for the care of landscaped common areas.	
✓	Legal description of the preliminary plat area: Include a metes & bounds description to the section line of all adjacent roadways stamped & signed by a registered professional land surveyor with a calculated closure sheet & a map showing the boundaries of the legal description.	X
✓	Proof of ownership—A copy of your deed <b>and</b> Affidavit of Legal Interest (for all interested parties involved).	X
✓	Letter of Intent indicating reasons and details for preliminary plat.	X
✓	Commitment of Property Posting form signed by the applicant/agent.	X
TIS underway N/A	If preliminary plat includes 100 lots or more, please submit a traffic impact study. If preliminary plat includes 50 lots or more, please submit an estimate of tax revenue generation and an estimate of the public service costs to provide adequate service to the development.	
✓	A letter from Ada County Engineer with the Subdivision Name reservation. <b>ANY</b> name change(s) needs to be submitted and approved by the Planning & Zoning Director and Ada County Engineer.	X
✓	Phasing Plan see plat	
N/A	Include Large Scale Development Requirements. KCC 6-5-4	—
✓	Landscape Plan— (in color)	X
✓	Neighborhood meeting certification (certification & neighborhood meeting list forms shall accompany this application).	X
✓	8 1/2 x 11 proposed preliminary plat.	X
✓	Preliminary plat drawing on 24x36 quality paper drawn to scale of 1 to 100' or more. The following information shall be contained on the preliminary plat: ◇ Topography at two foot (2') intervals ◇ Land uses (location, layout, types & dimensions): residential, commercial & industrial land uses. ◇ Street right-of-ways: dimensions of right-of-way dedication for all roadways, street sections, improvements, etc. ◇ Easements/common space: utility easements, parks, community spaces ◇ Lots: layout and dimensions of lots ◇ Preliminary improvement drawing: show water, sewer, drainage, electricity, irrigation, telephone, natural gas, proposed street lighting, proposed street names, proposed subdivision name, fire hydrant placement, storm water disposal, underground utilities, and sidewalks..	X

*Note: Only one copy of the above items need to be submitted when applying for multiple applications. This application shall not be considered complete (nor will a Public Hearing be set) until Staff has received all required information. Once the application is deemed complete, Staff will notify the applicant of the scheduled hearing date, fees due, additional copies needed, etc.*





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Planning & Zoning  
Department  
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208.922.5274  
Fax: 208.922.5989  
Web: Kunacity.id.gov

### Annexation Checklist

Annexation requires public hearings with both the Planning & Zoning Commission and City Council. Public hearing signs will be required to be posted by the applicant for both meetings. Sign posting regulations are available online.

<b>Project name:</b> Ledgestone Subdivision	<b>Applicant:</b> Jane Suggs / WHPacific
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All applications are required to contain one copy of the following:

Applicant (✓)	Description	Staff (✓)
✓	Completed and signed Commission & Council Review Application.	X
✓	Letter of Intent indicating reasons for proposed annexation and the availability of public services.	X
✓	Vicinity map drawn to scale, showing the location of the subject property. Map shall contain the following information: Shaded area showing the annexation property, Street names and names of surrounding subdivisions.	X
✓	Legal description of the annexation area: Include a metes & bounds description to the section line of all adjacent roadways stamped & signed by a registered professional land surveyor with a calculated closure sheet & a map showing the boundaries of the legal description.	X
✓	Recorded warranty deed for the property.	X
✓	Proof of ownership—A copy of your deed <b>and</b> Affidavit of Legal Interest (All parties involved)	X
N/A	Development Agreement & Development Agreement Checklist	—
✓	Neighborhood meeting certification (certification & neighborhood meeting list forms shall accompany this application).	X
✓	Commitment of Property Posting form signed by the applicant/agent.	X

*Note: Only one copy of the above items need to be submitted when applying for multiple applications.*

*This application shall not be considered complete (nor will a Public Hearing be set) until staff has received all required information. Once the application is deemed complete, staff will notify the applicant of the scheduled hearing date, fees due, additional copies needed, etc.*



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### Commission & Council Review Application

Note: Engineering fees shall be paid by the applicant if required.

\*Please submit the appropriate checklist (s) with application

For Office Use Only	
File Number (s)	18-08-S 18-06-AN 18-33-DR
Project name	ledge stone subdivision
Date Received	10/4/18
Date Accepted/ Complete	
Cross Reference Files	
Commission Hearing Date	
City Council Hearing Date	

#### Type of Review (check all that apply):

- Annexation
- Appeal
- Comprehensive Plan Amendment
- Design Review
- Development Agreement
- Final Planned Unit Development
- Final Plat
- Lot Line Adjustment
- Lot Split
- Planned Unit Development
- Preliminary Plat
- Rezone
- Special Use
- Temporary Business
- Vacation
- Variance

#### Contact/Applicant Information

Owners of Record: <u>TJ Johnson</u>	Phone Number: _____
Address: <u>2425 N. Locust Grove Road</u>	E-Mail: _____
City, State, Zip: <u>Kuna, ID 83634</u>	Fax #: _____
Applicant (Developer): <u>Trilogy Development, Inc</u>	Phone Number: <u>208-895-8858</u>
Address: <u>9839 Cable Car Street, Suite 101</u>	E-Mail: _____
City, State, Zip: <u>Boise, ID 83709</u>	Fax #: _____
Engineer/Representative: <u>Jane Suggs / WHPacific</u>	Phone Number: <u>208-275-8729</u>
Address: <u>2141 W. Airport Way, Suite 104</u>	E-Mail: <u>jsuggs@whpacific.com</u>
City, State, Zip: <u>Boise, ID 83705</u>	Fax #: _____

#### Subject Property Information

Site Address: <u>Hubbard Road and Stroebel Road (new), to Locust Grove Road and Mason Creek</u>
Site Location (Cross Streets): <u>E. Hubbard Road, N. Locust Grove Road</u>
Parcel Number (s): <u>S1418121126, S1418123400</u>
Section, Township, Range: <u>Section 18, 2N, 1E</u>
Property size : <u>60.85 acres</u>
Current land use: <u>farming</u> Proposed land use: <u>single family subdivision</u>
Current zoning district: <u>RR</u> Proposed zoning district: <u>R-8</u>



**Project Description**

Project / subdivision name: Ledgestone Subdivision

General description of proposed project / request: single family homes, some with alleys for rear loaded garages, parks, pathways and open spaces, over 2000 feet of pathway along Mason Creek

Type of use proposed (check all that apply):

Residential \_\_\_\_\_

Commercial \_\_\_\_\_

Office \_\_\_\_\_

Industrial \_\_\_\_\_

Other \_\_\_\_\_

Amenities provided with this development (if applicable): park with tot lot, pathways throughout the subdivision, 2000+ feet of pathway along Mason Creek, alley loaded homes

**Residential Project Summary (if applicable)**

Are there existing buildings?  Yes  No

Please describe the existing buildings: \_\_\_\_\_

Any existing buildings to remain?  Yes  No

Number of residential units: 253 Number of building lots: 253

Number of common and/or other lots: 45

Type of dwellings proposed:

Single-Family \_\_\_\_\_

Townhouses \_\_\_\_\_

Duplexes \_\_\_\_\_

Multi-Family \_\_\_\_\_

Other \_\_\_\_\_

Minimum Square footage of structure (s): \_\_\_\_\_

Gross density (DU/acre-total property): 4.16 Net density (DU/acre-excluding roads): 5.59

Percentage of open space provided: 14% Acreage of open space: 18.51 acres

Type of open space provided (i.e. landscaping, public, common, etc.): parks, Mason Creek path, pathways

**Non-Residential Project Summary (if applicable)**

Number of building lots: \_\_\_\_\_ Other lots: \_\_\_\_\_

Gross floor area square footage: \_\_\_\_\_ Existing (if applicable): \_\_\_\_\_

Hours of operation (days & hours): \_\_\_\_\_ Building height: \_\_\_\_\_

Total number of employees: \_\_\_\_\_ Max. number of employees at one time: \_\_\_\_\_

Number and ages of students/children: \_\_\_\_\_ Seating capacity: \_\_\_\_\_

Fencing type, size & location (proposed or existing to remain): \_\_\_\_\_

Proposed Parking:

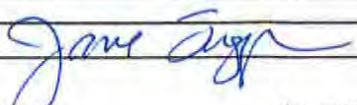
a. Handicapped spaces: \_\_\_\_\_ Dimensions: \_\_\_\_\_

b. Total Parking spaces: \_\_\_\_\_ Dimensions: \_\_\_\_\_

c. Width of driveway aisle: \_\_\_\_\_

Proposed Lighting: \_\_\_\_\_

Proposed Landscaping (berms, buffers, entrances, parking areas, common areas, etc.): \_\_\_\_\_

Applicant's Signature:  Date: 10/1/18



October 1, 2018

Ms. Wendy Howell, Director  
Kuna Planning and Zoning Department  
751 W. 4<sup>th</sup> Street  
Kuna, Idaho 83634

Subject: LedgeStone Subdivision – Hubbard Road, between Meridian and Locust Grove Roads  
Applications for annexation with zoning and preliminary plat

Dear Ms. Howell:

On behalf my client, Trilogy Development, Inc., please accept the attached applications to annex the subject property into the City of Kuna and to subdivide the property into 253 residential lots and 44 4 common lots. The property is located south of Hubbard Road and Mason Creek, between Meridian Road/Hwy 69 and Locust Grove Road. The subdivision property totals 60.85 acres and is currently located in Ada County with a zoning designation of RR. We are requesting annexation with a zoning designation of R-8. This residential zone meets the Kuna Comprehensive Planning designation for this area as *Medium Density Residential*. According to the Kuna Comp Plan: *this designation describes areas where residential development densities generally range from four to seven units per acre. These areas will be made up of single-family homes, but may include townhomes, row houses, duplexes and other types of multi-family land uses.* It is important to note that this zoning designation affords the flexibility and creativity to provide a mix of lot sizes and home projects, with a density that is still at the low end of the Comprehensive Plan, 4.16 du/acre.

The property is currently used as farm land. To the north of the property is Hubbard Road and the Patagonia Subdivision, which the Comprehensive Plan designates as Mixed Use. The agricultural land to the east is shown on Kuna’s Future Land Use Map as Professional Office. The property to the south is expected to be developed with additional Medium Density Residential uses and the land to the east, across Locus Grove Road, is shown as Low Density Residential in the Comprehensive Plan. The LedgeStone Subdivision will be complementary to all of the surrounding existing and proposed land uses.

**Preliminary Plat**

Two hundred lots in LedgeStone Subdivision have been designed to be larger than the zoning regulations and dimensional standards for the R-8 zone in the Kuna City Code. These lots are all significantly larger than the minimum of the requested zone, with sizes ranging from 5476 sf to 10,681 sf, and an average size of 6822 sf. These 200 standard lots have front yard setbacks of 20’, rear yard setbacks at 15’, interior side yards are 5’ and street side yards are 20’, which are typical for all Kuna residential zones.

We are excited to introduce a new alley loaded home design to Kuna. These single family detached homes will face the street; however, the garages will be accessed via an ACHD maintained, public alley. This design provide a charming streetscape and a home product that is popular with young professionals and active seniors, because there is little-to-no yard to maintain. These lots are typically 40’ wide and





110' feet deep for a total lot area of 4400 sf. To provide the appropriate home design on these lots we are requesting a Director's exception to allow a 15' setback to the living space, a 20' setback to the garage and a lot coverage of 52%. The Kuna code allows for the Director to modify some dimensional requirements with the Planning and Zoning Commission's concurrence, provided that there are not building or fire code issues. Our requested modifications do not impact building or fire code requirements.

Although the R-8 zone would allow up to eight dwelling units per acre, Ledgestone Subdivision will be built out at just half of that density or 4.15 du/acre. The local streets and open spaces will take up a large portion of the property. In fact, 36.74 acres (60%) of the subdivision will be used for buildable lots and the remaining 24.11 acres (40%) of the 60.85 acre property are used for street right-of-way and open space/buffers.

A Record of Survey has been submitted to the Ada County Surveyor to adjust the southern lot line as shown on the Preliminary Plat. The ROS will be approved and recorded prior to the approval of the Ledgestone Subdivision applications. If fact, we expect the ROS to be approved and recorded prior to the public hearings for the Ledgestone project.

**Buffers and Open Space**

The development of Ledgestone Subdivision will include the construction of over 2000 linear feet of public pathway along the south side of Mason Creek. This path is a part of the regional pathway system that is included in Kuna's Recreation and Pathways Master Plan Map. The multi-use pathway will be constructed by the developer and dedicated to the City upon completion.

A centrally located park will include a tot lot, gazebo and a popular half –basketball court, along with open play areas. Five additional open spaces lots will be landscaped and will provide passive play areas and neighborhood meeting spots. Pathways have been strategically located to provide good pedestrian connectivity to the park and to the Mason Creek pathway.

The landscape plan also shows additional open space and landscaping at the end caps of most blocks, adding shade and providing an attractive streetscape

The Mason Creek pathway area, plus parks, pathways and landscaped open areas add up to 6.2 acres, or over 10% pf the project area. If we add in the landscaped buffers and end caps, the total amount of landscaped common area is over 8.5 acres or 14% of the site. The buffers include a 25' landscape buffer along Hubbard Road, Locust Grove Road and Stroebel Road. Detached 8' sidewalks are located in these buffer areas.

**Streets/ and Utilities**

The main entry into Ledgestone will be taken from the new mid-mile collector, Stroebel Road. In addition, Lot 14, Block 1 will constructed to serve as a temporary access to the subdivision from Hubbard Road. Once the connection to Locust Grove Road is completed, Lot 14 will become a building lot. All local streets will be constructed to Kuna's standard of a 36' b/c-b/c street cross section in a 50' wide right-of-way. The public alleys will be constructed to ACHD standards.



We have worked with ACHD to provide one stub street to the northeast that will eventually cross Mason Creek, should the adjacent property be developed. There is one stub street to south, Moonshadow Avenue. We have designed Rio Villegas Street at Locust Grove Road to run along the south property boundary. This will allow a flexibility in designing a new street connection when the property to the south develops.

A Traffic Impact Study has recently been completed that reflects the newly installed traffic signal at Hubbard Road and Hwy 69/Meridian Road. The TIS will be reviewed by ACHD and ITD.

An 18" sewer main will be extended to and through the site from Hubbard Road to Locust Grove Road, as will a 12" water main. Pressurized irrigation will be provide to each lot from the Patagonia irrigation pond.

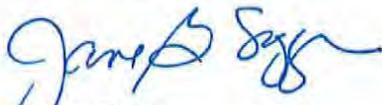
The existing FEMA flood hazard area for Mason Creek is shown on the plat. The project engineer is currently completing a model of the regulated 100-year flood for Mason Creek adjacent to the property. This is the same type of flood study that was completed for the Patagonia Subdivision downstream of Ledgestone. That previous study showed that the 100-year flood plain is contained entirely within the banks of Mason Creek. We expect that the study for this upstream area of Mason Creek in the Ledgestone Subdivision will have similar results. Upon approval of the study, the project engineer will file a Letter of Map Revision (LOMR) to remove the affected properties from the flood hazard designation.

**Neighborhood Meeting**

A neighborhood meeting was held on the site on Monday, July 9, 2018, at 6 pm. The neighborhood meeting notice and sign-up sheet are included in our application package. Due to the development of Patagonia Subdivision to the north, most neighbors were aware that this property would be developed in the not-too-distant future. There were questions concerned the schedule to begin construction and the time table for buildout.

We are pleased to submit the applications and support materials to annex Ledgestone Subdivision into Kuna and to provide an attractive mix of lot sizes and homes, especially the alley loaded single family home product. We look forward to working with you and your staff on the approval process. And, as always, do not hesitate to contact me if you have questions about the project or the application.

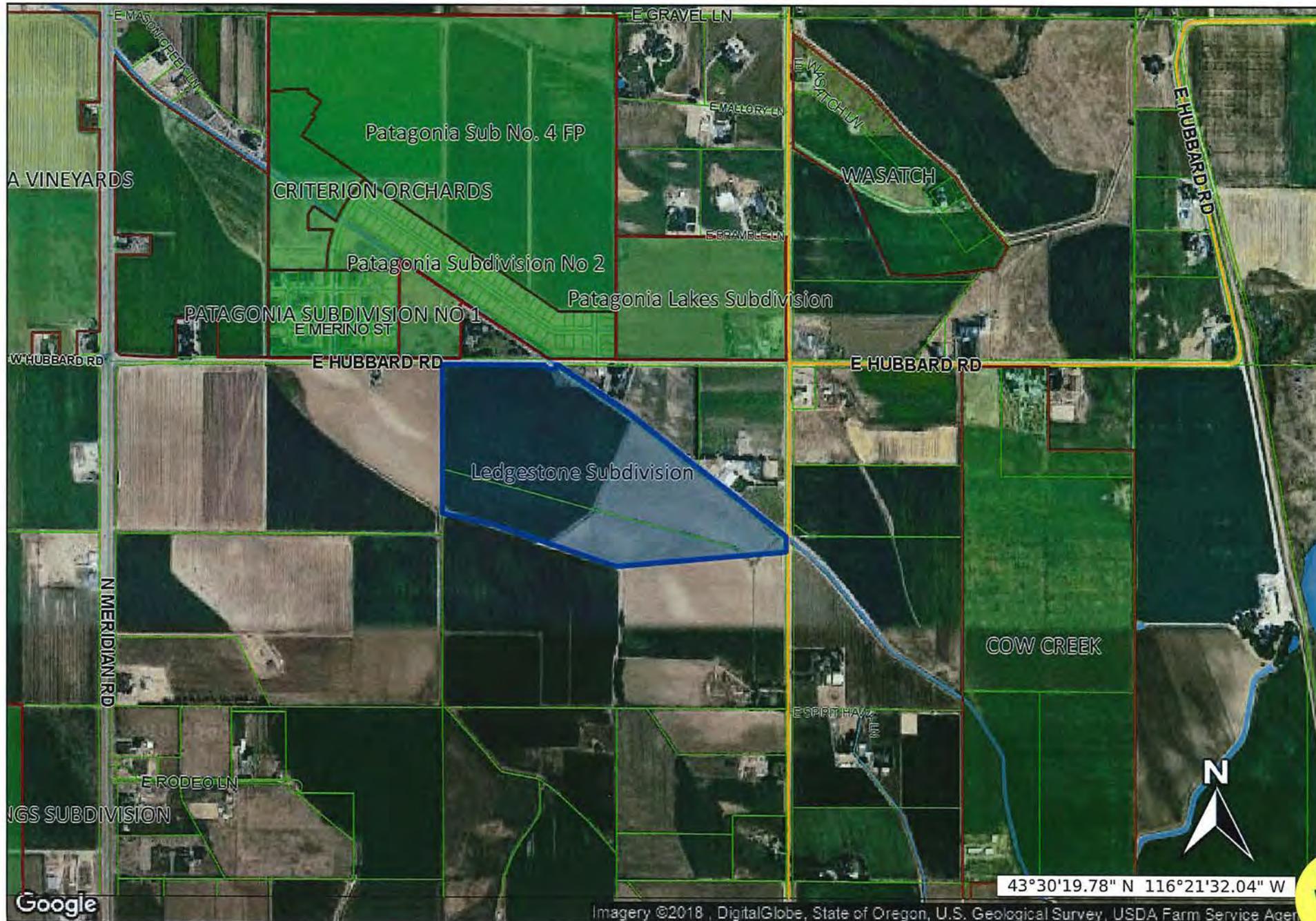
Sincerely,

  
Jane Suggs

cc: Shawn Brownlee



# Ledgestone Subdivision



Google

Imagery ©2018, DigitalGlobe, State of Oregon, U.S. Geological Survey, USDA Farm Service Agency

Exhibit  
A2c

**DESCRIPTION FOR  
LEDGESTONE SUBDIVISION**

A parcel of land located in the NE 1/4 of Section 18, Township 2 North, Range 1 East, Boise Meridian, Ada County, Idaho being more particularly described as follows:

**BEGINNING** at the N1/4 corner of said Section 18 from which the NE corner of said Section 18 bears South 89°25'47" East, 2651.44 feet;

thence along the North boundary line of said Section 18 South 89°25'47" East, 550.00 feet;

thence leaving said North boundary line South 00°34'13" West, 25.00 feet to a point on the South right-of-way line of E. Hubbard Road;

thence along the South right-of-way line of E. Hubbard Road the following 5 courses and distances:

thence South 89°25'47" East, 227.00 feet;

thence South 42°31'14" East, 42.45 feet;

thence South 89°25'47" East, 62.00 feet;

thence North 00°34'13" East, 31.00 feet;

thence South 89°25'47" East, 15.51 feet to a point on the centerline of the Mason Creek Feeder;

thence along the centerline of the Mason Creek Feeder the following 2 courses and distances:

thence South 55°34'36" East, 631.36 feet;

thence South 51°36'36" East, 1,580.10 feet to a point on the East boundary line of said Section 18;

thence along said East boundary line South 00°22'30" West, 215.64 feet;

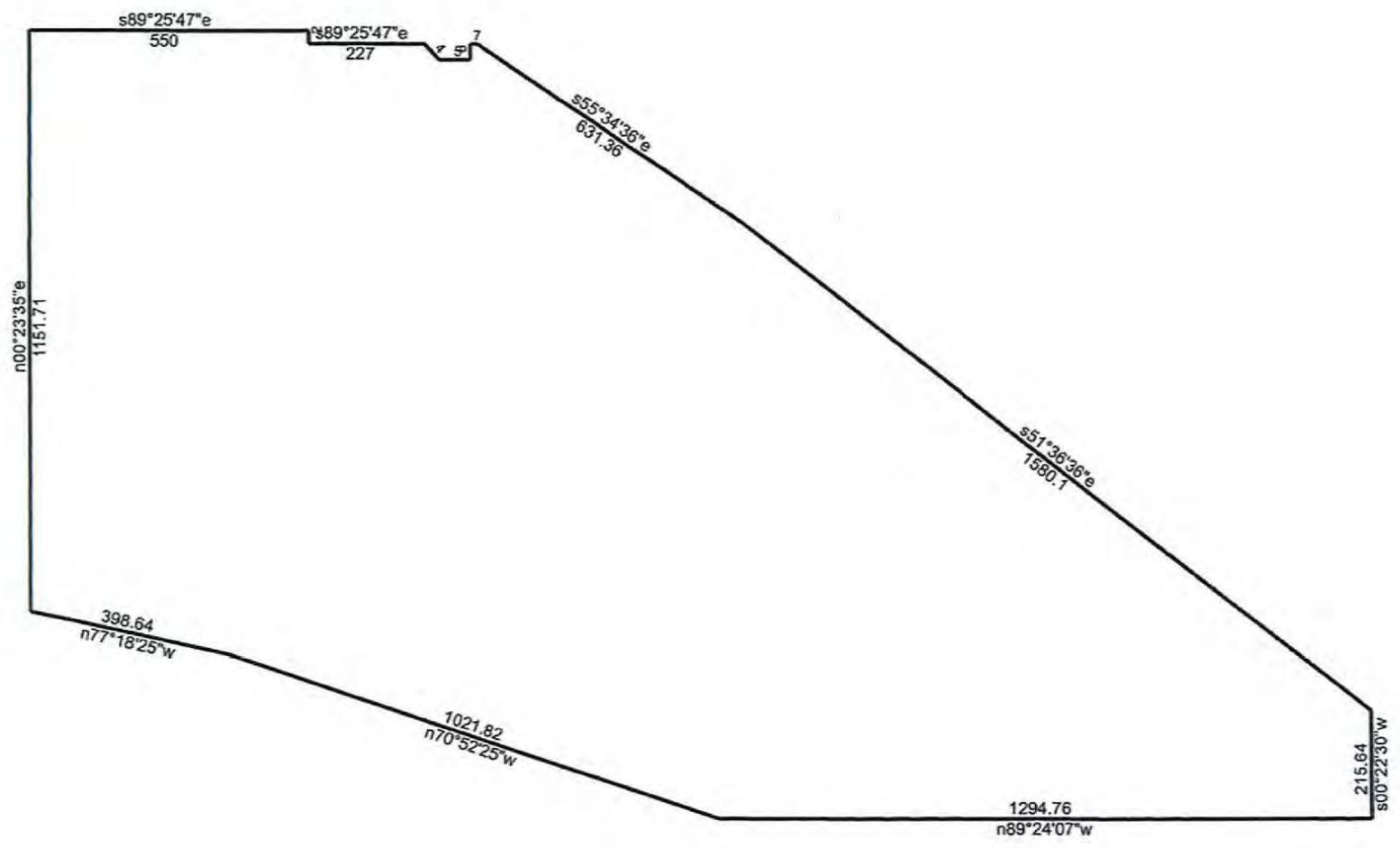
thence leaving said East boundary line North 89°24'07" West, 1,294.76 feet;

thence North 70°52'25" West, 1,021.82 feet;

thence North 77°18'25" West, 398.64 feet to a point on the North-South centerline of said Section 18;

thence along said North-South centerline North 00°23'35" East, 1,151.71 feet to the **REAL POINT OF BEGINNING**. Containing 60.85 acres, more or less.





**LEDGESTONE SUBDIVISION CLOSURE SHEET** 10/3/2018

Scale: 1 inch= 340 feet      File:

Tract 1: 60.8477 Acres, Closure: n65.1540e 0.01 ft. (1/642788), Perimeter=7247 ft.

- |                     |                      |
|---------------------|----------------------|
| 01 s89.2547e 550    | 11 n89.2407w 1294.76 |
| 02 s00.3413w 25     | 12 n70.5225w 1021.82 |
| 03 s89.2547e 227    | 13 n77.1825w 398.64  |
| 04 s42.3114e 42.45  | 14 n00.2335e 1151.71 |
| 05 s89.2547e 62     |                      |
| 06 n00.3413e 31     |                      |
| 07 s89.2547e 15.51  |                      |
| 08 s55.3436e 631.36 |                      |
| 09 s51.3636e 1580.1 |                      |
| 10 s00.2230w 215.64 |                      |



City of Kuna  
P.O. Box 13  
Kuna, Idaho 83634

Phone: (208) 922-5274  
Fax: (208) 922-5989  
Web: www.kunacity.id.gov



# City of Kuna AFFIDAVIT OF LEGAL INTEREST

State of Idaho )  
) ss  
County of Ada )

I, TJ Johnson / G Elaine Johnson , 2425 N. Locust Grove Road  
Name Address  
Kuna, ID 83634  
City State Zip Code

being first duly sworn upon oath, depose and say:

(If Applicant is also Owner of Record, skip to B)

A. That I am the record owner of the property described on the attached, and I grant my

Permission to Jane Suggs / WHPacific 2141 Airport Way, Suite 104, Boise, ID 83705 Name Address  
to submit the accompanying application pertaining to that property.

B. I agree to indemnify, defend and hold City of Kuna and its employees harmless from any claim or liability resulting from any dispute as to the statements contained herein or as to the ownership of the property which is the subject of the application.

C. I hereby grant permission to the City of Kuna staff to enter the subject property for the purpose of site inspections related to processing said application(s).

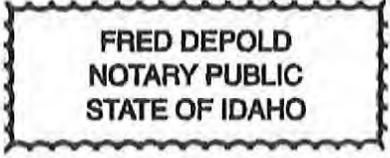
Dated this 1st day of October, 2018

TJ Johnson / G Elaine Johnson  
Signature

Subscribed and sworn to before me the day and year first above written.  
Fred Depold  
Notary Public for Idaho

Residing at: Meridian Idaho

My commission expires: 12-18-2023



1184000473

EXHIBIT A

TRACT I:

An irregular tract in the Northeast one-quarter of Section 18, T. 2 N., R. 1 E., Boise Meridian, Ada County, Idaho, and lying Southerly of the Crest Ditch and Northerly of a drain ditch described as follows:

From a point on the East boundary of Section 18, T. 2 N., R. 1 E., Boise Meridian, situated 1208.46 feet North 0°02' West of the quarter section corner between Sections 17 and 18; thence South 85°01' West along the center line of the Crest Ditch 351.6 feet to the real place of beginning; thence South 85°01' West along the center line of the drain ditch, 980.3 feet to a point; thence North 71°14' West along the center line of a drain ditch, 990.00 feet to a point; thence North 77°40' West along the center line of a drain ditch, 398.64 feet to a point on the North and South center line of Section 18; thence North 0°02' East along the North and South center line of Section 18, 337.7 feet to a point in the center of the Crest ditch; thence South 75°01' East along the center line of the Crest Ditch 2324.8 feet to a point; thence South 41°09' East along the center line of the Crest Ditch 78.95 feet to the real place of beginning.

TRACT II:

Part of the Northeast one-quarter of Section 18, T. 2 N., R. 1 E., Boise Meridian, Ada County, Idaho.

Beginning at the East quarter corner of said Section; thence West 2654.5 feet to center of said Section; thence North 0°2' East 1496.2 feet; thence South 77°40' East, 398.64 feet; thence South 71°14' East, 990 feet; thence North 85°1' East 1331.9 feet; thence South 0°2' East 1208.46 feet to a point of beginning.

EXCEPT roads and ditches and rights of way.

1184000474

TRACT III:

An irregular tract in the Northeast one-quarter of Section 18, T. 2 N., R. 1 E., Boise Meridian, Ada County, Idaho described as follows:

Beginning at the Quarter Section corner between Sections 7 and 18, T. 2 N., R. 1 E., Boise Meridian; thence North 89°59' East along the Section line 846.6 feet to a point in the center of the Mason Creek Ditch; thence South 56°06' East along the center line of the Mason Creek Ditch 673.11 feet to a point; thence South 52°08' East continuing along the center line of the Mason Creek Ditch 1580.1 feet to a point on the East boundary of Section 18; thence South 0°02' East 93.86 feet to a point in the Center of the Crest Ditch; thence South 85°01' West along the center line of the Crest Ditch 351.6 feet to a point; thence North 41°09' West continuing along the center line of the Crest Ditch 78.95 feet to a point; thence North 75°01' West continuing along the center line of the Crest Ditch 2324.8 feet to a point on the North and South center line of Section 18; thence North 0°02' East 814 feet to the place of beginning.

together with all and singular the tenements, hereditaments and appurtenances thereunto belonging or in anywise appertaining;

subject to all easements and rights of way of record or appearing on the land;

subject, also, to taxes and assessments levied and assessed for the year 1977, which are now liens but not yet due nor payable.

1184000475

*Michigan*  
STATE OF ~~NEW-YORK~~ )  
County of ~~Albany~~ ) : ss.

On the 21 day of May, 1977, before me, personally came ROBERT MONTGOMERY, one of the Trustees of the A. E. MONTGOMERY and HELEN B. MONTGOMERY TRUST, to me, known to be the person described in and who executed the foregoing instrument, and acknowledged that he executed the same.

*Wm. Thomas Long*  
NOTARY PUBLIC FOR NEW-YORK ~~MICHIGAN~~  
Residence: LELAND, MICHIGAN 49654  
Expiration Date - OCTOBER 18, 1977

*This document is being recorded as is at the request of T. J. Johnson. No seal on the Notary*

**ANNEXATION DESCRIPTION FOR  
LEDGSTONE SUBDIVISION**

A parcel of land located in the NE 1/4 of Section 18, Township 2 North, Range 1 East, Boise Meridian, Ada County, Idaho being more particularly described as follows:

**BEGINNING** at the N1/4 corner of said Section 18 from which the NE corner of said Section 18 bears South 89°25'47" East, 2651.44 feet;

thence along the North boundary line of said Section 18 South 89°25'47" East, 846.24 feet to a point on the centerline of the Mason Creek Feeder;

thence along the centerline of the Mason Creek Feeder the following 2 courses and distances:

thence leaving said North boundary line South 55°34'36" East, 676.24 feet;

thence South 51°36'36" East, 1,580.10 feet to a point on the East boundary line of said Section 18;

thence along said East boundary line South 00°22'30" West, 215.64 feet;

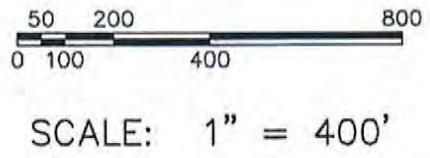
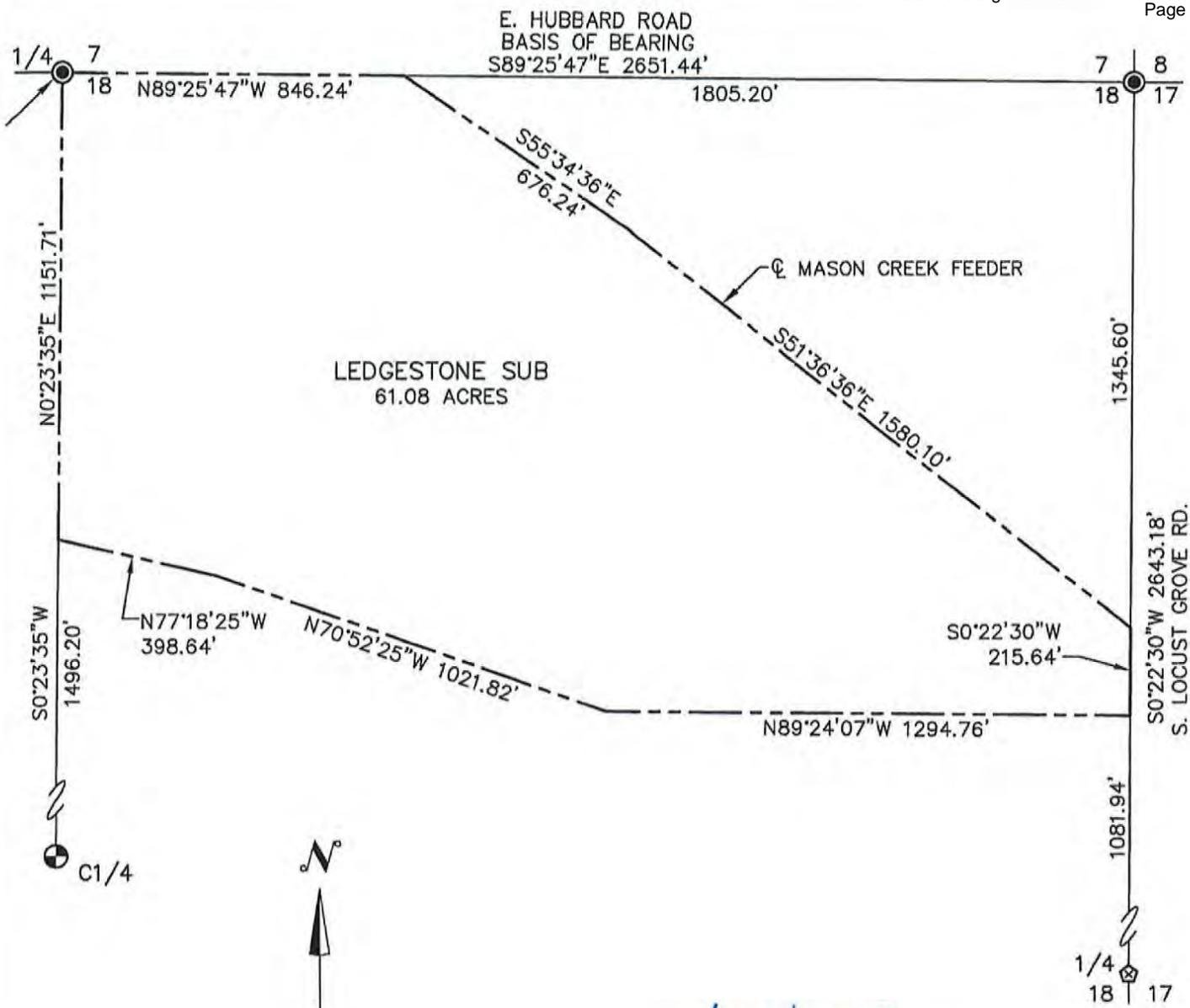
thence leaving said East boundary line North 89°24'07" West, 1,294.76 feet;

thence North 70°52'25" West, 1,021.82 feet;

thence North 77°18'25" West, 398.64 feet to a point on the North-South centerline of said Section 18;

thence along said North-South centerline North 00°23'35" East, 1,151.71 feet to the **REAL POINT OF BEGINNING**. Containing 61.08 acres, more or less.





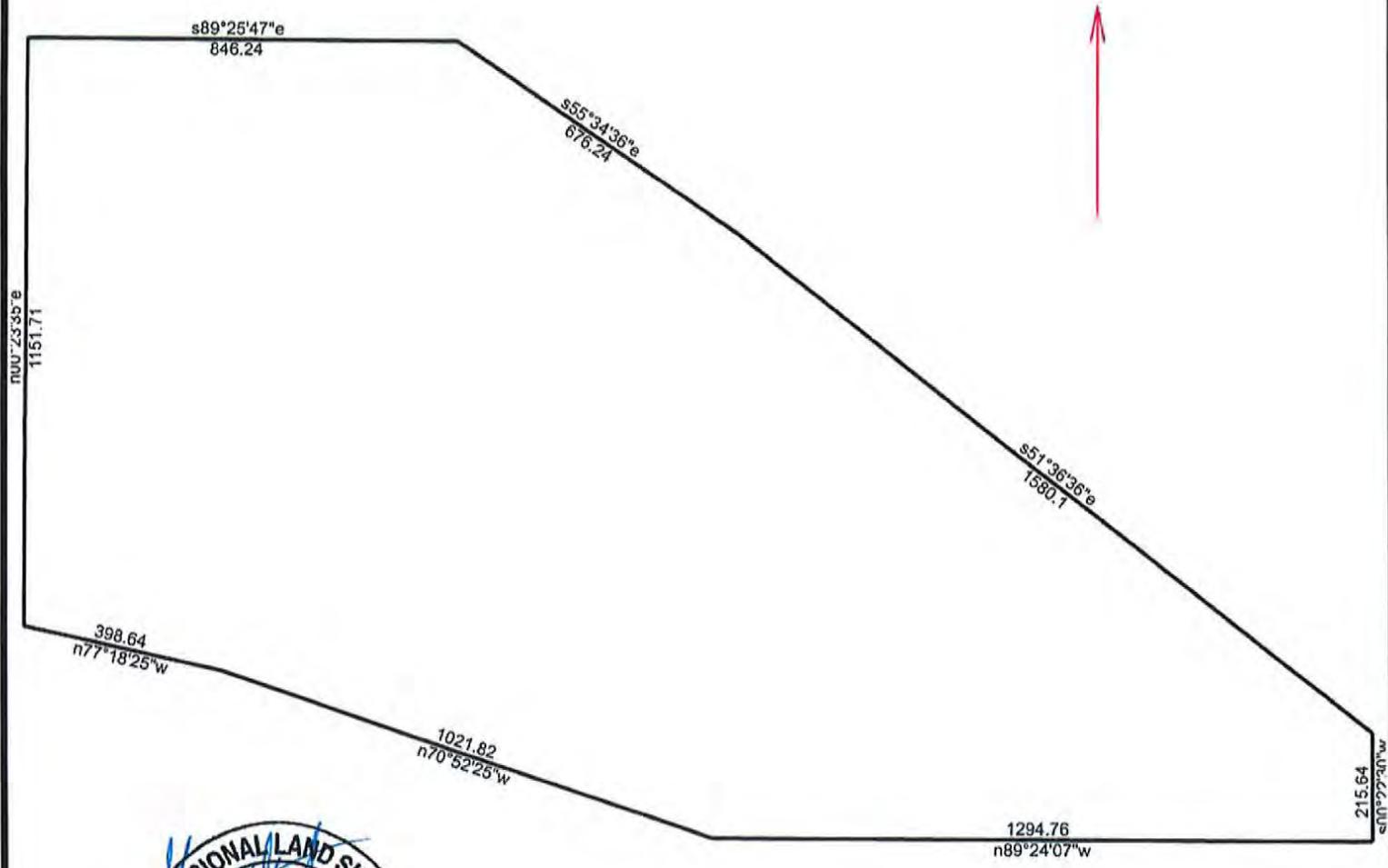
P:\Johnson\_58oc\_Body\_Topics\_18-148\dwg\Annexation\_exhibit.dwg 10/3/2018 9:30:37 AM

**IDAHO SURVEY GROUP, LLC**  
9955 W. EMERALD ST.  
BOISE, IDAHO 83704  
(208) 846-8570

ANNEXATION EXHIBIT DRAWING FOR  
LEDGESTONE SUBDIVISION

LOCATED IN THE NE 1/4 OF SECTION 18, T.2N., R.1E., B.M.,  
ADA COUNTY, IDAHO

JOB NO. 18-148
SHEET NO. 1
DWG. DATE 10/3/2018



Ledgestone Subdivision Annexation Closure Sheet 10/3/2018

Scale: 1 inch= 340 feet | File:

Tract 1: 61.0829 Acres, Closure: n56.4606e 0.01 ft. (1/681860), Perimeter=7185 ft.

- |                      |                      |
|----------------------|----------------------|
| 01 s89.2547e 846.24  | 08 n00.2335e 1151.71 |
| 02 s55.3436e 676.24  |                      |
| 03 s51.3636e 1580.1  |                      |
| 04 s00.2230w 215.64  |                      |
| 05 n89.2407w 1294.76 |                      |
| 06 n70.5225w 1021.82 |                      |
| 07 n77.1825w 398.64  |                      |



*City of Kuna*  
**COMMITMENT TO  
PROPERTY POSTING**

City of Kuna  
P.O. Box 13  
Kuna, Idaho 83634

Phone: (208) 922-5274  
Fax: (208) 922-5989  
Web: [www.kunacity.id.gov](http://www.kunacity.id.gov)

Per City Code 5-1A-8, the applicant for all applications requiring a public hearing shall post the subject property not less than ten (10) days prior to the hearing. The applicant shall post a copy of the public hearing notice or the application(s) on the property under consideration.

The applicant shall submit proof of property posting in the form of a notarized statement and a photograph of the posting to the City no later than seven (7) days prior to the public hearing attesting to where and when the sign(s) were posted. Unless such Certificate is received by the required date, the hearing will be continued.

The sign(s) shall be removed no later than three (3) days after the end of the public hearing for which the sign(s) had been posted.

I am aware of the above requirements and will comply with the posting requirements as stated in Kuna City Code 5-1A-8.

Jane Suss  
Applicant/agent signature:

August 29, 2018  
Date:



**TRAFFIC IMPACT STUDY  
FOR  
LEDGESTONE SUBDIVISION  
ADA COUNTY, ID**

**Prepared for:  
TRILOGY DEVELOPMENT, INC.  
9839 W. Cable Car Street, Ste. 101  
Boise, ID 83709**

**Prepared By:  
WHPacific  
2141 W. Airport Way, Ste. 104  
Boise, ID 83705  
(208) 342-5400**

**October 10, 2018**



October 10, 2018

## EXECUTIVE SUMMARY

This study was prepared in accordance with the ADA County Highway District's (ACHD's) requirements for a Traffic Impact Study listed in Section 7106 of the current *ACHD Policy Manual*. It evaluates the traffic impacts associated with the TJ Johnson property in Kuna, ID, also known as "Ledgestone Subdivision." The study area, scope and specific analysis parameters and requirements are the result of an Area of Influence Review performed by the Community Planning Association of Southwest Idaho (COMPASS) and a subsequent Initial Meeting with ACHD. The study's principal findings and recommendations are summarized below.

### Proposed Development

- 1.0 Ledgestone Subdivision is a proposed development consisting of 254 single-family dwelling units on a 60.85 acre parcel located south of Hubbard Road, between Meridian Road (SH 69) and Locust Grove Road located in Ada County, Idaho.
- 2.0 The development is planned to be constructed over a period of approximately seven years, or to the period 2025/2026. Do to the short duration of buildout, an interim evaluation was not required by ACHD.
- 3.0 The proposed development is expected to generate 2,398 daily trips, 188 AM peak hour trips and 251 PM peak hour trips.
- 4.0 The primary roadway network serving this proposed subdivision includes the following roadway segments and intersections:

#### Intersections:

- Hubbard Road and SH69 (Meridian Road)
- Hubbard Road and Locust Grove Road
- Columbia Road and Locust Grove Road
- All site access points

#### Segments:

- Hubbard Road, between SH69 and Locust Grove Road
- Locust Grove Road, between Hubbard Road and Columbia Road
- All internal and new collectors

Primary access to the site will be provided via S. Stroebel Road, constructed along the ½ mile alignment, between Meridian Road and Locust Grove Road. Additional access will be provided south of the Mason Creek Ditch at Locust Grove Road and E. Initially, a temporary access will be constructed to the subdivision approximately 500 feet east of S. Stroebel Road. This access will ultimately be closed at the completion of development.

### Proposed Mitigation for Existing Traffic

- 5.0 For the existing traffic conditions analyzed with the existing roadway lane configuration, all study area roadways meet ACHD's minimum operational thresholds. No roadway improvements are needed to mitigate existing traffic.
- 6.0 For the existing traffic conditions analyzed with the existing intersection control and lane configuration, all study area intersections meet ACHD's minimum operational thresholds. No intersection improvements are needed to mitigate the existing traffic.

October 10, 2018

**Proposed Mitigation for 2025 Background Traffic**

- 7.0 For the 2025 background traffic conditions analyzed with the existing roadway lane configuration, all study area roadways meet ACHD's minimum operational thresholds. No roadway improvements are needed to mitigate 2025 background traffic.
- 8.0 For the 2025 background traffic conditions analyzed with the existing intersection control and lane configuration, one of the three study area intersections do not meet ACHD's minimum operational thresholds. At the intersection of Columbia Road and Locust Grove Road, Installation of a traffic signal is recommended to mitigate 2025 background traffic conditions. A single-lane roundabout is deemed as another viable alternative to the recommended traffic signal, however the roundabout alternative was not fully analyzed under this review. While this improvement has been identified to accommodate 2025 background traffic, it is not currently included in ACHD's Capital Improvements Plan (CIP).

**Proposed Mitigation for 2025 Site Plus Background Traffic**

- 9.0 For the 2025 site plus background traffic conditions analyzed with the existing roadway lane configuration, all study area roadways meet ACHD's minimum operational thresholds. No roadway improvements are needed to mitigate 2025 site plus background traffic.
- 10.0 For the 2025 site plus background traffic conditions analyzed with the existing (and 2025 background improvements) intersection control and lane configuration, all study area intersections meet ACHD's minimum operation thresholds. Therefore, no intersection improvements are needed to mitigate 2025 site plus background traffic.

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## PROPOSED DEVELOPMENT

### Project Description

The TJ Johnson property near Kuna, ID is a proposed development consisting of 254 single-family dwelling units on a 60.85 acre parcel located south of Hubbard Road, between Meridian Road (SH 69) and Locust Grove Road. The development is formally known as the “Ledgestone Subdivision.” Primary access to the site will be provided via S. Stroebel Road, constructed along the ½ mile alignment, between Meridian Road and Locust Grove Road. Additional access will be provided south of the Mason Creek Ditch on Locust Grove Road. Initially, a temporary access will be constructed to the subdivision approximately 500 feet east of S. Stroebel Road. This access will ultimately be closed at the completion of development. The existing site is currently undeveloped farm land and is zoned Rural Residential (RR). The project proposes to rezone to Medium Density Residential (R8). The proposed site plan is illustrated in Figure 1.

Buildout of the Ledgestone Subdivision is expected to occur over an approximate seven year period, or approximately 2025/2026. Due to the short duration of buildout it was confirmed in the initial meeting with Ada County Highway District (ACHD) that an interim evaluation period would not be needed.

### STUDY APPROACH

This Traffic Impact Study is required by ACHD as part of the development approval process and follows the requirements for Traffic Impact Studies listed in Section 7106 of the current ACHD Policy Manual.

### Initial Meeting

Prior to the initial meeting, ACHD requested that Community Planning Association of Southwest Idaho (COMPASS) perform an area of influence model run. The proposed development falls within TAZ 1181. The current COMPASS model assumes 6 households (HH) and 15 jobs within this TAZ. Under the proposed development of 254 single family homes, the total HH equals 260. Using the 2025 forecast year, COMPASS ran the model with and without the proposed development to confirm likely trip impacts. The review concluded that the following intersections and roadway segments be include in the TIS evaluation:

Intersections:

- Hubbard Road and SH69 (Meridian Road)
- Hubbard Road and Locust Grove Road
- Columbia Road and Locust Grove Road
- All site access points

Segments:

- Hubbard Road, between SH69 and Locust Grove Road
- Locust Grove Road, between Hubbard Road and Columbia Road
- All internal and new collectors

This area of influence analysis was provide to ACHD and Idaho Transportation Department (ITD) and is included in the Appendix. Also, prior to the Initial Meeting ACHD approved of collecting traffic counts prior to the start of Kuna schools due to the remote location of the proposed development.



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The initial meeting with ACHD was held on August 7, 2018 and was attended by ACHD (Mindy Wallace and Aimee Loudenslager) and WHPacific (Jane Suggs and Bob Beckman). The developer, Trilogy, and ITD were not in attendance at this meeting but were consulted afterward regarding the items discussed. ITD has requested to be involved in review of the TIS due to the potential impacts related to SH69. Other items discussed included:

- ACHD indicated that Capital Projects staff were involved in development of an interim traffic signal at SH69 and Hubbard Road. Subsequent to the meeting, WHPacific contacted both ACHD and ITD regarding this recently installed signal which has been identified as an interim project at this location.
- Other development is planned in the vicinity between Hubbard Road and Columbia Road. According to ACHD, this entitled development is already included in the COMPASS demographic data.
- Due to the short development period, a multiple phase review is not needed. The TIS will only need to assess existing conditions and full buildout (expected in 2025/2026)
- A \$500 fee is included at the time of the TIS submittal. No DRAFT review is needed.
- For trip generation computations use *ITE Trip Generation Manual, 10th Edition*.
- ACHD encouraged WHPacific to ask questions if they came up during development of the traffic study.
- WHPacific should consult with ITD to determine if items above and beyond ACHD Policy will need to be reviewed for ITD purposes. Subsequent contact with ITD indicated that additional review was not needed for this project.

### **Study Area**

In accordance with the area of influence review performed by COMPASS the following intersections and roadway segments will be reviewed:

Intersections:

- Hubbard Road and SH69 (Meridian Road)
- Hubbard Road and Locust Grove Road
- Columbia Road and Locust Grove Road
- All site access points

Segments:

- Hubbard Road, between SH69 and Locust Grove Road
- Locust Grove Road, between Hubbard Road and Columbia Road
- All internal and new collectors

### **Study Period**

The study periods as identified in the Initial Meeting will include:

- Existing (2018)
- 2025/2026 (Buildout)

The following time intervals will be evaluated:

- Weekday AM Peak Hour
- Weekday PM Peak Hour

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As this development is comprised entirely of single-family homes, a weekend peak hour review was not deemed necessary.

## **ANALYSIS OF EXISTING (2018) CONDITIONS**

### **Roadway Network**

Table 1 summarizes the characteristics of the roadway network within the study area.

**Table 1 – Study Area Roadways**

Roadway	Functional Classification	Posted Speed (mph)	Lanes (total)
SH69	Principal Arterial	55	5 (includes TWLTL)
Columbia Road	Minor Arterial	50	2
Hubbard Road	Minor Arterial	45	2
Locust Grove Road	Minor Arterial	50	2

Functional Classification noted in accordance with 2040 Functional Classification Map, COMPASS

TWLTL = Two-Way-Left Turn Lane

Two of the three intersections within the study area are stop-controlled (unsignalized). A four-way stop exists at Columbia Road and Locust Grove Road and a two-way stop in the north and south directions is present at Hubbard Road and Locust Grove Road. At SH69 and Hubbard Road a newly installed traffic signal is present. Figure 2 illustrates existing lane configuration and traffic control conditions.

### **Transit Service**

Due to the rural nature of the study area no existing transit routes in the vicinity exist. The closest available transit routes are located along Overland Road to the north, with stops in the vicinity of Eagle Road and SH69.

### **Bicycle and Pedestrian Facilities**

No bicycle or pedestrian facilities exist within the study area.

### **Traffic Volumes**

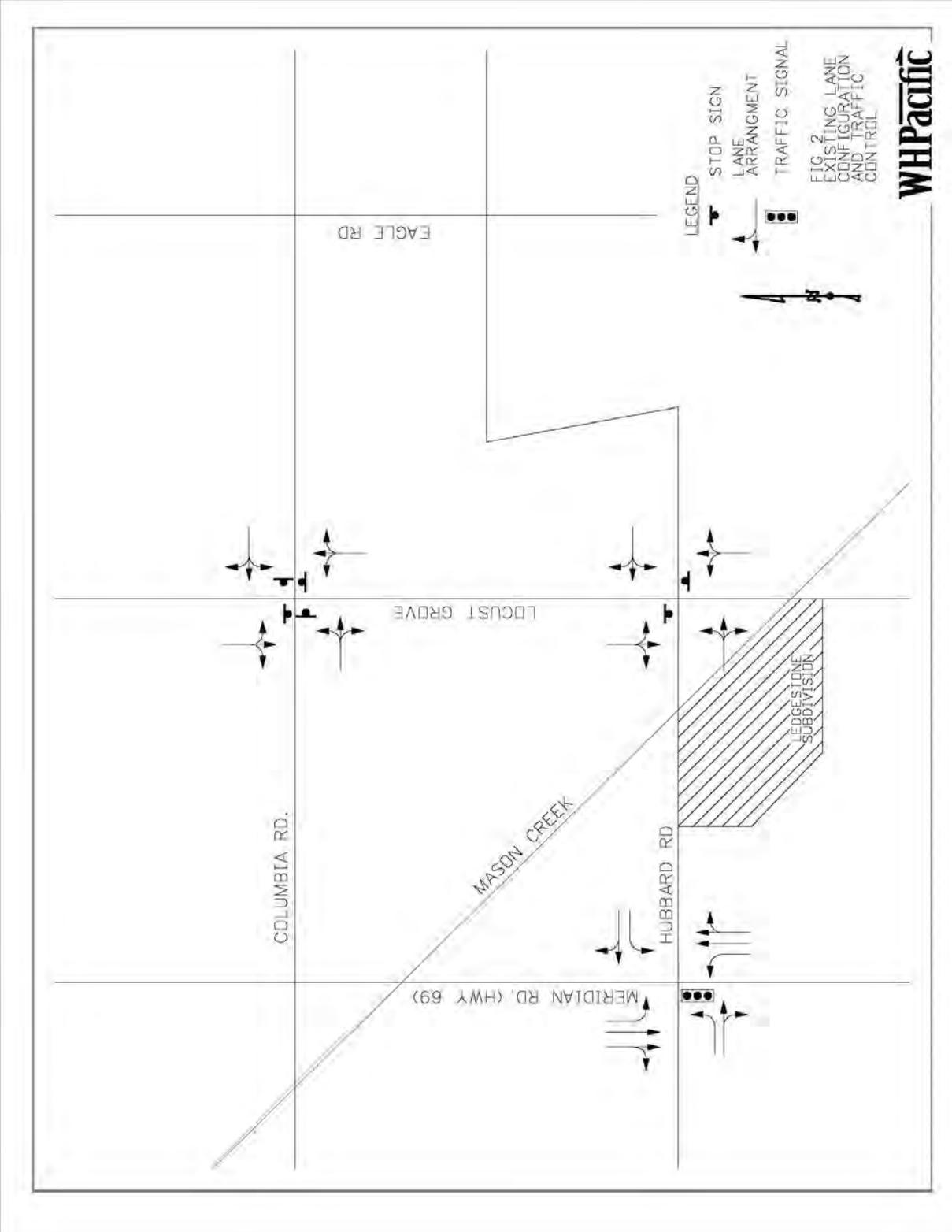
Existing 24-hour counts and intersection turn movement counts were collected on Tuesday, August 7, 2018. 24-hour counts were recorded 1) on Hubbard Road, between SH69 and Locust Grove Road and 2) on Locust Grove Road, between Hubbard Road and Columbia Road. Intersection turn movement counts were recorded between 7:00 AM – 9:00 AM and 4:00 PM to 6:00 PM in order to isolate the AM and PM peak hour conditions. Intersection count locations included 1) Hubbard Road and SH69, 2) Hubbard Road and Locust Grove Road and 3) Locust Grove Road and Columbia Road. Vehicle classification, pedestrian, and bicycle movements were not recorded for purposes of this review. Figure 3 illustrates resultant 24-hour and intersection turn movement counts. Relative count summaries are also included in the Appendix.

### **Level-of-Service Roadway Segments**

ACHD has developed level-of-service (LOS) thresholds for roadway segments based on directional peak hour volumes for various roadway functional classifications, number of lanes and left-turn treatments. Based on the current *ACHD Policy Manual*, the minimum acceptable LOS for a roadway segment is LOS E for principal arterials and minor arterials, and LOS D for collectors. Table 2 summarizes ACHD's LOS thresholds for roadway segments.

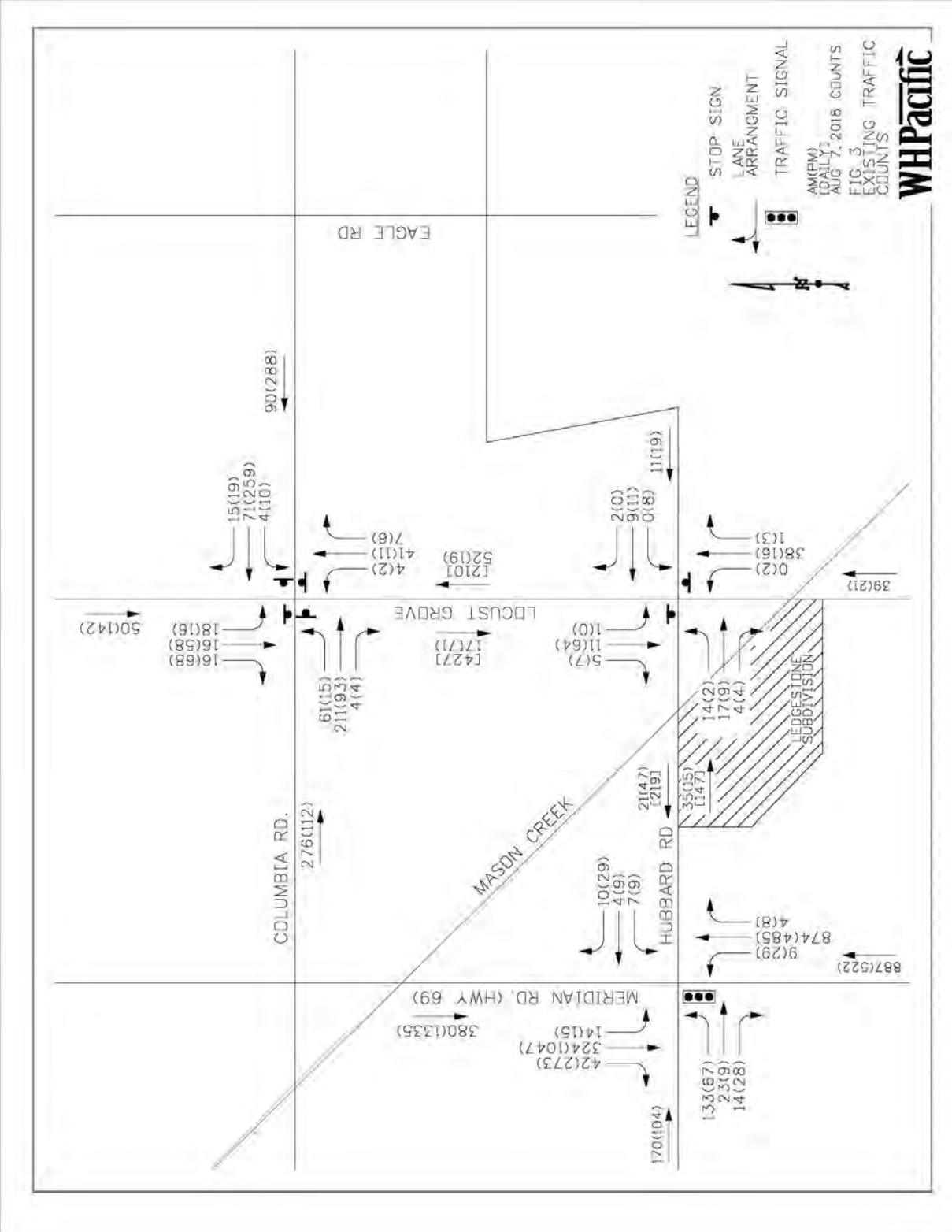
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Figure 2



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Figure 3



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**Table 2 – ACHD LOS Thresholds for Roadway Segments**

Functional Classification	Lanes		
		LOS D	LOS E
<b>Principal Arterials</b>			
No Left-Turn Lanes			
	1	600	690
Continuous Center Left-Turn Lane			
	1	770	880
	2	1680	1780
	3	2560	2720
Median-Control, Channelized Left-Turn Lanes @ Major Intersections			
	1	850	920
	2	1860	1960
	3	2800	3000
<b>Minor Arterials</b>			
No Left-Turn Lane			
	1	540	575
Unrestricted Median, Continuous Left-Turn Lane			
	1	675	720
	2	1395	1540
	3	2155	2370
Median-Control, Channelized Left-Turn Lanes @ Major Intersections			
	1	710	770
	2	1465	1670
	3	2270	2530
<b>Collectors</b>			
No Left-Turn Lanes			
	1	425	525
Unrestricted Median, Continuous Left-Turn Lane			
	1	530	660
	2	1080	1250

Table 3 summarizes the existing LOS for the roadway segments in the study area. As noted, all roadway segments currently operate at LOS D or better under the current lane configuration and traffic volumes. No roadway improvements are needed to mitigate existing traffic conditions.

**Table 3 – Roadway Segment LOS – Existing (2018) Traffic**

Roadway Segment	Functional Class	No. of Thru Lanes	Left-Turn Treatment	Threshold Volume		AM Peak Hour Major Direction		PM Peak Hour Major Direction	
				LOS D	LOS E	Vol (vph)	LOS	Vol (vph)	LOS
Hubbard Rd, SH69 to Locust Grove Rd	Minor Arterial	1	No LT Lane	550	575	35	< D	47	< D
Locust Grove Rd, Hubbard Rd to Columbia Rd	Minor Arterial	1	No LT Lane	540	575	52	< D	71	< D

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**Level-of-Service Intersections**

Intersection LOS was evaluated using *Highway Capacity Software (HCS7)*. In accordance with the *ACHD Policy Manual*, the maximum overall v/c ratio is 0.90 for signalized intersection. For unsignalized intersections, the intersection v/c ratio is undefined. The maximum lane group v/c ratio for signalized and unsignalized intersections is 1.0. Each of the intersections within the study area was evaluated under existing traffic control, lane configuration and peak hour volumes. *HCS7* Reports are included in the Appendix and results are summarized in Table 4.

**Table 4 – Intersection Traffic Operations – Existing (2018) Traffic**

Intersection	Traffic Control Lane Group	AM LOS/Delay/v/c	PM LOS/Delay/v/c
Hubbard Rd/ SH69	Traffic Signal	B/13.8	B/12.9
	Eastbound	D/46.9/0.82	D/48.3/0.78
	Westbound	D/48.4/0.38	D/48.2/0.47
	Northbound	A/9.2/0.41	A/7.9/0.54
	Southbound	A/8.0/0.43	B/10.9/0.61
Hubbard Rd/ Locust Grove	TWSC	NR	NR
	Eastbound	NR/3.0/0.01	NR/0.9/0.00
	Westbound	NR/0.0/0.00	NR/3.1/0.01
	Northbound	A/9.6/0.05	A/9.3/0.03
	Southbound	A/9.2/0.02	A/9.6/0.09
Columbia Rd/ Locust Grove	AWSC	A/9.3/NR	A/9.7/NR
	Eastbound	A/10.0/NR	A/8.7/NR
	Westbound	A/8.1/NR	B/10.5/NR
	Northbound	A/8.3/NR	A/8.2/NR
	Southbound	A/8.2/NR	A/9.0/NR

NR = not reported

TWSC = Two-way stop control

AWSC = All-way stop control

All study area intersections currently operate at acceptable, LOS D or better, conditions. Reported v/c ratios are also under 1.0. No intersection improvements are needed to mitigate existing traffic conditions.

**ANALYSIS OF 2025 BACKGROUND TRAFFIC CONDITIONS****Roadway Network**

Both the *ACHD Five-Year Work Plan (FYWP)* and the *ACHD Capital Improvements Plan (CIP)* were reviewed for purposes of the study. The currently adopted *FYWP* identifies projects programmed from 2018 to 2022 while the *CIP* is a long-range (20 years) transportation plan identifying existing transportation facilities, existing deficiencies, and future improvement needs. The only specific project noted in the *FYWP* is an interim traffic signal at the intersection of SH69 and Hubbard Road. This project has recently been completed and is therefore considered as part of the existing roadway and traffic control network. A long-term project is also planned at this intersection to modify this signal and reconstruct/widen approaches, by adding an exclusive right turn lane in the southbound and westbound directions. This work is planned during the period 2031 – 2035 and will therefore not be included in the assumed 2025 lane configuration, unless needed to achieve acceptable operations.

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### **Transit Service**

Valley Regional Transit (VRT) has recently adopted (April 2018) *ValleyConnect 2.0* which is a plan for long-range transit service and related capital projects. Scenarios considered in this plan include linking Kuna to Meridian via SH69. As of now the plan is dependent on securing various funding sources and actual projects or programmed improvements are not defined. As such, no further improvements beyond the existing transit network are assumed for the project study area.

### **Bicycle and Pedestrian Facilities**

ACHD's current *FYWP* and *CIP* do not have bicycle or pedestrian improvement projects designated for the study roadways.

### **Traffic Volumes**

The COMPASS travel demand model was used to estimate 2025 background traffic volumes. As no roadway network improvements or significant demographic changes are planned in this vicinity for the foreseeable future, the existing turn movement distribution is considered a reliable estimate for the distribution of future 2025 peak hour traffic. The 2025 peak hour forecast provided by COMPASS is considered a representation of PM peak hour conditions and a separate AM peak hour model has not been developed. In order to forecast future PM peak hour conditions the COMPASS directional link volumes were distributed in accordance with the associated existing turn movement percentages. Subsequent to this initial computation further manual balancing between nodes was required. This process is consistent with the *Furness Method* where existing intersection turning movement percentages and forecasted peak hour approach volumes are used to alternatively balance the entering and departing traffic until results converge, resulting in balanced forecast turn movement volumes at each intersection.

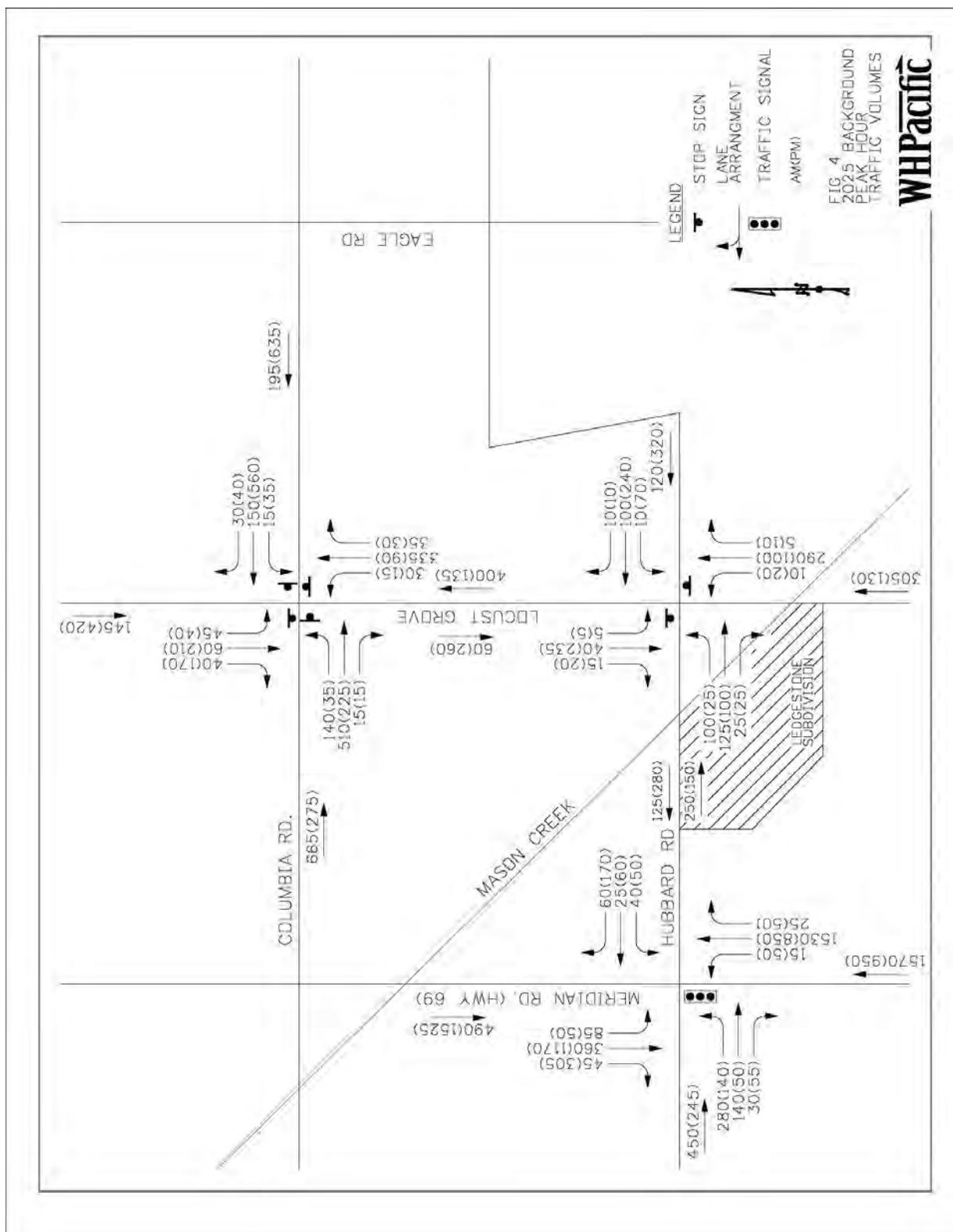
As an AM forecast is unavailable, further computation were required to generate 2025 background AM peak hour conditions. This was accomplished by computing the growth ratios for each intersection movement (2025 background PM peak hour conditions as compared to existing PM peak hour conditions). These same growth ratios were then applied to the existing AM peak hour traffic volumes in order to generate the 2025 background AM peak hour forecast conditions. Figure 4 illustrates 2025 AM and PM background (without project) peak hour conditions.

### **Off-Site Development**

As indicated previously, development is planned in the vicinity between Hubbard Road and Columbia Road. This entitled development is included in the COMPASS demographic data for 2025 background conditions.

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Figure 4



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**Level-of-Service Roadway Segments**

Table 5 summarizes the 2025 background LOS for the roadway segments in the study area. As noted, all roadway segments are projected to operate at LOS D or better under the current lane configuration and 2025 background traffic volumes. No roadway improvements are needed to mitigate 2025 background traffic conditions.

**Table 5 – Roadway Segment LOS – 2025 Background Traffic**

Roadway Segment	Functional Class	No. of Thru Lanes	Left-Turn Treatment	Threshold Volume		AM Peak Hour Major Direction		PM Peak Hour Major Direction	
				LOS D	LOS E	Vol (vph)	LOS	Vol (vph)	LOS
Hubbard Rd, SH69 to Locust Grove Rd	Minor Arterial	1	No LT Lane	550	575	250	< D	280	< D
Locust Grove Rd, Hubbard Rd to Columbia Rd	Minor Arterial	1	No LT Lane	540	575	400	< D	260	< D

**Level-of-Service Intersections**

Each of the intersections within the study area was evaluated under existing traffic control, lane configuration and 2025 background peak hour volumes. HCS7 Reports are included in the Appendix and results are summarized in Table 6.

**Table 6 – Intersection Traffic Operations – 2025 Background Traffic**

Intersection	Traffic Control Lane Group	AM LOS/Delay/v/c	PM LOS/Delay/v/c
Hubbard Rd/ SH69	Traffic Signal	C/45.6	C/34.1
	Eastbound	D/38.9/0.89	D/50.5/0.85
	Westbound	D/48.2/0.71	E/56.4/0.89
	Northbound	D/54.7/1.00	C/22.0/0.78
	Southbound	C/22.0/0.79	C/34.9/0.87
Hubbard Rd/ Locust Grove	TWSC	NR	NR
	Eastbound	NR/3.5/0.08	NR/1.5/0.02
	Westbound	NR/0.7/0.01	NR/2.1/0.05
	Northbound	E/40.9/0.81	D/28.2/0.49
	Southbound	C/16.2/0.17	E/39.7/0.77
Columbia Rd/ Locust Grove	AWSC	F/116.6/NR	F/118.1/NR
	Eastbound	F/214.5/NR	C/24.5/NR
	Westbound	C/16.6/NR	F/225.2/NR
	Northbound	E/39.4/NR	C/16.0/NR

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Intersection	Traffic Control Lane Group	AM LOS/Delay/v/c	PM LOS/Delay/v/c
	Southbound	B/15.0/NR	F/50.2/NR
Columbia Rd/ Locust Grove (with traffic signal)	Traffic Signal	C/24.6	B/14.7
	Eastbound	B/12.9/0.53	B/11.9/0.32
	Westbound	A/7.0/0.17	B/11.5/0.64
	Northbound	D/48.9/0.87	B/17.2/0.25
	Southbound	D/35.5/0.43	C/20.6/0.83

NR = not reported

TWSC = Two-way stop control

AWSC = All-way stop control

At Hubbard Road and SH69, the heavy through volume in the northbound direction under AM peak hour conditions is at capacity. Overall, the intersection operates at LOS D in both the AM and PM peak hour conditions and other critical movement v/c ratios are less than 1.0. Under this scenario additional capacity improvements have not specifically been identified, however continued monitoring of this issue is recommended. As SH69 is a state highway, further improvements at this location, beyond that anticipated by the ACHD *CIP*, would be subject to ITD approval.

The Hubbard and Locust Grove intersection experiences poor LOS for the northbound and southbound stop-controlled movements in the AM and PM peak hours respectively, however the associated v/c ratios are considered acceptable (less than 0.90). Therefore, no further improvements are recommended at this location.

The four-way stop at Columbia Road and Locust Grove is expected to operate poorly under both AM and PM peak hour conditions. ACHD Policy requires that intersections operating at LOS D or worse be evaluated for signalized control in accordance with the *Manual on Uniform Traffic Control Devices (MUTCD)* procedures. In accordance with these procedures, hourly traffic conditions were estimated based on projected 2025 volumes and the hourly distribution of daily traffic volumes, as recorded by the existing 24-hour counts. Under this scenario, it appears that this intersection would meet warrants for a traffic signal. Evaluation of this condition with a traffic signal yields very favorable traffic operations. As such, the subsequent intersection capacity analysis at this location will be completed assuming signalized control. The detailed signal warrant analysis is provided in the Appendix.

## ANALYSIS OF 2025 TOTAL (SITE PLUS BACKGROUND) TRAFFIC CONDITIONS

### Trip Generation

The number of trips generated by the proposed development was estimated using rates provide in the *ITE Trip Generation Manual, 10<sup>th</sup> Edition*. Table 6 provides a summary of these results for Daily, AM Peak Hour and PM Peak hour conditions.

**Table 7 – 2025 Trip Generation Summary**

Land Use Category	ITE Code	Size	Period	Trip Rate	Total Trips	Enter		Exit	
Single Family Detached Housing	210	254 DU	Weekday (vpd)	9.44	2398	50%	1199	50%	1199
			AM Peak Hr(vph)	0.74	188	25%	47	75%	141
			PM Peak Hr(vph)	0.99	251	63%	158	37%	93

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**Trip Distribution and Assignment**

Site traffic was distributed in consideration of existing travel patterns, site layout and the generalized development within this area. These preliminary assumptions were also reviewed with ACHD for concurrence and are summarized as follows:

- SH69 (North) 20%
- SH69 (South) 5%
- Locust Grove Rd (North) 40%
- Locust Grove Rd (South) 5%
- Hubbard Rd (East) to Eagle Rd 30%

Generally, this distributions assumes the 90% of traffic origins and destinations are to the north and 10% are to the south. Figure 5 illustrates the resultant site traffic distribution. The projected percent increase at each intersection (as compared to 2025 background volumes) is noted as follows:

- Hubbard Road and SH69, AM = +1.8%, PM = +2.1%
- Hubbard Road and Locust Grove Road, AM = +18.0%, PM = +20.5%
- Columbia Road and Locust Grove Road, AM = +5.6%, PM = + 6.8%

**Site Plus Background Traffic**

Site traffic was added to the 2025 background traffic in order to produce the 2025 total traffic conditions with the proposed development. Fig 6 illustrates the resultant traffic volumes for AM and PM peak hour conditions.

**Level-of-Service Roadway Segments**

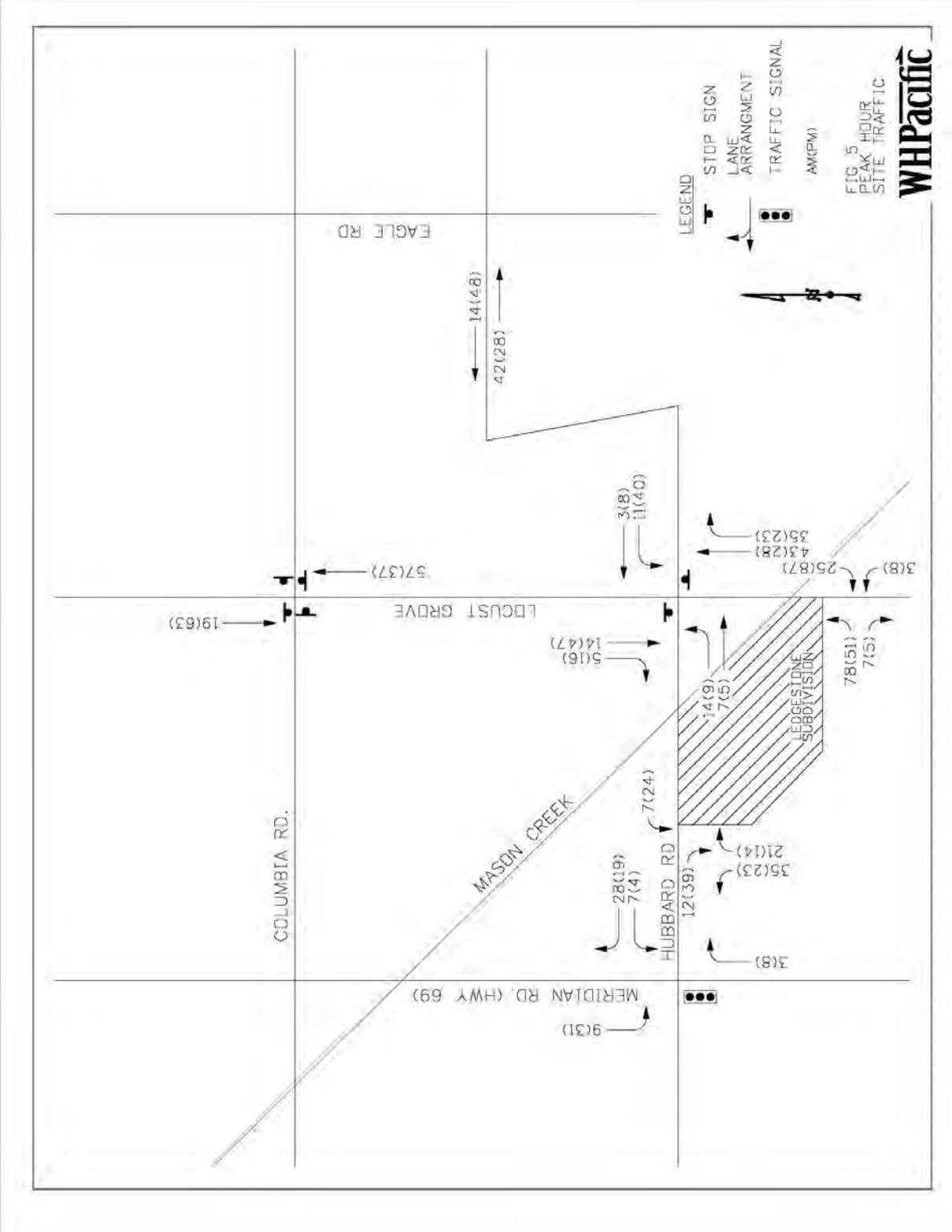
Table 8 summarizes the 2025 site plus background (total) LOS for the roadway segments in the study area. As noted, all roadway segments are projected to operate at LOS D or better under the current lane configuration and 2025 site plus background volumes. No roadway improvements are needed to mitigate these conditions.

**Table 8 – Roadway Segment LOS – 2025 Site Plus Background Traffic**

Roadway Segment	Functional Class	No. of Thru Lanes	Left-Turn Treatment	Threshold Volume		AM Peak Hour Major Direction		PM Peak Hour Major Direction	
				LOS D	LOS E	Vol (vph)	LOS	Vol (vph)	LOS
Hubbard Rd, SH69 to Locust Grove Rd	Minor Arterial	1	No LT Lane	550	575	262	< D	304	< D
Locust Grove Rd, Hubbard Rd to Columbia Rd	Minor Arterial	1	No LT Lane	540	575	457	< D	323	< D

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Figure 5





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**Level-of-Service Intersections**

Each of the intersections within the study area was evaluated under existing (or previously mitigated) traffic control, lane configuration and 2025 site plus background peak hour volumes. *HCS7* Reports are included in the Appendix and results are summarized in Table 9.

**Table 9 – Intersection Traffic Operations – 2025 Site Plus Background Traffic**

Intersection	Traffic Control Lane Group	AM LOS/Delay/v/c	PM LOS/Delay/v/c
Hubbard Rd/ SH69	Traffic Signal	D/53.4	D/38.0
	Eastbound	D/40.3/0.90	D/42.4/0.83
	Westbound	D/50.2/0.77	D/45.8/0.88
	Northbound	E/67.7/1.04	C/23.3/0.70
	Southbound	C/21.0/0.80	D/44.9/0.97
Hubbard Rd/ Locust Grove	TWSC	NR	NR
	Eastbound	NR/4.1/0.11	NR/2.3/0.04
	Westbound	NR/1.2/0.02	NR/2.8/0.08
	Northbound	F/116.2/1.12	NR/NR/NR
	Southbound	NR/NR/NR	F/159.2/1.22
Columbia Rd/ Locust Grove	Traffic Signal	B/16.6	B/15.9
	Eastbound	B/17.5/0.72	B/13.8/0.35
	Westbound	A/8.1/0.21	B/13.9/0.68
	Northbound	B/18.5/0.78	B/15.8/0.29
	Southbound	B/17.5/0.23	B/19.6/0.85
Stroebe Rd/Hubbard Rd	TWSC	NR	NR
	Eastbound	NR	NR
	Westbound	NR/0.6/0.01	NR/1.0/0.03
	Northbound	B/11.4/0.13	B/11.5/0.09
	Southbound	NA	NA
Rio Vallegas/Locust Grove	TWSC	NR	NR
	Eastbound	B/11.9/0.13	B/13.2/0.10
	Westbound	NA	NA
	Northbound	NR/0.1/0.00	NR/0.6/0.01
	Southbound	NR	NR

NR = not reported  
 TWSC = Two-way stop control  
 AWSC = All-way stop control  
 NA = Not applicable

As previously noted at Hubbard Road and SH69, the heavy through volume in the northbound direction under AM peak hour conditions is slightly over capacity. Overall, the intersection operates at LOS D in both the AM and PM peak hour conditions and other critical movement v/c ratios are less than 1.0. The additional site generated traffic at this location with the development is low, approximately 2% of 2025 background volumes. Further improvement at this location, beyond that anticipated by the ACHD *CIP*, would be subject to ITD approval.

Due to an approximate 20% increase in traffic volumes, further operational impacts are observed at Hubbard and Locust Grove under this scenario. In the northbound and southbound direction, LOS F and

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v/c ratios in excess of 1.0 are expected. Under these conditions, signal warrants at this location were reviewed. Results indicate that prevailing forecast traffic conditions at this intersection would be well shy of meeting warrants for a traffic signal. The detailed signal warrant analysis is provided in the Appendix. Alternatively, a four-way stop controlled intersection was also considered as a mitigation measure, but forecast traffic conditions do not meet *MUTCD* multi-way stop application thresholds. Further, forecast turn movements do not indicate unusually heavy traffic volumes so additional auxiliary lane capacity does not appear justified. Therefore, further improvements at this location are not recommended.

### **Turn Lane Analysis**

As indicated above two full access approaches are proposed for the development. One is located approximately 300 feet south of Hubbard Road, off S. Stroebel Road, toward the east, and the other is off Locust Grove, toward the west, south of Mason Creek Ditch (aka E. Rio Vallegas Street). Each site access approach forms a T-intersection with the existing roadway and is proposed to be stop-controlled. As noted in the above stop-controlled analysis both locations are expected to operate under favorable LOS and v/c conditions.

A turn lane analysis was further conducted at each of the locations using the turn lane threshold graphs provided in the ACHD Policy. While neither location appears to warrant a left turn lane, and a right turn lane is not warranted at S. Stroebel Road; a right turn lane appears to be warranted in the southbound direction at Locust Grove Road and E. Rio Vallegas Street.

### **SUMMARY OF RESULTS**

The study's key findings are summarized below.

#### **Existing Traffic Conditions**

1. For the existing traffic conditions analyzed with the existing roadway lane configuration, all study area roadway segments meet ACHD's minimum operational thresholds. Therefore, no roadway improvements are needed to mitigate the existing traffic.
2. For the existing traffic conditions analyzed with the existing intersection control and lane configuration, all study area intersections meet ACHD's minimum operational thresholds. Therefore, no intersection improvements are needed to mitigate the existing traffic.

#### **2025 Background Traffic Conditions**

3. There are no planned improvements to the study roadways or intersections by 2025 according to ACHD's current *FYWP* and *CIP*.
4. For the 2025 background traffic conditions analyzed with the existing roadway lane configuration, all study area roadway segments meet ACHD's minimum operational thresholds. Therefore, no roadway improvements are needed to mitigate 2025 background traffic.
5. For the 2025 background traffic conditions analyzed with the existing intersection control and lane configuration, one of the three study area intersections do not meet ACHD's minimum operational thresholds. The intersection of Columbia Road and Locust Grove is expected to meet traffic signal warrants under 2025 forecast conditions, therefore installation of a traffic signal is recommended to fully mitigate 2025 background traffic conditions.

Isolated performance issues are noted at the intersection of SH69 and Hubbard Road in the northbound and southbound direction. Overall, the intersection operates at an acceptable LOS D and no improvements have been recommended at this location. Further mitigation is subject to ITD review and approval.

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**2025 Site Plus Background Traffic Conditions**

6. This scenario reflects the full buildout of 254 single family dwelling units which is expected to generate 2,298 daily trips, 188 AM peak hour trips, and 251 PM peak hour trips.
7. Site traffic is anticipated to have the following general distribution pattern:
  - SH69 (North) 20%
  - SH69 (South) 5%
  - Locust Grove Rd (North) 40%
  - Locust Grove Rd (South) 5%
  - Hubbard Rd (East) to Eagle Rd 30%

8. For the 2025 site plus background traffic conditions analyzed with the existing roadway lane configuration, all study area roadway segments meet ACHD's minimum operation thresholds. Therefore, no roadway improvements are needed to mitigate the site plus background traffic.
9. For the 2025 site plus background traffic conditions analyzed with the existing (and 2025 background improvements) intersection control and lane configuration, all study area intersections meet ACHD's minimum operational thresholds. Therefore, no further intersection improvements are needed to mitigate the 2025 site plus traffic.

Isolated performance issues are noted at the intersection of SH69 and Hubbard Road in the northbound and southbound direction. Overall, the intersection operates at an acceptable LOS D and no improvements have been recommended at this location. Further mitigation is subject to ITD review and approval.

10. Two full access (T-intersection approaches) at Hubbard Road and S. Stroebel Road, and Locust Grove Road and E. Rio Vallegas Street will serve primary access to the subdivision. With the 2025 site plus background traffic conditions, stop control and the proposed lane configuration, the critical minor movements at the proposed site access intersections are expected to operate at LOS B or better.
11. With the 2025 site plus background traffic conditions, turn lane warrants are satisfied as follows:
  - Locust Grove and E. Rio Vallegas Street – southbound right turn lane

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APPENDIX

### Johnson-Kuna Proposed Development

The following summarizes the results of an area of influence model run for a proposed development located southwest of Hubbard and Locust Grove Roads. The proposed development will consist of 254 single family homes with an anticipated build out by 2025. See figure 1.

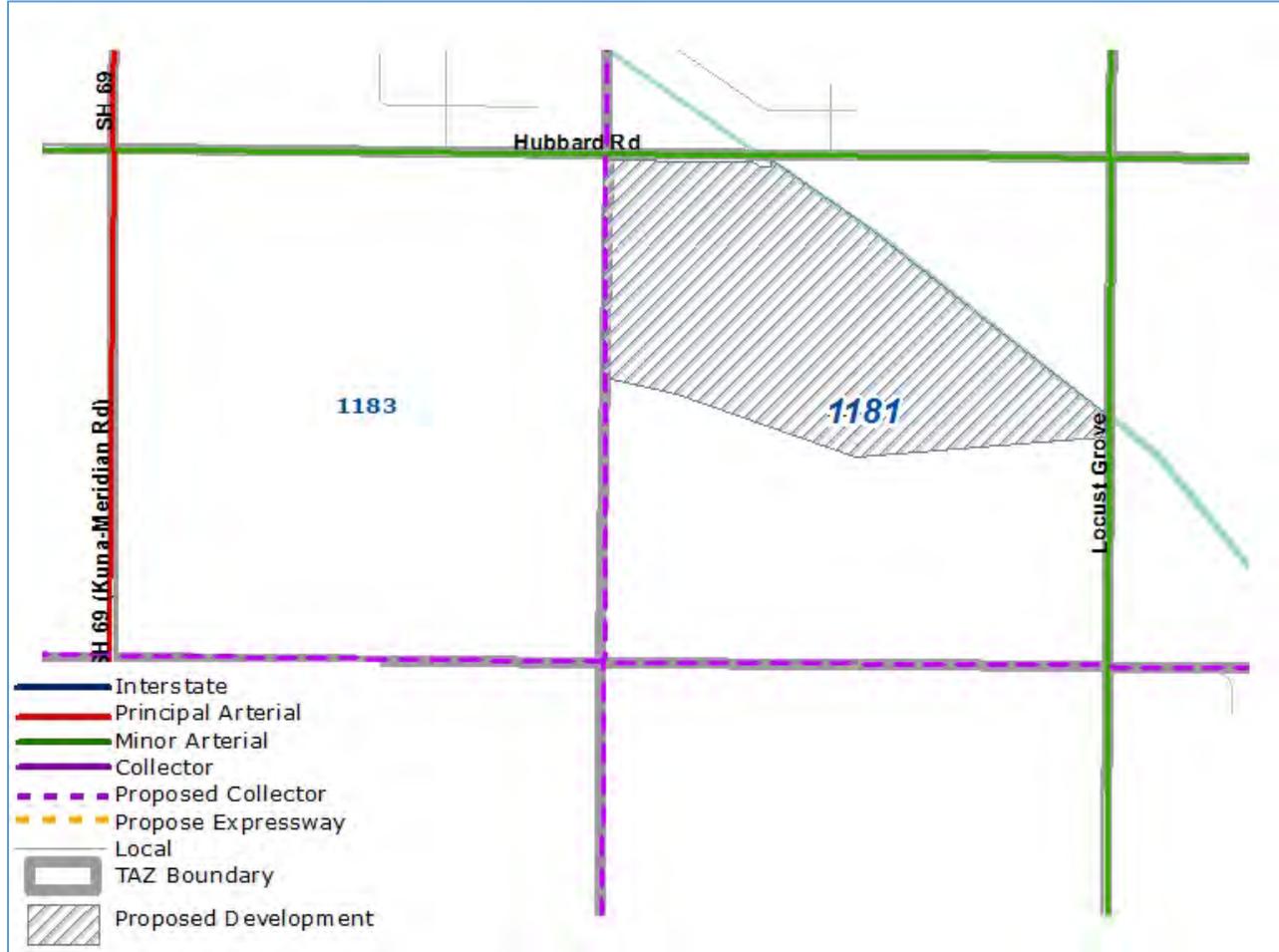


Figure 1: TAZ 1181

Table 1 provides the existing demographics for TAZ 1181, and the proposed development’s demographics used for the area of influence model run.

Table 1

	2018		2025 with proposal		2040	
	HH	Jobs	HH	Jobs	HH	Jobs
<b>TAZ 1181</b>	6	15	260	15	6	15

The area of influence results for the proposed development are shown in figures 2. The 2025 peak hour results are shown in figures 3 and 4.







### Cumulative Development

Figure 5 below shows the location of the preliminary plats adjacent to the proposed development. This entitled development is already included the demographic data set therefore, a cumulative development model run was not necessary.

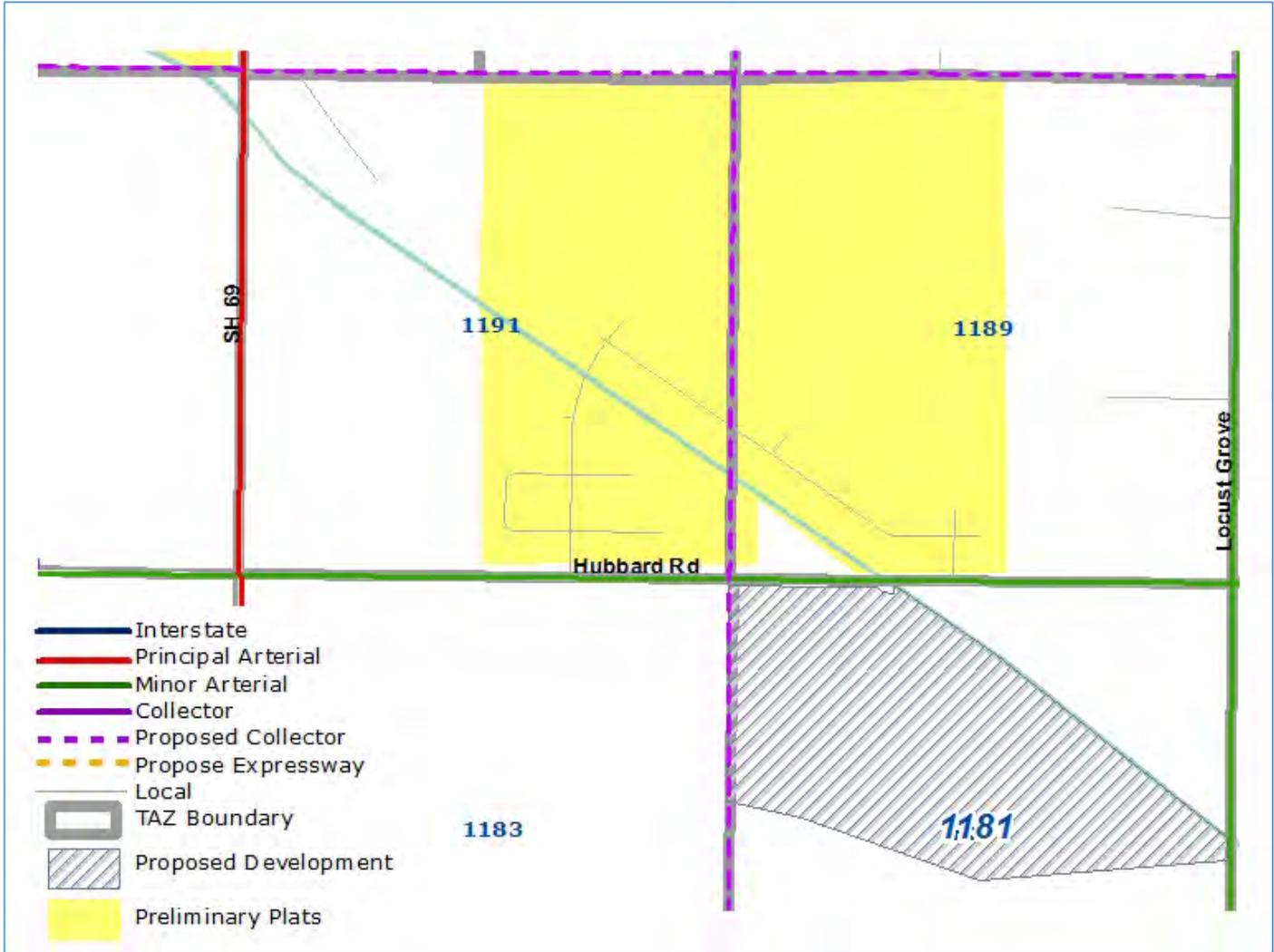


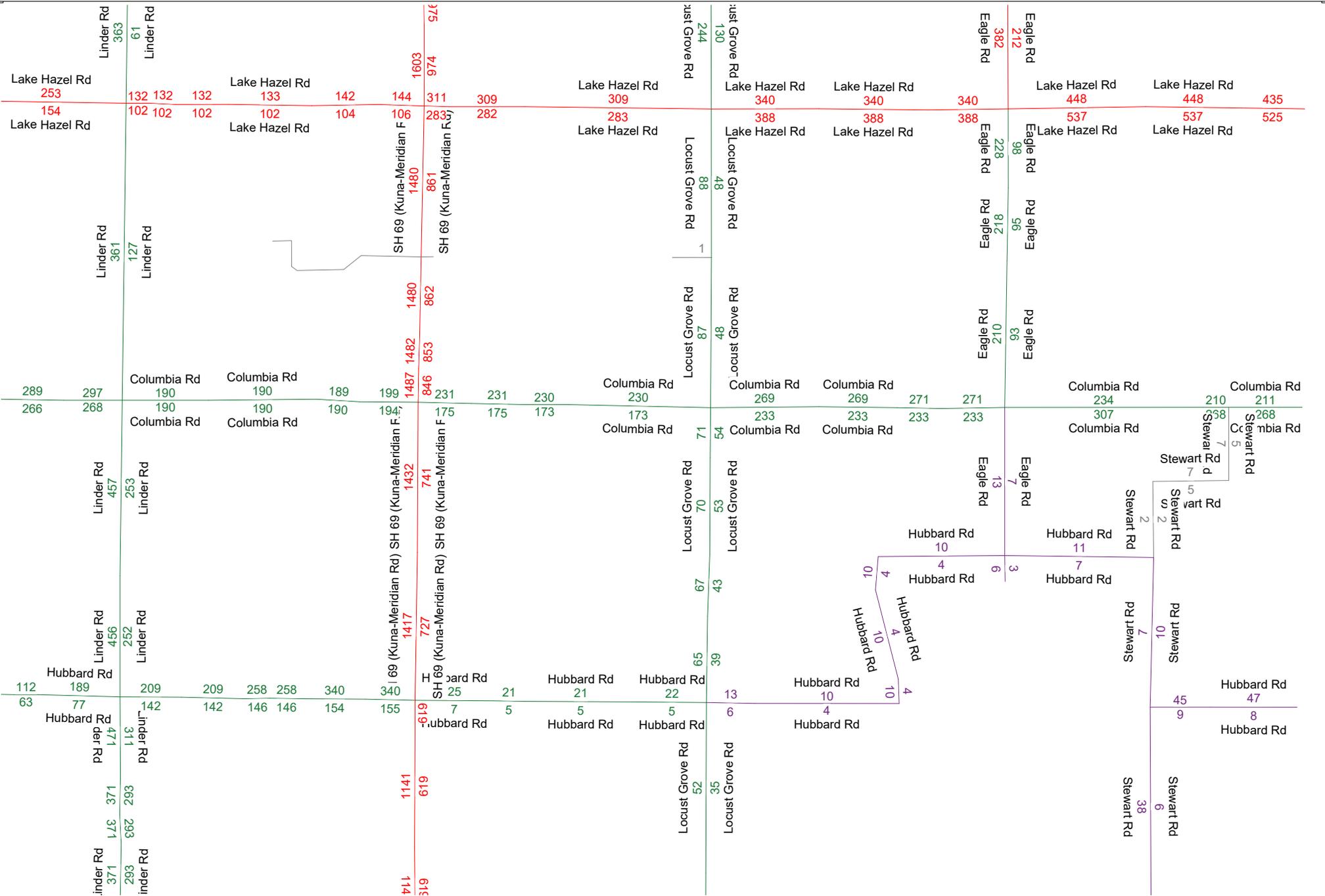
Figure 5

Table 2 provides the existing and forecasted demographics for TAZ 1181, 1189 and 1191.

Table 2

TAZ	2018		2025		2040	
	HH	Jobs	HH	Jobs	HH	Jobs
1181	6	1	260*	1	6	1
1189	7	25	242	25	242	25
1191	39	57	486	113	544	232

\*Includes the proposed development used for the special model run.



## L2 Data Collection

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Idaho (208) 860-7554 Utah (801) 431-2993

Study: WHPA0002  
Intersection: Meridian Rd / Hubbard Rd  
City, State: Ada County, Idaho  
Control: Stop Sign

File Name : Meridian Rd & Hubbard Rd  
Site Code : 00000000  
Start Date : 8/7/2018  
Page No : 1

**Groups Printed- General Traffic**

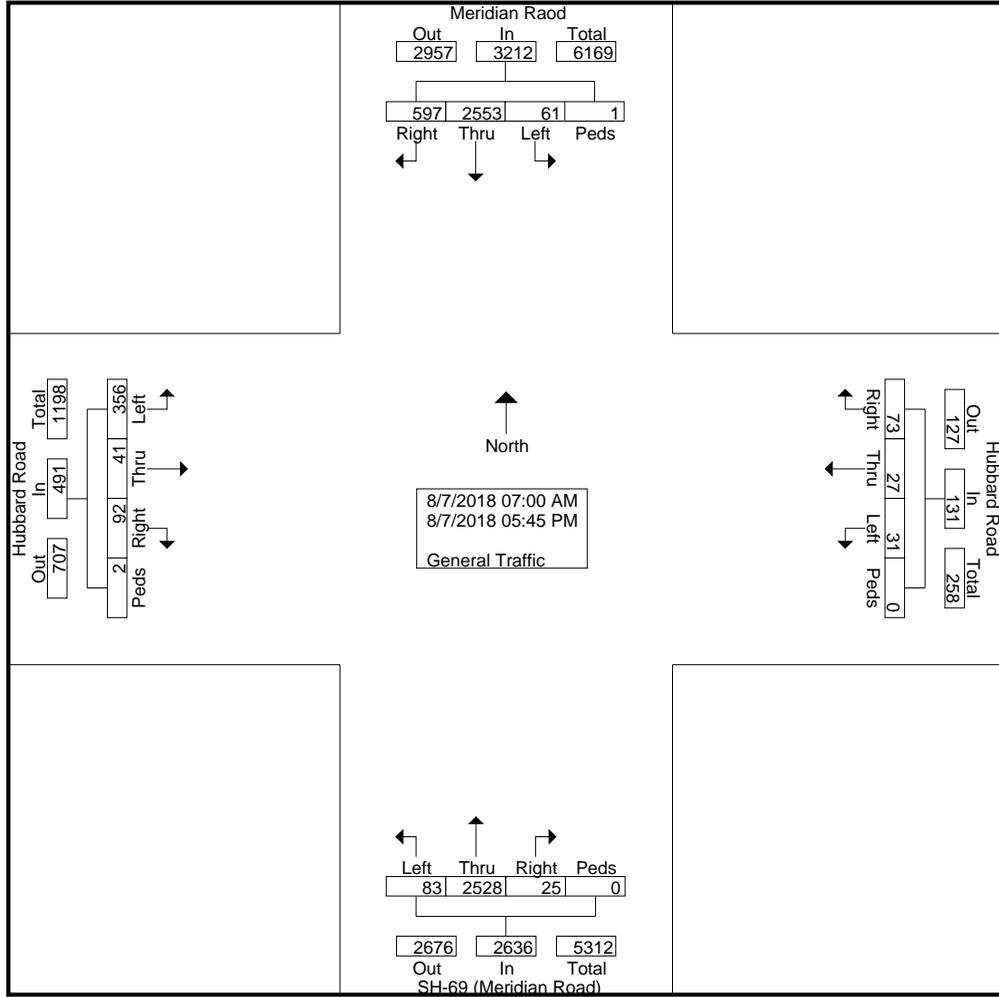
Start Time	Meridian Raod From North					Hubbard Road From East					SH-69 (Meridian Road) From South					Hubbard Road From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
07:00 AM	8	52	0	0	60	1	0	3	0	4	0	229	3	0	232	1	6	36	0	43	339
07:15 AM	5	72	5	0	82	2	1	2	0	5	1	254	2	0	257	2	8	46	0	56	400
07:30 AM	10	75	5	0	90	4	1	0	0	5	1	226	2	0	229	5	7	39	0	51	375
07:45 AM	19	125	4	0	148	3	2	2	0	7	2	165	2	0	169	6	2	12	1	21	345
<b>Total</b>	<b>42</b>	<b>324</b>	<b>14</b>	<b>0</b>	<b>380</b>	<b>10</b>	<b>4</b>	<b>7</b>	<b>0</b>	<b>21</b>	<b>4</b>	<b>874</b>	<b>9</b>	<b>0</b>	<b>887</b>	<b>14</b>	<b>23</b>	<b>133</b>	<b>1</b>	<b>171</b>	<b>1459</b>
08:00 AM	7	89	5	0	101	2	0	1	0	3	1	184	4	0	189	5	0	28	0	33	326
08:15 AM	10	70	3	0	83	1	0	2	0	3	2	176	3	0	181	4	1	26	1	32	299
08:30 AM	17	85	5	1	108	8	2	4	0	14	1	193	4	0	198	5	4	29	0	38	358
08:45 AM	13	81	2	0	96	3	0	0	0	3	2	143	4	0	149	4	1	30	0	35	283
<b>Total</b>	<b>47</b>	<b>325</b>	<b>15</b>	<b>1</b>	<b>388</b>	<b>14</b>	<b>2</b>	<b>7</b>	<b>0</b>	<b>23</b>	<b>6</b>	<b>696</b>	<b>15</b>	<b>0</b>	<b>717</b>	<b>18</b>	<b>6</b>	<b>113</b>	<b>1</b>	<b>138</b>	<b>1266</b>
-----																					
04:00 PM	46	182	6	0	234	3	4	1	0	8	4	115	6	0	125	8	1	13	0	22	389
04:15 PM	70	239	3	0	312	1	1	1	0	3	1	122	6	0	129	8	0	10	0	18	462
04:30 PM	60	209	4	0	273	8	4	2	0	14	1	117	10	0	128	9	1	8	0	18	433
04:45 PM	59	227	4	0	290	8	3	4	0	15	1	119	8	0	128	7	1	12	0	20	453
<b>Total</b>	<b>235</b>	<b>857</b>	<b>17</b>	<b>0</b>	<b>1109</b>	<b>20</b>	<b>12</b>	<b>8</b>	<b>0</b>	<b>40</b>	<b>7</b>	<b>473</b>	<b>30</b>	<b>0</b>	<b>510</b>	<b>32</b>	<b>3</b>	<b>43</b>	<b>0</b>	<b>78</b>	<b>1737</b>
05:00 PM	70	251	4	0	325	9	2	3	0	14	4	117	6	0	127	9	4	18	0	31	497
05:15 PM	71	266	4	0	341	12	3	2	0	17	3	134	9	0	146	7	2	17	0	26	530
05:30 PM	65	249	2	0	316	6	3	2	0	11	0	126	9	0	135	7	1	14	0	22	484
05:45 PM	67	281	5	0	353	2	1	2	0	5	1	108	5	0	114	5	2	18	0	25	497
<b>Total</b>	<b>273</b>	<b>1047</b>	<b>15</b>	<b>0</b>	<b>1335</b>	<b>29</b>	<b>9</b>	<b>9</b>	<b>0</b>	<b>47</b>	<b>8</b>	<b>485</b>	<b>29</b>	<b>0</b>	<b>522</b>	<b>28</b>	<b>9</b>	<b>67</b>	<b>0</b>	<b>104</b>	<b>2008</b>
<b>Grand Total</b>	<b>597</b>	<b>2553</b>	<b>61</b>	<b>1</b>	<b>3212</b>	<b>73</b>	<b>27</b>	<b>31</b>	<b>0</b>	<b>131</b>	<b>25</b>	<b>2528</b>	<b>83</b>	<b>0</b>	<b>2636</b>	<b>92</b>	<b>41</b>	<b>356</b>	<b>2</b>	<b>491</b>	<b>6470</b>
<b>Apprch %</b>	<b>18.6</b>	<b>79.5</b>	<b>1.9</b>	<b>0</b>		<b>55.7</b>	<b>20.6</b>	<b>23.7</b>	<b>0</b>		<b>0.9</b>	<b>95.9</b>	<b>3.1</b>	<b>0</b>		<b>18.7</b>	<b>8.4</b>	<b>72.5</b>	<b>0.4</b>		
<b>Total %</b>	<b>9.2</b>	<b>39.5</b>	<b>0.9</b>	<b>0</b>	<b>49.6</b>	<b>1.1</b>	<b>0.4</b>	<b>0.5</b>	<b>0</b>	<b>2</b>	<b>0.4</b>	<b>39.1</b>	<b>1.3</b>	<b>0</b>	<b>40.7</b>	<b>1.4</b>	<b>0.6</b>	<b>5.5</b>	<b>0</b>	<b>7.6</b>	

# L2 Data Collection

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Study: WHPA0002  
Intersection: Meridian Rd / Hubbard Rd  
City, State: Ada County, Idaho  
Control: Stop Sign

File Name : Meridian Rd & Hubbard Rd  
Site Code : 00000000  
Start Date : 8/7/2018  
Page No : 2



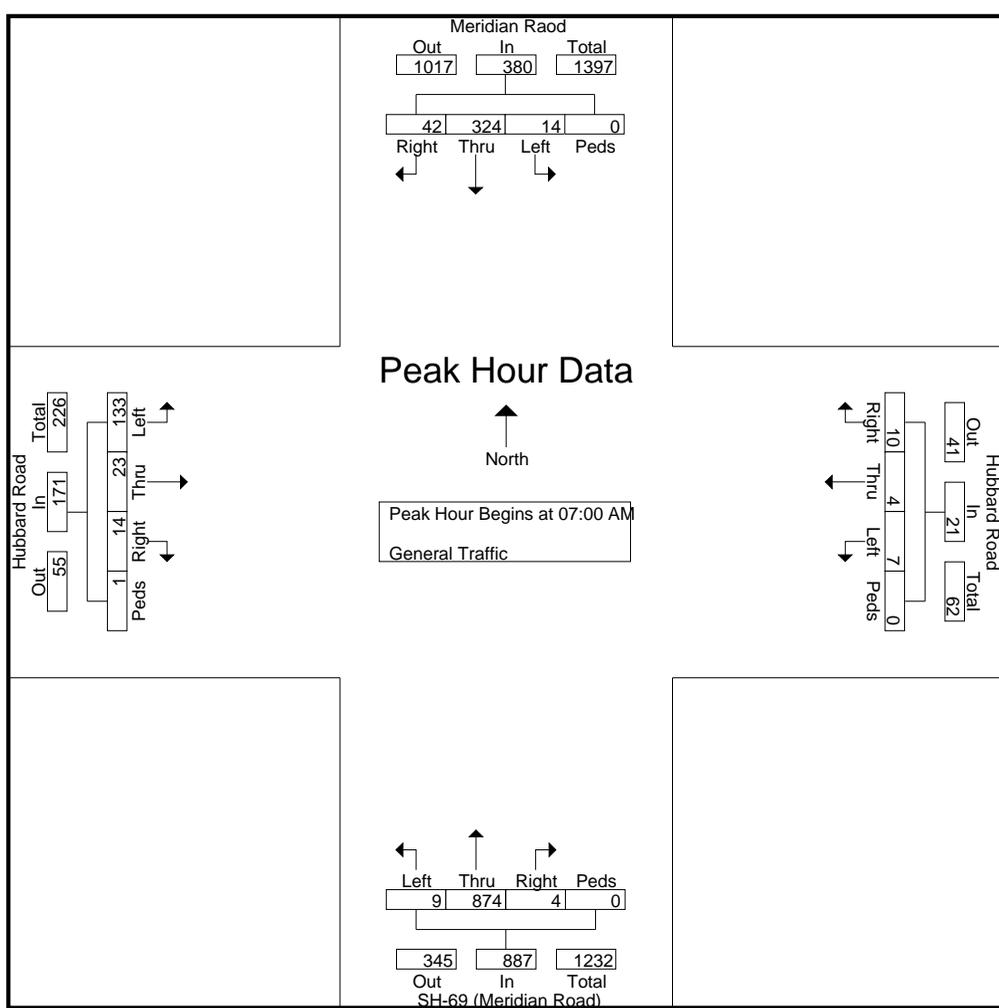
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Page No : 3

Start Time	Meridian Raod From North					Hubbard Road From East					SH-69 (Meridian Road) From South					Hubbard Road From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
<b>Peak Hour Analysis From 07:00 AM to 11:45 AM - Peak 1 of 1</b>																					
Peak Hour for Entire Intersection Begins at 07:00 AM																					
07:00 AM	8	52	0	0	60	1	0	3	0	4	0	229	3	0	232	1	6	36	0	43	339
07:15 AM	5	72	5	0	82	2	1	2	0	5	1	254	2	0	257	2	8	46	0	56	400
07:30 AM	10	75	5	0	90	4	1	0	0	5	1	226	2	0	229	5	7	39	0	51	375
07:45 AM	19	125	4	0	148	3	2	2	0	7	2	165	2	0	169	6	2	12	1	21	345
Total Volume	42	324	14	0	380	10	4	7	0	21	4	874	9	0	887	14	23	133	1	171	1459
% App. Total	11.1	85.3	3.7	0		47.6	19	33.3	0		0.5	98.5	1	0		8.2	13.5	77.8	0.6		
PHF	.553	.648	.700	.000	.642	.625	.500	.583	.000	.750	.500	.860	.750	.000	.863	.583	.719	.723	.250	.763	.912



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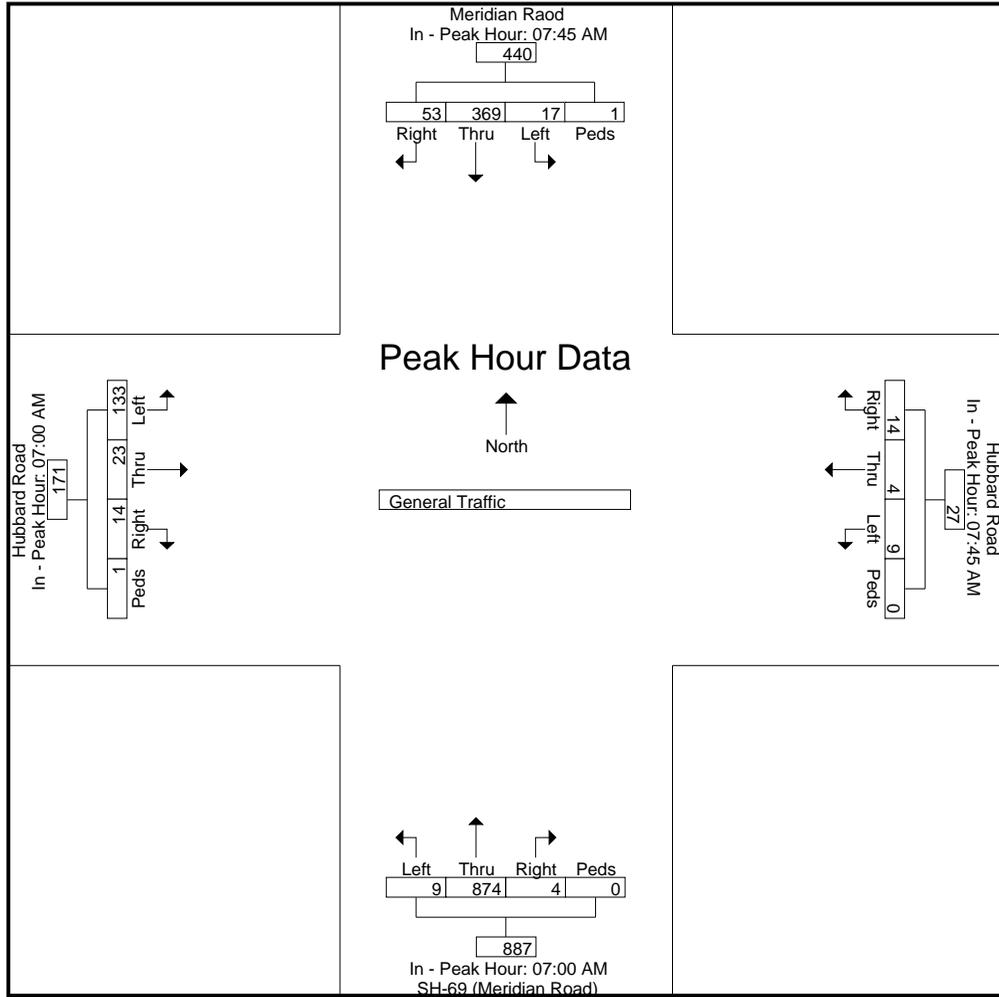
Study: WHPA0002  
Intersection: Meridian Rd / Hubbard Rd  
City, State: Ada County, Idaho  
Control: Stop Sign

File Name : Meridian Rd & Hubbard Rd  
Site Code : 00000000  
Start Date : 8/7/2018  
Page No : 4

Start Time	Meridian Raod From North					Hubbard Road From East					SH-69 (Meridian Road) From South					Hubbard Road From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	

**Peak Hour Analysis From 07:00 AM to 11:45 AM - Peak 1 of 1**  
Peak Hour for Each Approach Begins at:

	07:45 AM					07:45 AM					07:00 AM					07:00 AM				
+0 mins.	19	125	4	0	148	3	2	2	0	7	0	229	3	0	232	1	6	36	0	43
+15 mins.	7	89	5	0	101	2	0	1	0	3	1	254	2	0	257	2	8	46	0	56
+30 mins.	10	70	3	0	83	1	0	2	0	3	1	226	2	0	229	5	7	39	0	51
+45 mins.	17	85	5	1	108	8	2	4	0	14	2	165	2	0	169	6	2	12	1	21
Total Volume	53	369	17	1	440	14	4	9	0	27	4	874	9	0	887	14	23	133	1	171
% App. Total	12	83.9	3.9	0.2		51.9	14.8	33.3	0		0.5	98.5	1	0		8.2	13.5	77.8	0.6	
PHF	.697	.738	.850	.250	.743	.438	.500	.563	.000	.482	.500	.860	.750	.000	.863	.583	.719	.723	.250	.763



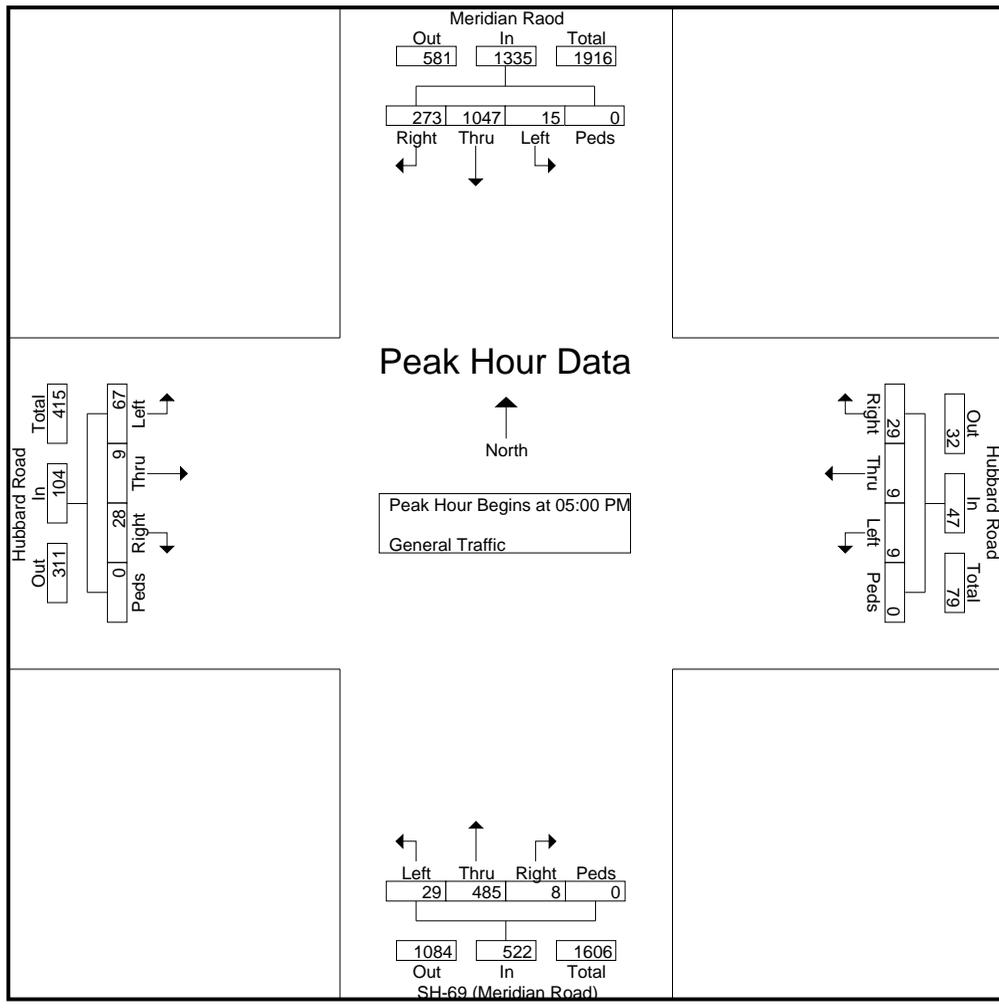
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Start Date : 8/7/2018  
Page No : 5

Start Time	Meridian Raod From North					Hubbard Road From East					SH-69 (Meridian Road) From South					Hubbard Road From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
<b>Peak Hour Analysis From 12:00 PM to 05:45 PM - Peak 1 of 1</b>																					
Peak Hour for Entire Intersection Begins at 05:00 PM																					
05:00 PM	70	251	4	0	325	9	2	3	0	14	4	117	6	0	127	9	4	18	0	31	497
05:15 PM	71	266	4	0	341	12	3	2	0	17	3	134	9	0	146	7	2	17	0	26	530
05:30 PM	65	249	2	0	316	6	3	2	0	11	0	126	9	0	135	7	1	14	0	22	484
05:45 PM	67	281	5	0	353	2	1	2	0	5	1	108	5	0	114	5	2	18	0	25	497
Total Volume	273	1047	15	0	1335	29	9	9	0	47	8	485	29	0	522	28	9	67	0	104	2008
% App. Total	20.4	78.4	1.1	0		61.7	19.1	19.1	0		1.5	92.9	5.6	0		26.9	8.7	64.4	0		
PHF	.961	.931	.750	.000	.945	.604	.750	.750	.000	.691	.500	.905	.806	.000	.894	.778	.563	.931	.000	.839	.947



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Study: WHPA0002  
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Control: Stop Sign

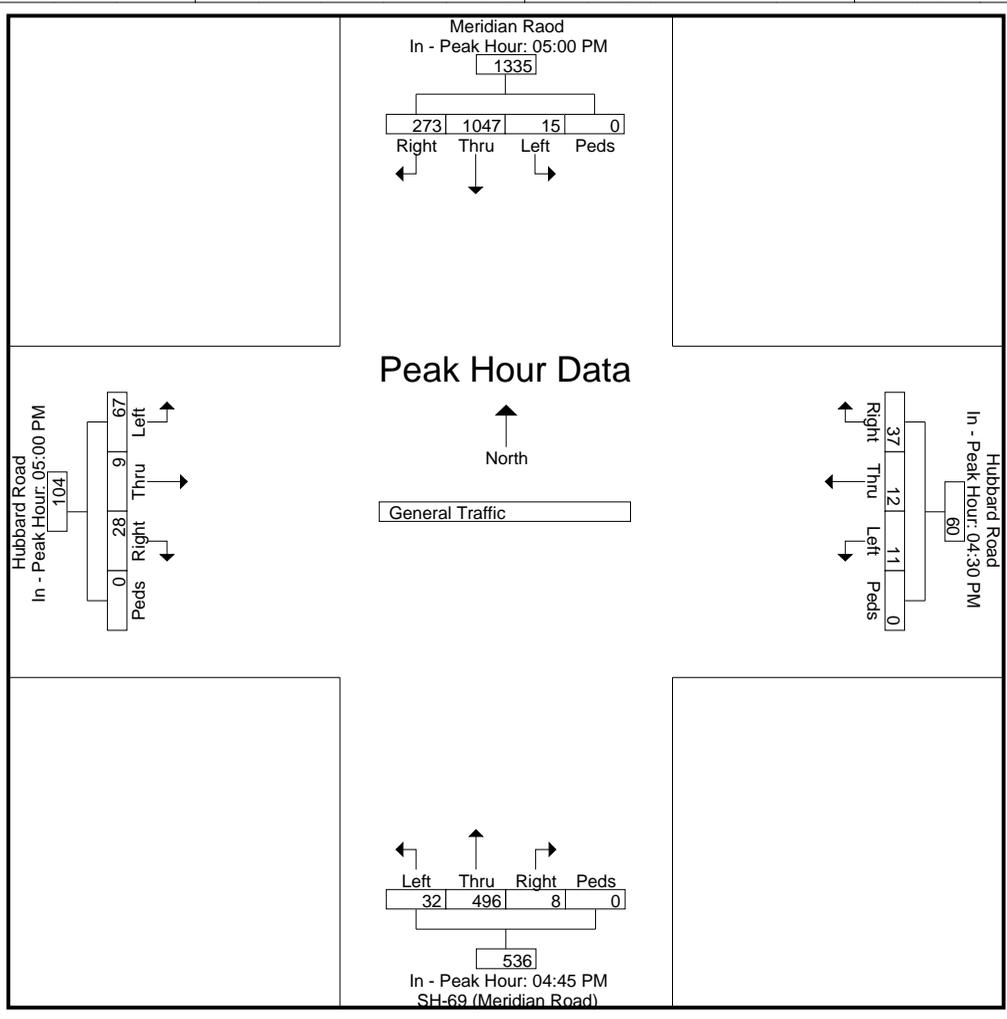
File Name : Meridian Rd & Hubbard Rd  
Site Code : 00000000  
Start Date : 8/7/2018  
Page No : 6

Start Time	Meridian Raod From North					Hubbard Road From East					SH-69 (Meridian Road) From South					Hubbard Road From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	

**Peak Hour Analysis From 12:00 PM to 05:45 PM - Peak 1 of 1**

Peak Hour for Each Approach Begins at:

	05:00 PM					04:30 PM					04:45 PM					05:00 PM				
+0 mins.	70	251	4	0	325	8	4	2	0	14	1	119	8	0	128	9	4	18	0	31
+15 mins.	71	266	4	0	341	8	3	4	0	15	4	117	6	0	127	7	2	17	0	26
+30 mins.	65	249	2	0	316	9	2	3	0	14	3	134	9	0	146	7	1	14	0	22
+45 mins.	67	281	5	0	353	12	3	2	0	17	0	126	9	0	135	5	2	18	0	25
Total Volume	273	1047	15	0	1335	37	12	11	0	60	8	496	32	0	536	28	9	67	0	104
% App. Total	20.4	78.4	1.1	0		61.7	20	18.3	0		1.5	92.5	6	0		26.9	8.7	64.4	0	
PHF	.961	.931	.750	.000	.945	.771	.750	.688	.000	.882	.500	.925	.889	.000	.918	.778	.563	.931	.000	.839



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Study: WHPA0002  
Intersection: Meridian Rd / Hubbard Rd  
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File Name : Meridian Rd & Hubbard Rd  
Site Code : 00000000  
Start Date : 8/7/2018  
Page No : 7

Image 1



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Study: WHPA0002  
Intersection: Locust Grove / Hubbard Rd  
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File Name : Locust Grove Rd & Hubbard Rd  
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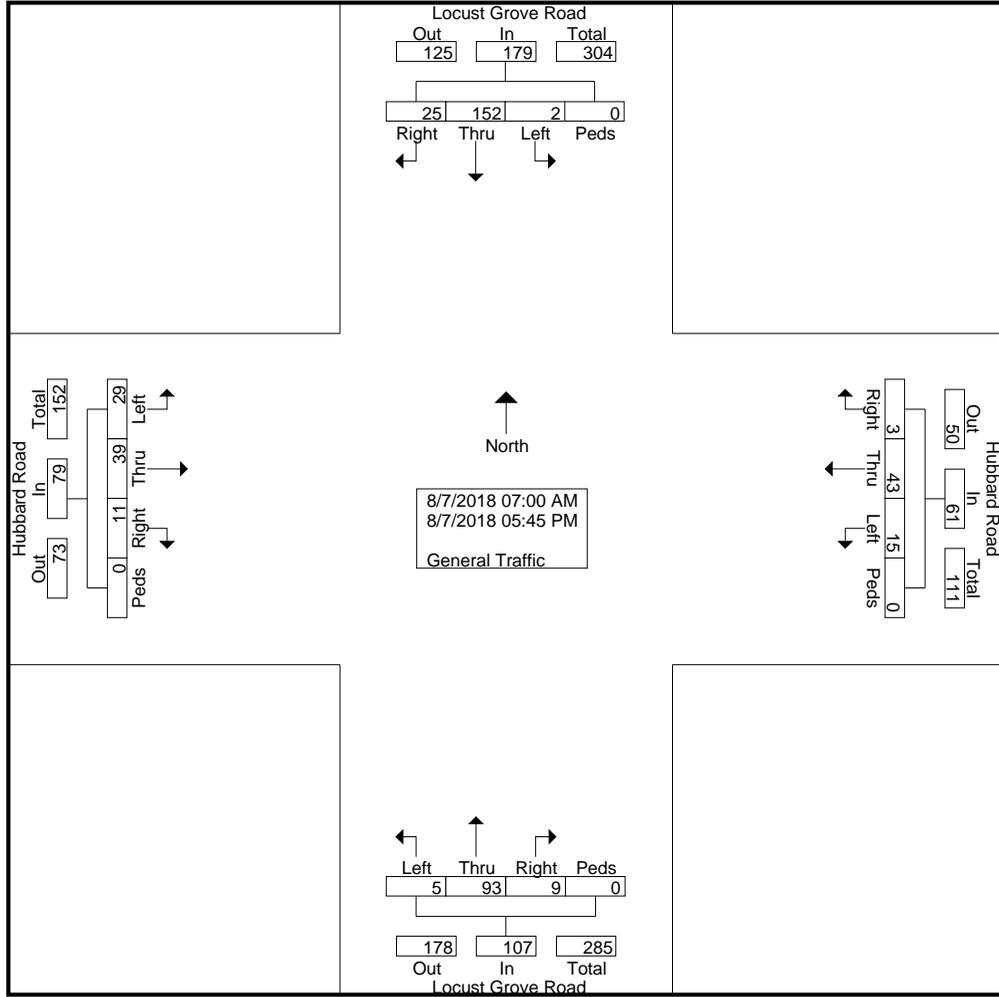
**Groups Printed- General Traffic**

Start Time	Locust Grove Road From North					Hubbard Road From East					Locust Grove Road From South					Hubbard Road From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
07:00 AM	0	1	0	0	1	0	3	0	0	3	0	10	0	0	10	0	3	4	0	7	21
07:15 AM	0	1	1	0	2	0	3	0	0	3	1	14	0	0	15	1	6	4	0	11	31
07:30 AM	4	5	0	0	9	0	2	0	0	2	0	8	0	0	8	1	5	3	0	9	28
07:45 AM	1	4	0	0	5	2	1	0	0	3	0	6	0	0	6	2	3	3	0	8	22
<b>Total</b>	<b>5</b>	<b>11</b>	<b>1</b>	<b>0</b>	<b>17</b>	<b>2</b>	<b>9</b>	<b>0</b>	<b>0</b>	<b>11</b>	<b>1</b>	<b>38</b>	<b>0</b>	<b>0</b>	<b>39</b>	<b>4</b>	<b>17</b>	<b>14</b>	<b>0</b>	<b>35</b>	<b>102</b>
08:00 AM	0	4	0	0	4	1	3	0	0	4	1	4	0	0	5	2	1	3	0	6	19
08:15 AM	0	6	1	0	7	0	2	0	0	2	0	10	0	0	10	0	1	1	0	2	21
08:30 AM	3	4	0	0	7	0	3	1	0	4	0	7	2	0	9	0	5	1	0	6	26
08:45 AM	1	4	0	0	5	0	0	0	0	0	1	2	0	0	3	0	1	0	0	1	9
<b>Total</b>	<b>4</b>	<b>18</b>	<b>1</b>	<b>0</b>	<b>23</b>	<b>1</b>	<b>8</b>	<b>1</b>	<b>0</b>	<b>10</b>	<b>2</b>	<b>23</b>	<b>2</b>	<b>0</b>	<b>27</b>	<b>2</b>	<b>8</b>	<b>5</b>	<b>0</b>	<b>15</b>	<b>75</b>
-----																					
04:00 PM	2	16	0	0	18	0	3	4	0	7	1	2	0	0	3	0	2	0	0	2	30
04:15 PM	2	16	0	0	18	0	1	1	0	2	1	9	0	0	10	0	2	5	0	7	37
04:30 PM	0	12	0	0	12	0	5	1	0	6	1	3	0	0	4	1	0	1	0	2	24
04:45 PM	5	15	0	0	20	0	6	0	0	6	0	2	1	0	3	0	1	2	0	3	32
<b>Total</b>	<b>9</b>	<b>59</b>	<b>0</b>	<b>0</b>	<b>68</b>	<b>0</b>	<b>15</b>	<b>6</b>	<b>0</b>	<b>21</b>	<b>3</b>	<b>16</b>	<b>1</b>	<b>0</b>	<b>20</b>	<b>1</b>	<b>5</b>	<b>8</b>	<b>0</b>	<b>14</b>	<b>123</b>
05:00 PM	1	16	0	0	17	0	1	1	0	2	1	7	2	0	10	0	1	1	0	2	31
05:15 PM	1	14	0	0	15	0	2	3	0	5	0	2	0	0	2	1	3	1	0	5	27
05:30 PM	2	19	0	0	21	0	5	1	0	6	1	4	0	0	5	1	1	0	0	2	34
05:45 PM	3	15	0	0	18	0	3	3	0	6	1	3	0	0	4	2	4	0	0	6	34
<b>Total</b>	<b>7</b>	<b>64</b>	<b>0</b>	<b>0</b>	<b>71</b>	<b>0</b>	<b>11</b>	<b>8</b>	<b>0</b>	<b>19</b>	<b>3</b>	<b>16</b>	<b>2</b>	<b>0</b>	<b>21</b>	<b>4</b>	<b>9</b>	<b>2</b>	<b>0</b>	<b>15</b>	<b>126</b>
Grand Total	25	152	2	0	179	3	43	15	0	61	9	93	5	0	107	11	39	29	0	79	426
Apprch %	14	84.9	1.1	0		4.9	70.5	24.6	0		8.4	86.9	4.7	0		13.9	49.4	36.7	0		
Total %	5.9	35.7	0.5	0	42	0.7	10.1	3.5	0	14.3	2.1	21.8	1.2	0	25.1	2.6	9.2	6.8	0	18.5	

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Control: Stop Sign

File Name : Locust Grove Rd & Hubbard Rd  
Site Code : 00000000  
Start Date : 8/7/2018  
Page No : 2



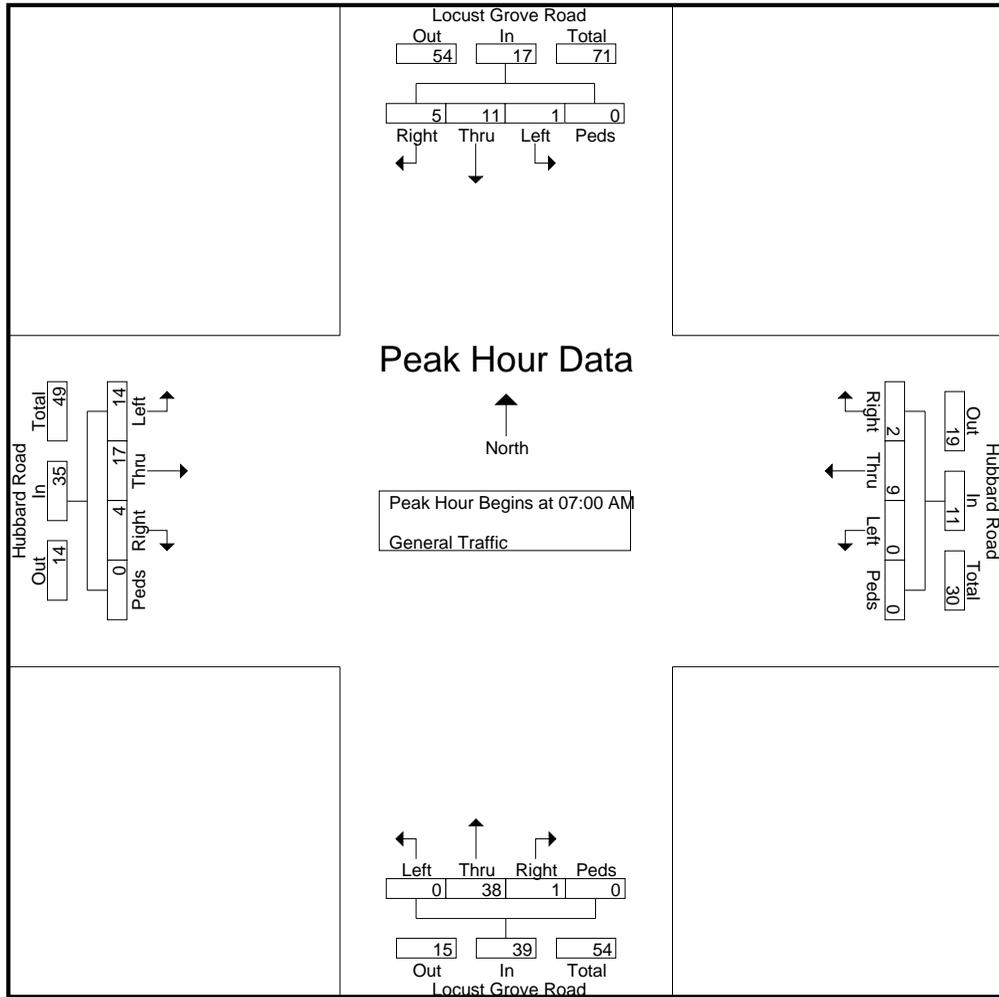
# L2 Data Collection

L2DataCollection.com  
Idaho (208) 860-7554 Utah (801) 431-2993

Study: WHPA0002  
Intersection: Locust Grove / Hubbard Rd  
City, State: Ada County, Idaho  
Control: Stop Sign

File Name : Locust Grove Rd & Hubbard Rd  
Site Code : 00000000  
Start Date : 8/7/2018  
Page No : 3

Start Time	Locust Grove Road From North					Hubbard Road From East					Locust Grove Road From South					Hubbard Road From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
<b>Peak Hour Analysis From 07:00 AM to 11:45 AM - Peak 1 of 1</b>																					
Peak Hour for Entire Intersection Begins at 07:00 AM																					
07:00 AM	0	1	0	0	1	0	3	0	0	3	0	10	0	0	10	0	3	4	0	7	21
07:15 AM	0	1	1	0	2	0	3	0	0	3	1	14	0	0	15	1	6	4	0	11	31
07:30 AM	4	5	0	0	9	0	2	0	0	2	0	8	0	0	8	1	5	3	0	9	28
07:45 AM	1	4	0	0	5	2	1	0	0	3	0	6	0	0	6	2	3	3	0	8	22
Total Volume	5	11	1	0	17	2	9	0	0	11	1	38	0	0	39	4	17	14	0	35	102
% App. Total	29.4	64.7	5.9	0		18.2	81.8	0	0		2.6	97.4	0	0		11.4	48.6	40	0		
PHF	.313	.550	.250	.000	.472	.250	.750	.000	.000	.917	.250	.679	.000	.000	.650	.500	.708	.875	.000	.795	.823



# L2 Data Collection

L2DataCollection.com  
Idaho (208) 860-7554 Utah (801) 431-2993

Study: WHPA0002  
Intersection: Locust Grove / Hubbard Rd  
City, State: Ada County, Idaho  
Control: Stop Sign

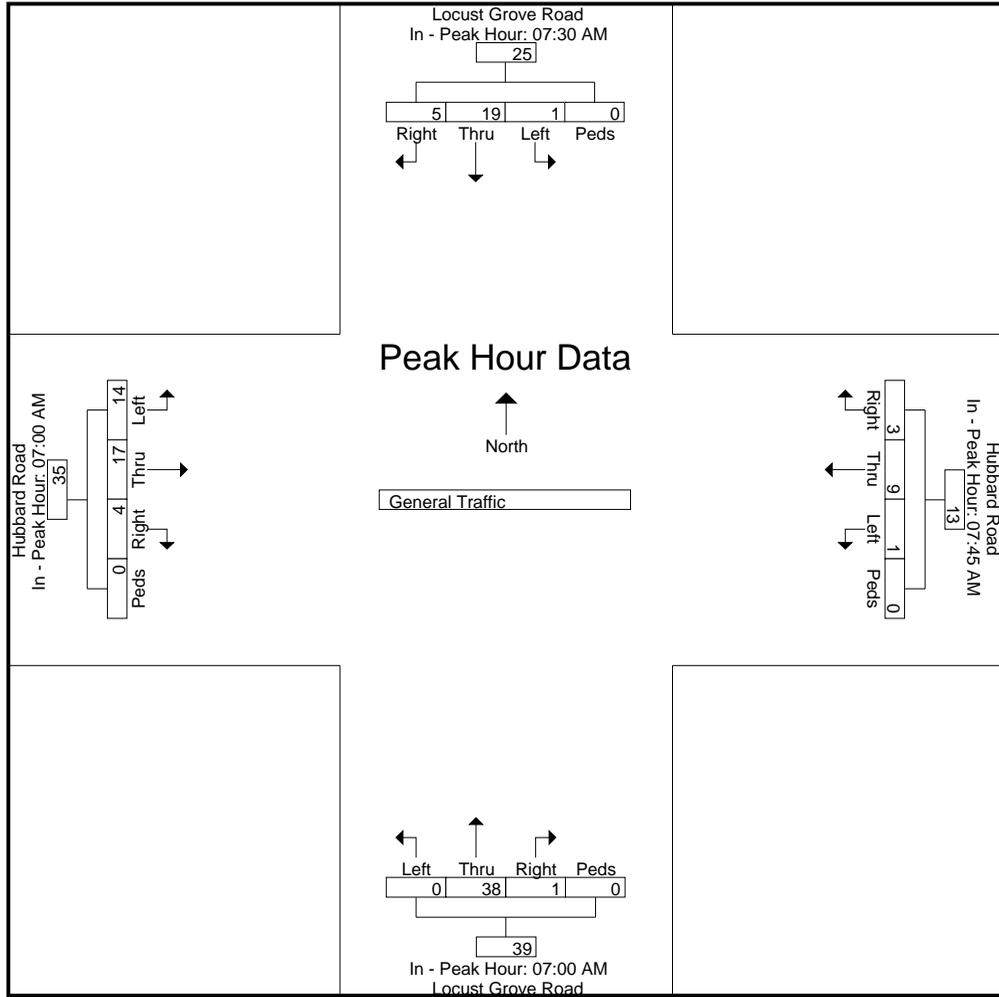
File Name : Locust Grove Rd & Hubbard Rd  
Site Code : 00000000  
Start Date : 8/7/2018  
Page No : 4

Start Time	Locust Grove Road From North					Hubbard Road From East					Locust Grove Road From South					Hubbard Road From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	

**Peak Hour Analysis From 07:00 AM to 11:45 AM - Peak 1 of 1**

Peak Hour for Each Approach Begins at:

	07:30 AM					07:45 AM					07:00 AM					07:00 AM				
+0 mins.	4	5	0	0	9	2	1	0	0	3	0	10	0	0	10	0	3	4	0	7
+15 mins.	1	4	0	0	5	1	3	0	0	4	1	14	0	0	15	1	6	4	0	11
+30 mins.	0	4	0	0	4	0	2	0	0	2	0	8	0	0	8	1	5	3	0	9
+45 mins.	0	6	1	0	7	0	3	1	0	4	0	6	0	0	6	2	3	3	0	8
Total Volume	5	19	1	0	25	3	9	1	0	13	1	38	0	0	39	4	17	14	0	35
% App. Total	20	76	4	0		23.1	69.2	7.7	0		2.6	97.4	0	0		11.4	48.6	40	0	
PHF	.313	.792	.250	.000	.694	.375	.750	.250	.000	.813	.250	.679	.000	.000	.650	.500	.708	.875	.000	.795



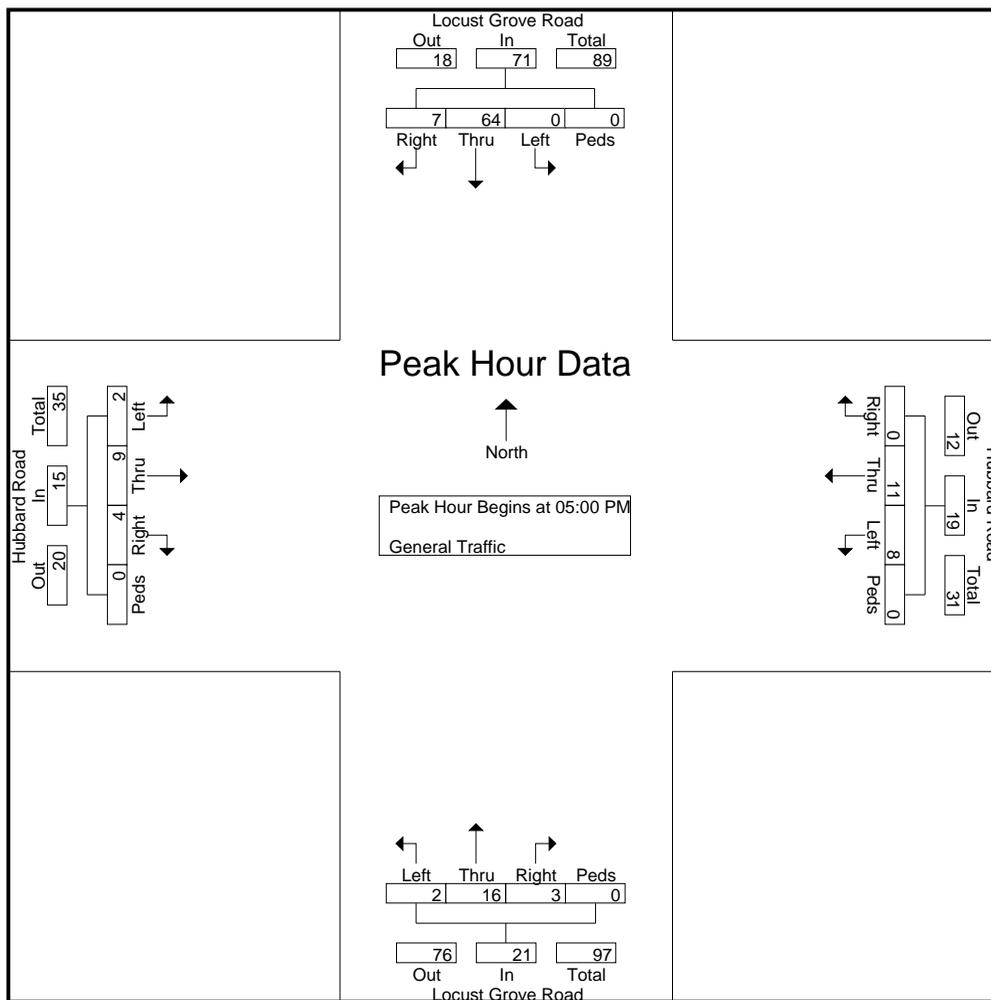
# L2 Data Collection

L2DataCollection.com  
Idaho (208) 860-7554 Utah (801) 431-2993

Study: WHPA0002  
Intersection: Locust Grove / Hubbard Rd  
City, State: Ada County, Idaho  
Control: Stop Sign

File Name : Locust Grove Rd & Hubbard Rd  
Site Code : 00000000  
Start Date : 8/7/2018  
Page No : 5

Start Time	Locust Grove Road From North					Hubbard Road From East					Locust Grove Road From South					Hubbard Road From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
<b>Peak Hour Analysis From 12:00 PM to 05:45 PM - Peak 1 of 1</b>																					
Peak Hour for Entire Intersection Begins at 05:00 PM																					
05:00 PM	1	16	0	0	17	0	1	1	0	2	1	7	2	0	10	0	1	1	0	2	31
05:15 PM	1	14	0	0	15	0	2	3	0	5	0	2	0	0	2	1	3	1	0	5	27
05:30 PM	2	19	0	0	21	0	5	1	0	6	1	4	0	0	5	1	1	0	0	2	34
05:45 PM	3	15	0	0	18	0	3	3	0	6	1	3	0	0	4	2	4	0	0	6	34
Total Volume	7	64	0	0	71	0	11	8	0	19	3	16	2	0	21	4	9	2	0	15	126
% App. Total	9.9	90.1	0	0		0	57.9	42.1	0		14.3	76.2	9.5	0		26.7	60	13.3	0		
PHF	.583	.842	.000	.000	.845	.000	.550	.667	.000	.792	.750	.571	.250	.000	.525	.500	.563	.500	.000	.625	.926



# L2 Data Collection

L2DataCollection.com  
Idaho (208) 860-7554 Utah (801) 431-2993

Study: WHPA0002  
Intersection: Locust Grove / Hubbard Rd  
City, State: Ada County, Idaho  
Control: Stop Sign

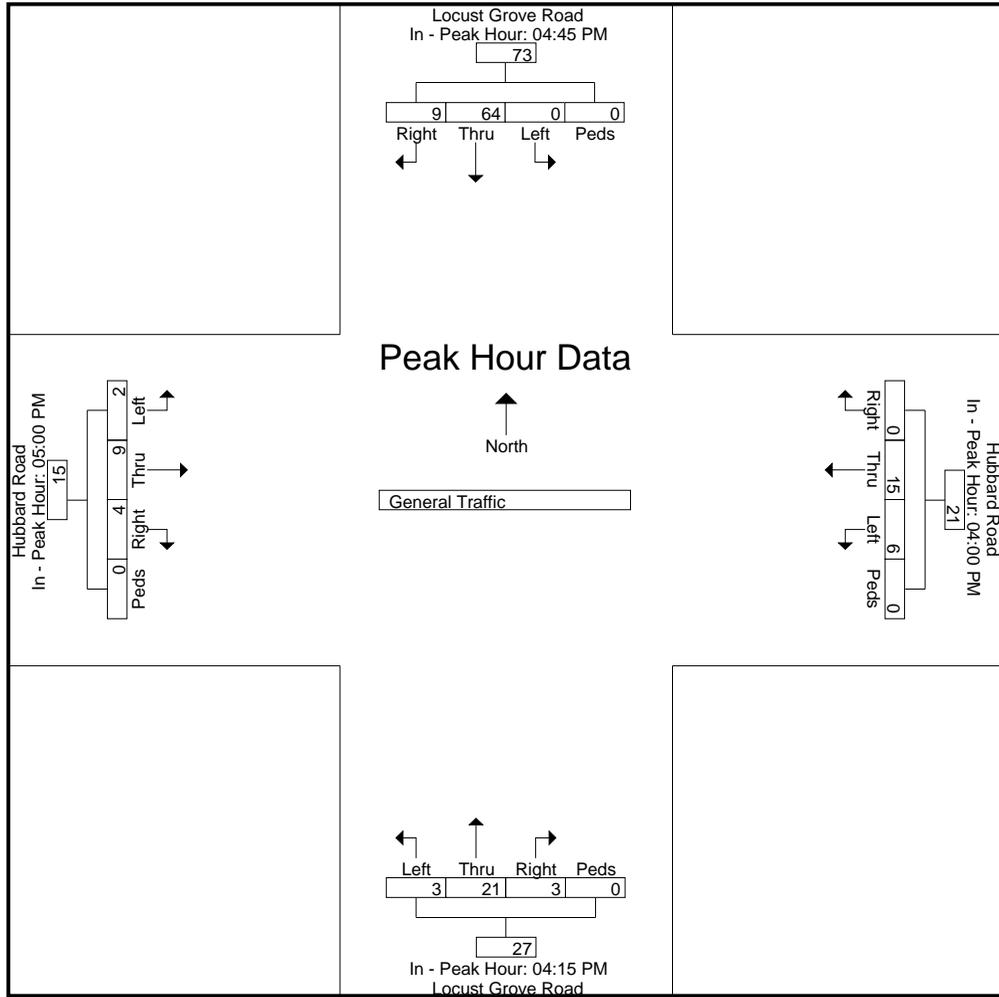
File Name : Locust Grove Rd & Hubbard Rd  
Site Code : 00000000  
Start Date : 8/7/2018  
Page No : 6

Start Time	Locust Grove Road From North					Hubbard Road From East					Locust Grove Road From South					Hubbard Road From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	

**Peak Hour Analysis From 12:00 PM to 05:45 PM - Peak 1 of 1**

Peak Hour for Each Approach Begins at:

	04:45 PM					04:00 PM					04:15 PM					05:00 PM				
+0 mins.	5	15	0	0	20	0	3	4	0	7	1	9	0	0	10	0	1	1	0	2
+15 mins.	1	16	0	0	17	0	1	1	0	2	1	3	0	0	4	1	3	1	0	5
+30 mins.	1	14	0	0	15	0	5	1	0	6	0	2	1	0	3	1	1	0	0	2
+45 mins.	2	19	0	0	21	0	6	0	0	6	1	7	2	0	10	2	4	0	0	6
Total Volume	9	64	0	0	73	0	15	6	0	21	3	21	3	0	27	4	9	2	0	15
% App. Total	12.3	87.7	0	0		0	71.4	28.6	0		11.1	77.8	11.1	0		26.7	60	13.3	0	
PHF	.450	.842	.000	.000	.869	.000	.625	.375	.000	.750	.750	.583	.375	.000	.675	.500	.563	.500	.000	.625



# L2 Data Collection

L2DataCollection.com  
Idaho (208) 860-7554 Utah (801) 431-2993

Study: WHPA0002  
Intersection: Locust Grove / Hubbard Rd  
City, State: Ada County, Idaho  
Control: Stop Sign

File Name : Locust Grove Rd & Hubbard Rd  
Site Code : 00000000  
Start Date : 8/7/2018  
Page No : 7

Image 1



# L2 Data Collection

L2DataCollection.com

Idaho (208) 860-7554 Utah (801) 431-2993

Study: WHPA0002  
Intersection: Locust Grove / Columbia Rd  
City, State: Ada County, Idaho  
Control: All Stop

File Name : Locust Grove Rd & Columbia Rd  
Site Code : 00000000  
Start Date : 8/7/2018  
Page No : 1

### Groups Printed- General Traffic

Start Time	Locust Grove Road From North					Hubbard Road From East					Locust Grove Road From South					Columbia Road From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
07:00 AM	1	1	2	0	4	1	12	0	0	13	2	11	0	0	13	0	41	15	0	56	86
07:15 AM	4	1	6	0	11	3	19	1	0	23	0	17	2	0	19	1	51	20	0	72	125
07:30 AM	8	7	7	0	22	4	21	2	0	27	3	9	0	0	12	3	69	14	0	86	147
07:45 AM	3	7	3	0	13	7	19	1	0	27	2	4	2	0	8	0	50	12	0	62	110
<b>Total</b>	<b>16</b>	<b>16</b>	<b>18</b>	<b>0</b>	<b>50</b>	<b>15</b>	<b>71</b>	<b>4</b>	<b>0</b>	<b>90</b>	<b>7</b>	<b>41</b>	<b>4</b>	<b>0</b>	<b>52</b>	<b>4</b>	<b>211</b>	<b>61</b>	<b>0</b>	<b>276</b>	<b>468</b>
08:00 AM	4	2	2	0	8	1	12	1	0	14	0	11	1	0	12	0	33	6	0	39	73
08:15 AM	1	3	1	0	5	5	10	0	0	15	4	10	1	0	15	2	19	8	0	29	64
08:30 AM	1	1	4	0	6	3	20	5	0	28	1	7	0	0	8	2	29	4	0	35	77
08:45 AM	0	2	3	0	5	5	11	1	0	17	1	5	1	0	7	2	25	4	0	31	60
<b>Total</b>	<b>6</b>	<b>8</b>	<b>10</b>	<b>0</b>	<b>24</b>	<b>14</b>	<b>53</b>	<b>7</b>	<b>0</b>	<b>74</b>	<b>6</b>	<b>33</b>	<b>3</b>	<b>0</b>	<b>42</b>	<b>6</b>	<b>106</b>	<b>22</b>	<b>0</b>	<b>134</b>	<b>274</b>
-----																					
04:00 PM	6	14	6	0	26	7	41	2	0	50	1	0	1	0	2	2	13	0	0	15	93
04:15 PM	4	15	2	0	21	4	39	2	0	45	1	6	4	0	11	1	21	1	0	23	100
04:30 PM	12	12	5	0	29	1	44	2	0	47	0	8	0	0	8	1	18	2	0	21	105
04:45 PM	20	15	4	0	39	6	64	4	0	74	1	2	0	0	3	0	14	5	0	19	135
<b>Total</b>	<b>42</b>	<b>56</b>	<b>17</b>	<b>0</b>	<b>115</b>	<b>18</b>	<b>188</b>	<b>10</b>	<b>0</b>	<b>216</b>	<b>3</b>	<b>16</b>	<b>5</b>	<b>0</b>	<b>24</b>	<b>4</b>	<b>66</b>	<b>8</b>	<b>0</b>	<b>78</b>	<b>433</b>
05:00 PM	18	15	1	0	34	4	66	0	0	70	3	5	2	0	10	1	22	3	0	26	140
05:15 PM	16	12	6	0	34	7	68	3	0	78	1	2	0	0	3	1	29	3	0	33	148
05:30 PM	14	16	5	0	35	2	61	3	0	66	1	2	0	0	3	2	28	4	0	34	138
05:45 PM	16	13	4	0	33	5	43	6	0	54	0	4	1	0	5	2	28	2	0	32	124
<b>Total</b>	<b>64</b>	<b>56</b>	<b>16</b>	<b>0</b>	<b>136</b>	<b>18</b>	<b>238</b>	<b>12</b>	<b>0</b>	<b>268</b>	<b>5</b>	<b>13</b>	<b>3</b>	<b>0</b>	<b>21</b>	<b>6</b>	<b>107</b>	<b>12</b>	<b>0</b>	<b>125</b>	<b>550</b>
Grand Total	128	136	61	0	325	65	550	33	0	648	21	103	15	0	139	20	490	103	0	613	1725
Apprch %	39.4	41.8	18.8	0		10	84.9	5.1	0		15.1	74.1	10.8	0		3.3	79.9	16.8	0		
Total %	7.4	7.9	3.5	0	18.8	3.8	31.9	1.9	0	37.6	1.2	6	0.9	0	8.1	1.2	28.4	6	0	35.5	

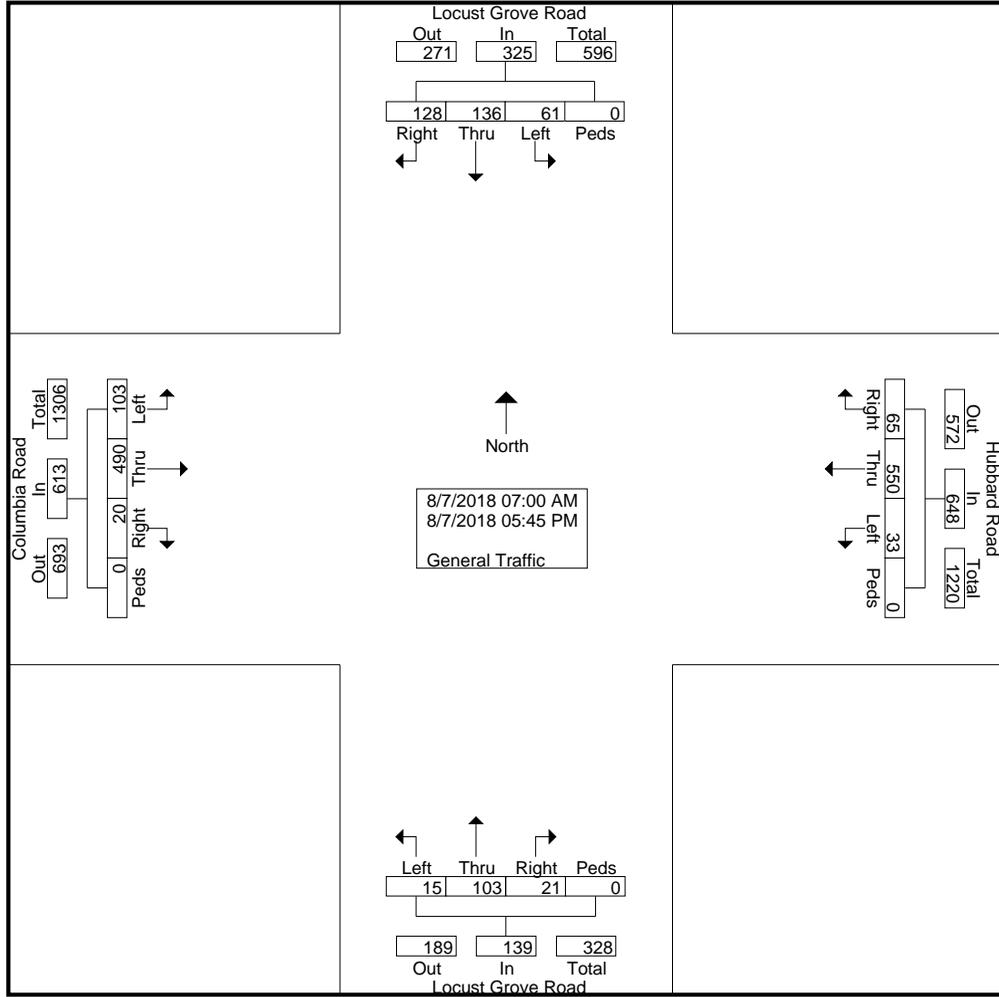
# L2 Data Collection

L2DataCollection.com

Idaho (208) 860-7554 Utah (801) 431-2993

Study: WHPA0002  
Intersection: Locust Grove / Columbia Rd  
City, State: Ada County, Idaho  
Control: All Stop

File Name : Locust Grove Rd & Columbia Rd  
Site Code : 00000000  
Start Date : 8/7/2018  
Page No : 2



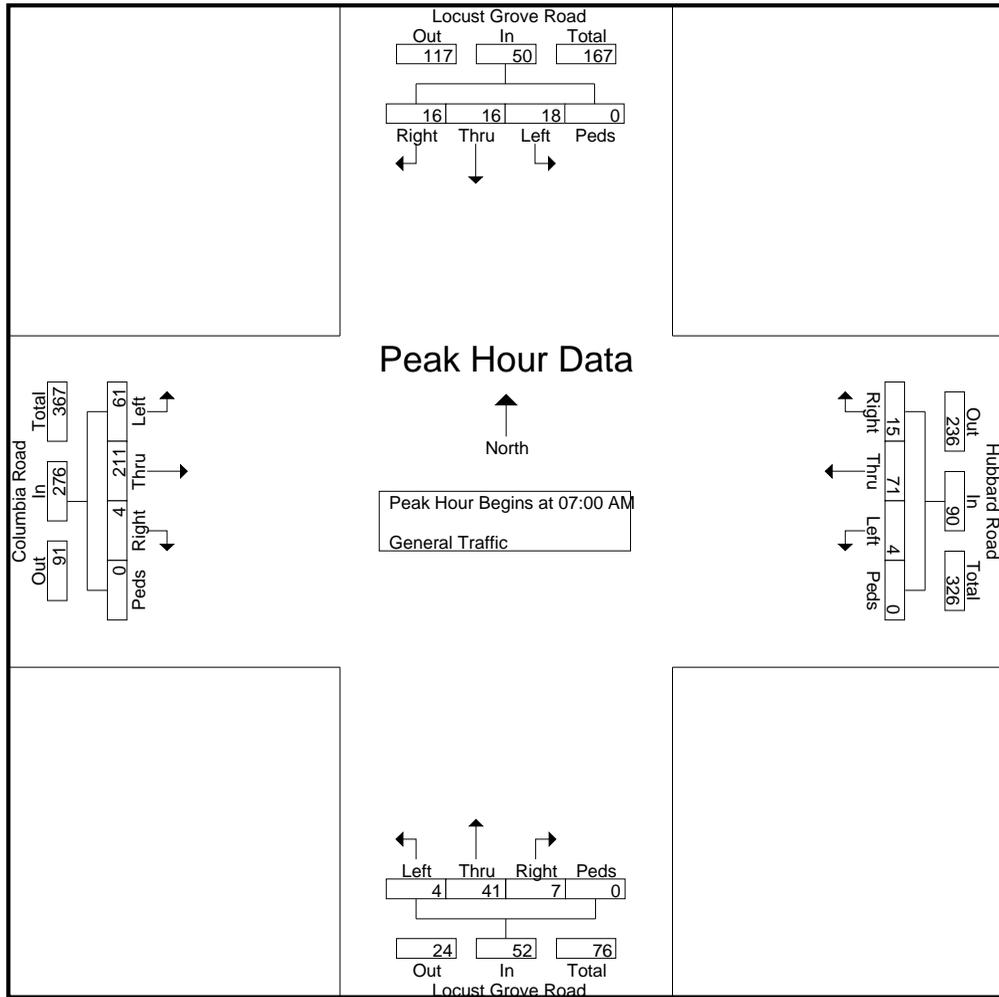
# L2 Data Collection

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Idaho (208) 860-7554 Utah (801) 431-2993

Study: WHPA0002  
Intersection: Locust Grove / Columbia Rd  
City, State: Ada County, Idaho  
Control: All Stop

File Name : Locust Grove Rd & Columbia Rd  
Site Code : 00000000  
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Page No : 3

Start Time	Locust Grove Road From North					Hubbard Road From East					Locust Grove Road From South					Columbia Road From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
<b>Peak Hour Analysis From 07:00 AM to 11:45 AM - Peak 1 of 1</b>																					
Peak Hour for Entire Intersection Begins at 07:00 AM																					
07:00 AM	1	1	2	0	4	1	12	0	0	13	2	11	0	0	13	0	41	15	0	56	86
07:15 AM	4	1	6	0	11	3	19	1	0	23	0	17	2	0	19	1	51	20	0	72	125
07:30 AM	8	7	7	0	22	4	21	2	0	27	3	9	0	0	12	3	69	14	0	86	147
07:45 AM	3	7	3	0	13	7	19	1	0	27	2	4	2	0	8	0	50	12	0	62	110
Total Volume	16	16	18	0	50	15	71	4	0	90	7	41	4	0	52	4	211	61	0	276	468
% App. Total	32	32	36	0		16.7	78.9	4.4	0		13.5	78.8	7.7	0		1.4	76.4	22.1	0		
PHF	.500	.571	.643	.000	.568	.536	.845	.500	.000	.833	.583	.603	.500	.000	.684	.333	.764	.763	.000	.802	.796



# L2 Data Collection

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Study: WHPA0002  
Intersection: Locust Grove / Columbia Rd  
City, State: Ada County, Idaho  
Control: All Stop

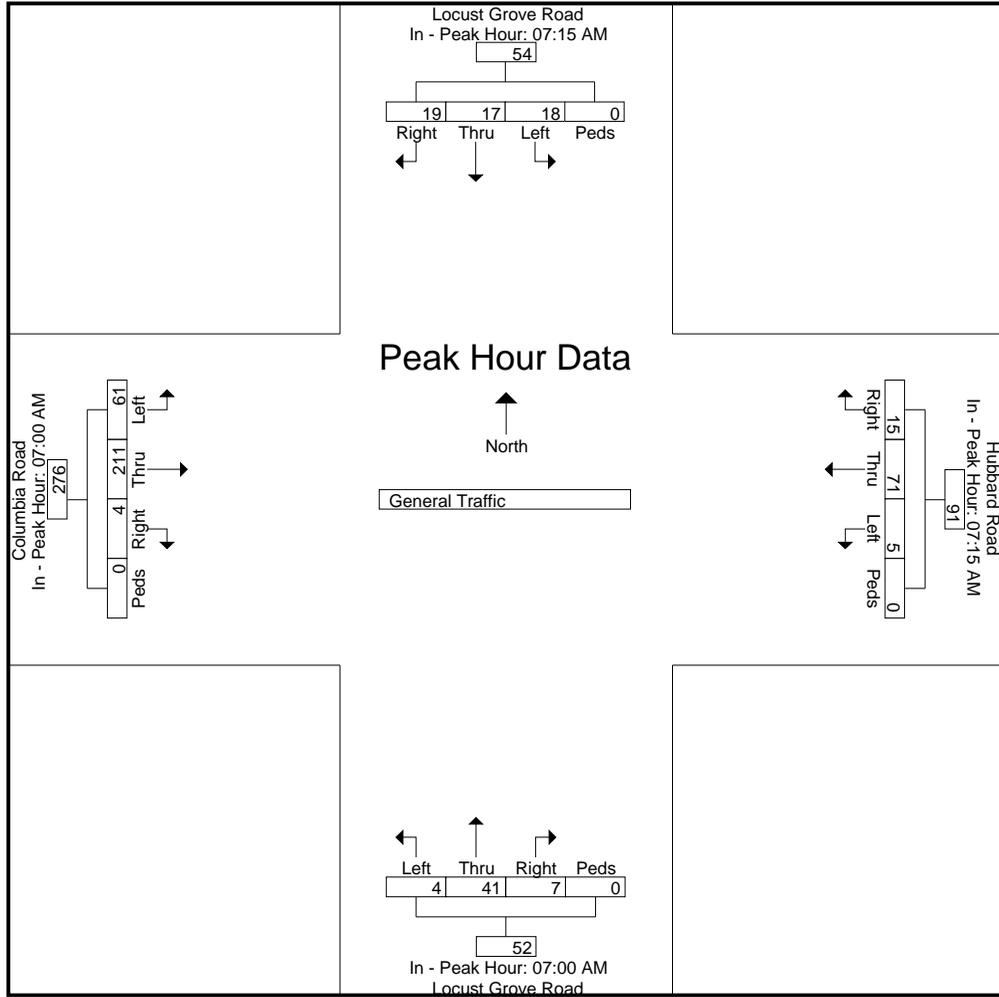
File Name : Locust Grove Rd & Columbia Rd  
Site Code : 00000000  
Start Date : 8/7/2018  
Page No : 4

Start Time	Locust Grove Road From North					Hubbard Road From East					Locust Grove Road From South					Columbia Road From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	

**Peak Hour Analysis From 07:00 AM to 11:45 AM - Peak 1 of 1**

Peak Hour for Each Approach Begins at:

	07:15 AM					07:15 AM					07:00 AM					07:00 AM				
+0 mins.	4	1	6	0	11	3	19	1	0	23	2	11	0	0	13	0	41	15	0	56
+15 mins.	8	7	7	0	22	4	21	2	0	27	0	17	2	0	19	1	51	20	0	72
+30 mins.	3	7	3	0	13	7	19	1	0	27	3	9	0	0	12	3	69	14	0	86
+45 mins.	4	2	2	0	8	1	12	1	0	14	2	4	2	0	8	0	50	12	0	62
Total Volume	19	17	18	0	54	15	71	5	0	91	7	41	4	0	52	4	211	61	0	276
% App. Total	35.2	31.5	33.3	0		16.5	78	5.5	0		13.5	78.8	7.7	0		1.4	76.4	22.1	0	
PHF	.594	.607	.643	.000	.614	.536	.845	.625	.000	.843	.583	.603	.500	.000	.684	.333	.764	.763	.000	.802



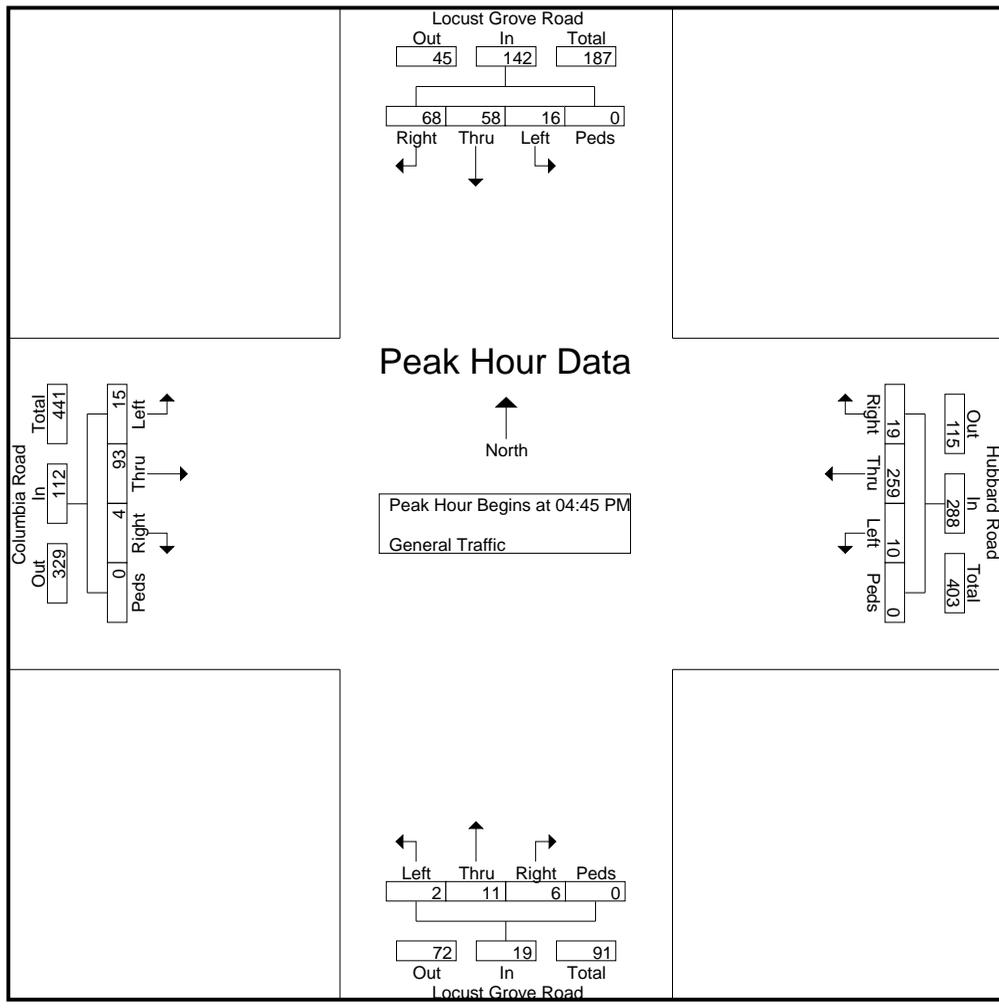
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Study: WHPA0002  
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	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
<b>Peak Hour Analysis From 12:00 PM to 05:45 PM - Peak 1 of 1</b>																					
<b>Peak Hour for Entire Intersection Begins at 04:45 PM</b>																					
04:45 PM	20	15	4	0	39	6	64	4	0	74	1	2	0	0	3	0	14	5	0	19	135
05:00 PM	18	15	1	0	34	4	66	0	0	70	3	5	2	0	10	1	22	3	0	26	140
05:15 PM	16	12	6	0	34	7	68	3	0	78	1	2	0	0	3	1	29	3	0	33	148
05:30 PM	14	16	5	0	35	2	61	3	0	66	1	2	0	0	3	2	28	4	0	34	138
Total Volume	68	58	16	0	142	19	259	10	0	288	6	11	2	0	19	4	93	15	0	112	561
% App. Total	47.9	40.8	11.3	0		6.6	89.9	3.5	0		31.6	57.9	10.5	0		3.6	83	13.4	0		
PHF	.850	.906	.667	.000	.910	.679	.952	.625	.000	.923	.500	.550	.250	.000	.475	.500	.802	.750	.000	.824	.948



# L2 Data Collection

L2DataCollection.com  
Idaho (208) 860-7554 Utah (801) 431-2993

Study: WHPA0002  
Intersection: Locust Grove / Columbia Rd  
City, State: Ada County, Idaho  
Control: All Stop

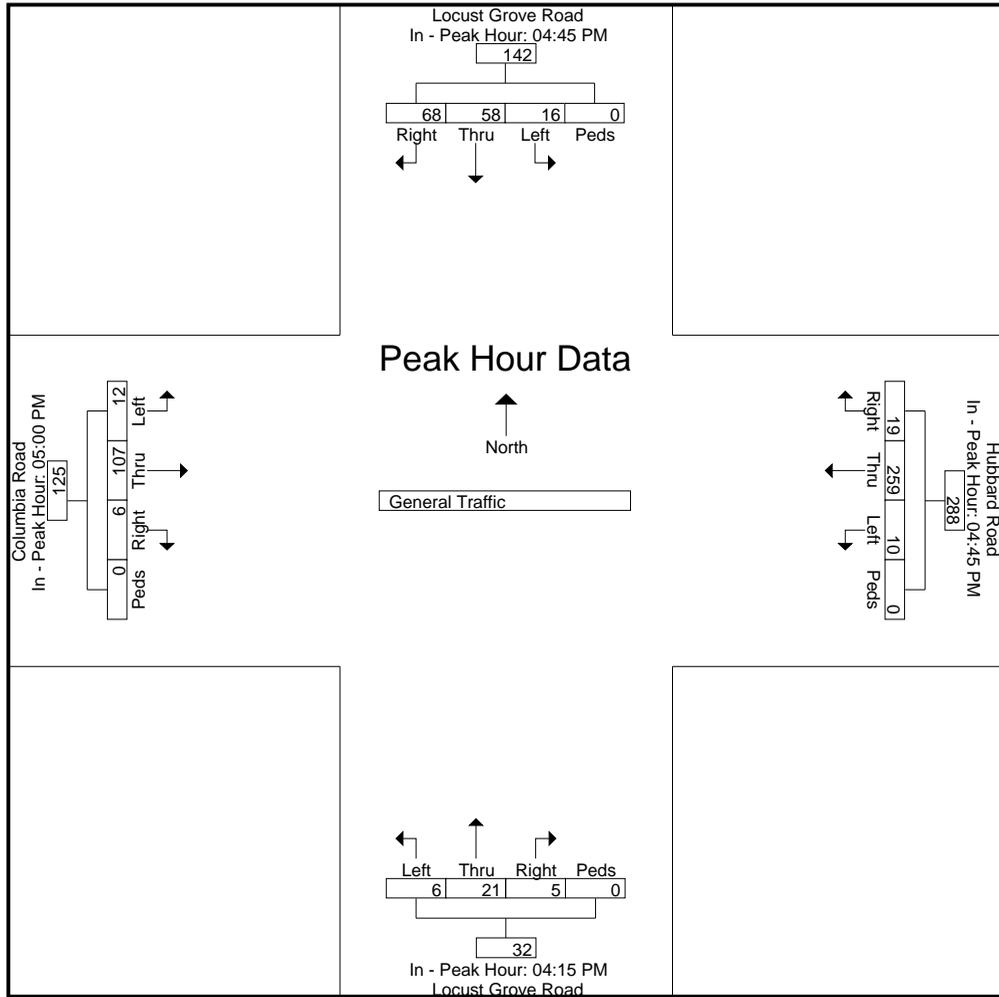
File Name : Locust Grove Rd & Columbia Rd  
Site Code : 00000000  
Start Date : 8/7/2018  
Page No : 6

Start Time	Locust Grove Road From North					Hubbard Road From East					Locust Grove Road From South					Columbia Road From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	

**Peak Hour Analysis From 12:00 PM to 05:45 PM - Peak 1 of 1**

Peak Hour for Each Approach Begins at:

	04:45 PM					04:45 PM					04:15 PM					05:00 PM				
+0 mins.	20	15	4	0	39	6	64	4	0	74	1	6	4	0	11	1	22	3	0	26
+15 mins.	18	15	1	0	34	4	66	0	0	70	0	8	0	0	8	1	29	3	0	33
+30 mins.	16	12	6	0	34	7	68	3	0	78	1	2	0	0	3	2	28	4	0	34
+45 mins.	14	16	5	0	35	2	61	3	0	66	3	5	2	0	10	2	28	2	0	32
Total Volume	68	58	16	0	142	19	259	10	0	288	5	21	6	0	32	6	107	12	0	125
% App. Total	47.9	40.8	11.3	0		6.6	89.9	3.5	0		15.6	65.6	18.8	0		4.8	85.6	9.6	0	
PHF	.850	.906	.667	.000	.910	.679	.952	.625	.000	.923	.417	.656	.375	.000	.727	.750	.922	.750	.000	.919



# L2 Data Collection

L2DataCollection.com  
Idaho (208) 860-7554 Utah (801) 431-2993

Study: WHPA0002  
Intersection: Locust Grove / Columbia Rd  
City, State: Ada County, Idaho  
Control: All Stop

File Name : Locust Grove Rd & Columbia Rd  
Site Code : 00000000  
Start Date : 8/7/2018  
Page No : 7

Image 1



**L2 Data Collection**

L2DataCollection.com

Study: WHPA0002  
Type: Volume / Direction  
Tech: Judd / Klaren  
Count: Axle Hits /2

Idaho (208) 860-7554 Utah (801) 431-2993 Hubbard Rd b Meridian Rd & Hubbard Rd VOL  
Date Start: 07-Aug-18  
Date End: 08-Aug-18  
Hubbard Rd between Meridian Rd & Locust  
Ada County, Idaho

Start Time	07-Aug-18 Tue	WB	EB	Total
12:00 AM		*	*	*
12:15		*	*	*
12:30		*	*	*
12:45		*	*	*
01:00		*	*	*
01:15		*	*	*
01:30		*	*	*
01:45		*	*	*
02:00		*	*	*
02:15		*	*	*
02:30		*	*	*
02:45		*	*	*
03:00		0	0	0
03:15		0	0	0
03:30		0	0	0
03:45		0	0	0
04:00		0	1	1
04:15		0	0	0
04:30		0	0	0
04:45		0	0	0
05:00		0	4	4
05:15		0	0	0
05:30		1	0	1
05:45		0	0	0
06:00		0	2	2
06:15		0	3	3
06:30		1	10	11
06:45		2	9	11
07:00		2	7	9
07:15		3	10	13
07:30		6	9	15
07:45		3	8	11
08:00		2	8	10
08:15		2	0	2
08:30		12	6	18
08:45		1	1	2
09:00		4	6	10
09:15		4	10	14
09:30		6	6	12
09:45		11	2	13
10:00		8	3	11
10:15		4	14	18
10:30		6	2	8
10:45		8	6	14
11:00		7	5	12
11:15		4	6	10
11:30		6	3	9
11:45		8	4	12
<b>Total</b>		<b>111</b>	<b>145</b>	<b>256</b>
<b>Percent</b>		<b>43.4%</b>	<b>56.6%</b>	
<b>Peak</b>	-	<b>09:15</b>	<b>06:30</b>	<b>09:30</b>
<b>Vol.</b>	-	<b>29</b>	<b>36</b>	<b>54</b>
<b>P.H.F.</b>		<b>0.659</b>	<b>0.900</b>	<b>0.750</b>

### L2 Data Collection

L2DataCollection.com

Study: WHPA0002  
Type: Volume / Direction  
Tech: Judd / Klaren  
Count: Axle Hits /2

Idaho (208) 860-7554 Utah (801) 431-2993 Hubbard Rd b Meridian Rd & Hubbard Rd VOL  
Date Start: 07-Aug-18  
Date End: 08-Aug-18  
Hubbard Rd between Meridian Rd & Locust  
Ada County, Idaho

Start Time	07-Aug-18 Tue	WB	EB	Total
12:00 PM		10	3	13
12:15		2	4	6
12:30		6	5	11
12:45		6	3	9
01:00		6	6	12
01:15		5	10	15
01:30		3	8	11
01:45		10	4	14
02:00		7	6	13
02:15		9	3	12
02:30		6	3	9
02:45		4	1	5
03:00		12	10	22
03:15		3	1	4
03:30		7	7	14
03:45		8	3	11
04:00		5	2	7
04:15		5	6	11
04:30		10	2	12
04:45		16	2	18
05:00		3	2	5
05:15		5	4	9
05:30		10	1	11
05:45		6	8	14
06:00		6	1	7
06:15		4	4	8
06:30		6	3	9
06:45		4	4	8
07:00		3	4	7
07:15		2	5	7
07:30		2	4	6
07:45		6	1	7
08:00		1	2	3
08:15		4	3	7
08:30		1	2	3
08:45		1	1	2
09:00		0	2	2
09:15		2	1	3
09:30		4	0	4
09:45		1	1	2
10:00		3	2	5
10:15		0	0	0
10:30		1	1	2
10:45		0	0	0
11:00		2	0	2
11:15		1	1	2
11:30		1	1	2
11:45		0	0	0
<b>Total</b>		<b>219</b>	<b>147</b>	<b>366</b>
<b>Percent</b>		<b>59.8%</b>	<b>40.2%</b>	
<b>Peak</b>	-	<b>16:00</b>	<b>13:00</b>	<b>13:15</b>
<b>Vol.</b>	-	<b>36</b>	<b>28</b>	<b>53</b>
<b>P.H.F.</b>		<b>0.563</b>	<b>0.700</b>	<b>0.883</b>

**L2 Data Collection**

L2DataCollection.com

Study: WHPA0002  
Type: Volume / Direction  
Tech: Judd / Klaren  
Count: Axle Hits /2

Idaho (208) 860-7554 Utah (801) 431-2993 Hubbard Rd b Meridian Rd & Hubbard Rd VOL  
Date Start: 07-Aug-18  
Date End: 08-Aug-18  
Hubbard Rd between Meridian Rd & Locust  
Ada County, Idaho

Start Time	08-Aug-18 Wed	WB	EB							Total
12:00 AM		0	0							0
12:15		1	0							1
12:30		1	0							1
12:45		0	1							1
01:00		0	0							0
01:15		0	1							1
01:30		0	0							0
01:45		0	1							1
02:00		1	0							1
02:15		0	0							0
02:30		0	0							0
02:45		0	0							0
03:00		*	*							*
03:15		*	*							*
03:30		*	*							*
03:45		*	*							*
04:00		*	*							*
04:15		*	*							*
04:30		*	*							*
04:45		*	*							*
05:00		*	*							*
05:15		*	*							*
05:30		*	*							*
05:45		*	*							*
06:00		*	*							*
06:15		*	*							*
06:30		*	*							*
06:45		*	*							*
07:00		*	*							*
07:15		*	*							*
07:30		*	*							*
07:45		*	*							*
08:00		*	*							*
08:15		*	*							*
08:30		*	*							*
08:45		*	*							*
09:00		*	*							*
09:15		*	*							*
09:30		*	*							*
09:45		*	*							*
10:00		*	*							*
10:15		*	*							*
10:30		*	*							*
10:45		*	*							*
11:00		*	*							*
11:15		*	*							*
11:30		*	*							*
11:45		*	*							*
Total		3	3							6
Percent		50.0%	50.0%							
Peak		-	12:00	00:30	-	-	-	-	-	12:00
Vol.		-	2	2	-	-	-	-	-	3
P.H.F.		0.500	0.500							0.750
Total		333	295							628
Percent		53.0%	47.0%							

### L2 Data Collection

L2DataCollection.com

Study: WHPA0002  
Type: Volume / Direction  
Tech: Judd / Klaren  
Count: Axle Hits / 2

Idaho (208) 860-7554 Utah (801) 431-2993  
Locust Grove Rd between Columbia & Hubbard VOL  
Date Start: 07-Aug-18  
Date End: 08-Aug-18  
Locust Grove between Columbia & Hubbard  
Ada County, Idaho

Start Time	07-Aug-18 Tue	SB	NB	Total
12:00 AM		*	*	*
12:15		*	*	*
12:30		*	*	*
12:45		*	*	*
01:00		*	*	*
01:15		*	*	*
01:30		*	*	*
01:45		*	*	*
02:00		*	*	*
02:15		*	*	*
02:30		*	*	*
02:45		*	*	*
03:00		0	0	0
03:15		1	0	1
03:30		0	0	0
03:45		0	0	0
04:00		0	1	1
04:15		0	0	0
04:30		0	0	0
04:45		0	0	0
05:00		0	3	3
05:15		2	0	2
05:30		1	5	6
05:45		0	4	4
06:00		0	1	1
06:15		2	0	2
06:30		0	9	9
06:45		6	15	21
07:00		1	13	14
07:15		4	19	23
07:30		12	12	24
07:45		10	8	18
08:00		6	12	18
08:15		6	16	22
08:30		8	9	17
08:45		4	6	10
09:00		5	6	11
09:15		5	9	14
09:30		7	8	15
09:45		9	6	15
10:00		10	4	14
10:15		4	10	14
10:30		10	8	18
10:45		7	9	16
11:00		3	3	6
11:15		6	9	15
11:30		6	6	12
11:45		10	8	18
<b>Total</b>		<b>145</b>	<b>219</b>	<b>364</b>
<b>Percent</b>		<b>39.8%</b>	<b>60.2%</b>	
Peak	-	07:30	06:45	07:15
Vol.	-	34	59	83
P.H.F.		0.708	0.776	0.865

### L2 Data Collection

L2DataCollection.com

Study: WHPA0002  
Type: Volume / Direction  
Tech: Judd / Klaren  
Count: Axle Hits / 2

Idaho (208) 860-7554 Utah (801) 431-2993  
Locust Grove Rd between Columbia & Hubbard VOL  
Date Start: 07-Aug-18  
Date End: 08-Aug-18  
Locust Grove between Columbia & Hubbard  
Ada County, Idaho

Start Time	07-Aug-18 Tue	SB	NB	Total
12:00 PM		5	5	10
12:15		10	4	14
12:30		5	8	13
12:45		8	6	14
01:00		11	4	15
01:15		6	4	10
01:30		5	5	10
01:45		8	2	10
02:00		13	6	19
02:15		8	5	13
02:30		14	4	18
02:45		9	4	13
03:00		12	10	22
03:15		7	6	13
03:30		12	10	22
03:45		14	8	22
04:00		21	2	23
04:15		16	14	30
04:30		16	12	28
04:45		20	5	25
05:00		18	12	30
05:15		17	4	21
05:30		19	5	24
05:45		19	4	23
06:00		15	8	23
06:15		9	4	13
06:30		14	4	18
06:45		7	6	13
07:00		6	5	11
07:15		10	4	14
07:30		9	5	14
07:45		4	2	6
08:00		1	3	4
08:15		6	0	6
08:30		6	2	8
08:45		5	3	8
09:00		8	3	11
09:15		8	2	10
09:30		4	1	5
09:45		6	1	7
10:00		4	5	9
10:15		3	0	3
10:30		0	2	2
10:45		4	0	4
11:00		2	0	2
11:15		1	0	1
11:30		2	0	2
11:45		0	1	1
<b>Total</b>		<b>427</b>	<b>210</b>	<b>637</b>
<b>Percent</b>		<b>67.0%</b>	<b>33.0%</b>	
<b>Peak</b>	-	<b>16:45</b>	<b>16:15</b>	<b>16:15</b>
<b>Vol.</b>	-	<b>74</b>	<b>43</b>	<b>113</b>
<b>P.H.F.</b>		<b>0.925</b>	<b>0.768</b>	<b>0.942</b>

### L2 Data Collection

L2DataCollection.com

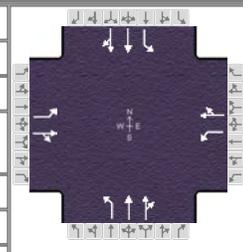
Study: WHPA0002  
Type: Volume / Direction  
Tech: Judd / Klaren  
Count: Axle Hits / 2

Idaho (208) 860-7554 Utah (801) 431-2993  
Locust Grove Rd between Columbia & Hubbard VOL  
Date Start: 07-Aug-18  
Date End: 08-Aug-18  
Locust Grove between Columbia & Hubbard  
Ada County, Idaho

Start Time	08-Aug-18 Wed	SB	NB							Total
12:00 AM		1	0							1
12:15		2	0							2
12:30		1	0							1
12:45		0	0							0
01:00		2	0							2
01:15		0	0							0
01:30		0	1							1
01:45		0	0							0
02:00		0	0							0
02:15		1	0							1
02:30		0	0							0
02:45		0	0							0
03:00		*	*							*
03:15		*	*							*
03:30		*	*							*
03:45		*	*							*
04:00		*	*							*
04:15		*	*							*
04:30		*	*							*
04:45		*	*							*
05:00		*	*							*
05:15		*	*							*
05:30		*	*							*
05:45		*	*							*
06:00		*	*							*
06:15		*	*							*
06:30		*	*							*
06:45		*	*							*
07:00		*	*							*
07:15		*	*							*
07:30		*	*							*
07:45		*	*							*
08:00		*	*							*
08:15		*	*							*
08:30		*	*							*
08:45		*	*							*
09:00		*	*							*
09:15		*	*							*
09:30		*	*							*
09:45		*	*							*
10:00		*	*							*
10:15		*	*							*
10:30		*	*							*
10:45		*	*							*
11:00		*	*							*
11:15		*	*							*
11:30		*	*							*
11:45		*	*							*
Total		7	1							8
Percent		87.5%	12.5%							
Peak	-	00:15	00:45	-	-	-	-	-	-	00:15
Vol.	-	5	1	-	-	-	-	-	-	5
P.H.F.		0.625	0.250							0.625
Total		579	430							1009
Percent		57.4%	42.6%							

# HCS7 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	WH Pacific			Duration, h	0.25		
Analyst	M Olson	Analysis Date	9/19/2018	Area Type	Other		
Jurisdiction	ADA County	Time Period	AM Weekday Peak	PHF	0.90		
Urban Street	Meridian Rd (SR 69)	Analysis Year	2018	Analysis Period	1 > 7:00		
Intersection	Meridian at Hubbard	File Name	A-Meridian_Hubb AM 2018 Existing.xus				
Project Description	A - 2018 AM Existing						



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand ( v ), veh/h	133	23	14	7	4	10	9	874	4	14	324	42

Signal Information				Signal Timing (s)													
Cycle, s	100.0	Reference Phase	2	Green	1.5	0.6	66.8	1.2	5.0	5.0	Yellow	4.0	0.0	4.0	4.0	4.0	4.0
Offset, s	0	Reference Point	End	Red	0.0	0.0	0.0	0.0	0.0	0.0	Force Mode	Fixed	Simult. Gap N/S	On			

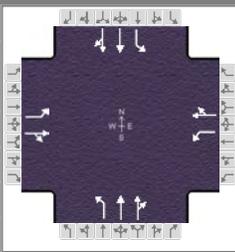
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	7	4	3	8	5	2	1	6
Case Number	2.0	4.0	2.0	4.0	2.0	4.0	2.0	4.0
Phase Duration, s	14.1	18.0	5.2	9.0	5.5	70.8	6.1	71.4
Change Period, ( Y+R <sub>c</sub> ), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Max Allow Headway ( MAH ), s	3.1	3.2	3.1	3.2	2.9	0.0	2.9	0.0
Queue Clearance Time ( g <sub>s</sub> ), s	10.2	4.1	2.4	2.9	2.6		2.9	
Green Extension Time ( g <sub>e</sub> ), s	0.1	0.1	0.0	0.1	0.0	0.0	0.0	0.0
Phase Call Probability	0.98	1.00	0.19	0.83	0.24		0.35	
Max Out Probability	0.05	0.00	0.00	0.00	0.00		0.00	

Movement Group Results	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	7	4	14	3	8	18	5	2	12	1	6	16
Adjusted Flow Rate ( v ), veh/h	148	41		8	16		10	488	487	16	206	201
Adjusted Saturation Flow Rate ( s ), veh/h/ln	1767	1737		1767	1644		1682	1767	1764	1711	1796	1724
Queue Service Time ( g <sub>s</sub> ), s	8.2	2.1		0.4	0.9		0.6	12.7	12.7	0.9	4.2	4.3
Cycle Queue Clearance Time ( g <sub>c</sub> ), s	8.2	2.1		0.4	0.9		0.6	12.7	12.7	0.9	4.2	4.3
Green Ratio ( g/C )	0.10	0.14		0.01	0.05		0.01	0.67	0.67	0.02	0.67	0.67
Capacity ( c ), veh/h	179	243		21	82		24	1179	1177	36	1211	1162
Volume-to-Capacity Ratio ( X )	0.824	0.169		0.377	0.189		0.408	0.414	0.414	0.432	0.170	0.173
Back of Queue ( Q ), ft/ln ( 50 th percentile)	97	22.6		5.5	9.6		7	104.3	104.1	10.4	33.7	32.9
Back of Queue ( Q ), veh/ln ( 50 th percentile)	3.8	0.9		0.2	0.4		0.3	3.9	3.9	0.4	1.3	1.2
Queue Storage Ratio ( RQ ) ( 50 th percentile)	1.62	0.00		0.06	0.00		0.05	0.00	0.00	0.06	0.00	0.00
Uniform Delay ( d <sub>1</sub> ), s/veh	44.1	37.9		49.1	45.6		48.8	7.6	7.6	48.4	6.0	6.0
Incremental Delay ( d <sub>2</sub> ), s/veh	5.4	0.1		4.2	0.4		4.0	1.1	1.1	3.0	0.3	0.3
Initial Queue Delay ( d <sub>3</sub> ), s/veh	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Control Delay ( d ), s/veh	49.4	38.0		53.2	46.0		52.9	8.7	8.7	51.4	6.3	6.3
Level of Service ( LOS )	D	D		D	D		D	A	A	D	A	A
Approach Delay, s/veh / LOS	46.9		D	48.4		D	9.2		A	8.0		A
Intersection Delay, s/veh / LOS	13.8						B					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	2.30	B	2.31	B	1.87	B	1.86	B
Bicycle LOS Score / LOS	0.80	A	0.53	A	1.30	A	0.84	A

# HCS7 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	WH Pacific			Duration, h	0.25		
Analyst	M Olson	Analysis Date	9/19/2018	Area Type	Other		
Jurisdiction	ADA County	Time Period	PM Peak	PHF	0.90		
Urban Street	Meridian Rd (SR 69)	Analysis Year	2018	Analysis Period	1 > 7:00		
Intersection	Meridian at Hubbard	File Name	B-Meridian_Hubb PM 2018 Existing.xus				
Project Description	B-2018 PM Existing						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	67	9	28	9	9	29	29	485	8	15	1047	273

Signal Information				Signal Timing (s)																				
Cycle, s	100.0	Reference Phase	2	Green	2.2	1.3	69.5	1.5	4.0	5.6	Yellow	4.0	0.0	4.0	4.0	0.0	4.0	Red	0.0	0.0	0.0	0.0	0.0	0.0
Offset, s	0	Reference Point	End																					
Uncoordinated	No	Simult. Gap E/W	On																					
Force Mode	Fixed	Simult. Gap N/S	On																					

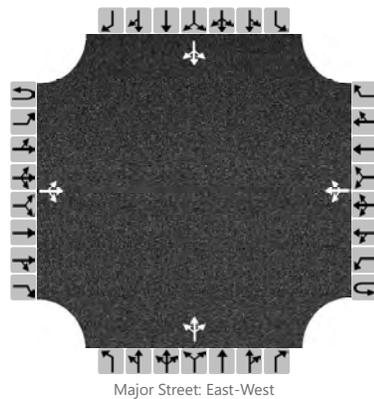
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	7	4	3	8	5	2	1	6
Case Number	2.0	4.0	2.0	4.0	2.0	4.0	2.0	4.0
Phase Duration, s	9.4	13.5	5.5	9.6	7.5	74.8	6.2	73.5
Change Period, (Y+R <sub>c</sub> ), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Max Allow Headway (MAH), s	3.1	3.3	3.1	3.3	2.9	0.0	2.9	0.0
Queue Clearance Time (g <sub>s</sub> ), s	6.2	4.3	2.6	4.5	3.9		3.0	
Green Extension Time (g <sub>e</sub> ), s	0.1	0.2	0.0	0.1	0.0	0.0	0.0	0.0
Phase Call Probability	0.87	0.99	0.24	0.93	0.59		0.37	
Max Out Probability	0.00	0.00	0.00	0.00	0.00		0.00	

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	7	4	14	3	8	18	5	2	12	1	6	16
Adjusted Flow Rate (v), veh/h	74	41		10	42		32	275	273	17	754	713
Adjusted Saturation Flow Rate (s), veh/h/ln	1767	1633		1767	1631		1682	1767	1756	1711	1796	1669
Queue Service Time (g <sub>s</sub> ), s	4.2	2.3		0.6	2.5		1.9	5.4	5.4	1.0	22.1	22.8
Cycle Queue Clearance Time (g <sub>c</sub> ), s	4.2	2.3		0.6	2.5		1.9	5.4	5.4	1.0	22.1	22.8
Green Ratio (g/C)	0.05	0.10		0.01	0.06		0.04	0.71	0.71	0.02	0.69	0.69
Capacity (c), veh/h	96	156		26	91		60	1251	1243	38	1248	1159
Volume-to-Capacity Ratio (X)	0.775	0.264		0.389	0.466		0.540	0.220	0.220	0.438	0.604	0.615
Back of Queue (Q), ft/ln (50th percentile)	49.4	24.1		6.9	26.6		21.3	39.5	39.6	11.1	174.1	168.3
Back of Queue (Q), veh/ln (50th percentile)	1.9	0.9		0.3	1.0		0.8	1.5	1.5	0.4	6.6	6.4
Queue Storage Ratio (RQ) (50th percentile)	0.82	0.00		0.08	0.00		0.14	0.00	0.00	0.07	0.00	0.00
Uniform Delay (d <sub>1</sub> ), s/veh	46.7	42.0		48.8	45.8		47.4	5.1	5.1	48.3	8.0	8.1
Incremental Delay (d <sub>2</sub> ), s/veh	4.9	0.3		3.5	1.4		2.8	0.4	0.4	2.9	2.2	2.4
Initial Queue Delay (d <sub>3</sub> ), s/veh	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (d), s/veh	51.6	42.3		52.4	47.2		50.2	5.5	5.5	51.2	10.2	10.6
Level of Service (LOS)	D	D		D	D		D	A	A	D	B	B
Approach Delay, s/veh / LOS	48.3		D	48.2		D	7.9		A	10.9		B
Intersection Delay, s/veh / LOS	12.9						B					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	2.31	B	2.31	B	1.86	B	1.86	B
Bicycle LOS Score / LOS	0.68	A	0.57	A	0.97	A	1.71	B

General Information		Site Information	
Analyst	Jerry Liu	Intersection	Locust Grove and Hubbard
Agency/Co.	WHPacific	Jurisdiction	
Date Performed	9/15/2018	East/West Street	Hubbard R
Analysis Year	2018	North/South Street	Locust Grove
Time Analyzed	2018 Existing AM	Peak Hour Factor	0.90
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	Trilogy		

**Lanes**



**Vehicle Volumes and Adjustments**

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Priority																
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	1	0
Configuration			LTR				LTR				LTR				LTR	
Volume, V (veh/h)		14	17	4		0	9	2		0	38	1		1	11	5
Percent Heavy Vehicles (%)		3				3				3	3	3		3	3	3
Proportion Time Blocked																
Percent Grade (%)									0				0			
Right Turn Channelized	No				No				No				No			
Median Type/Storage	Undivided															

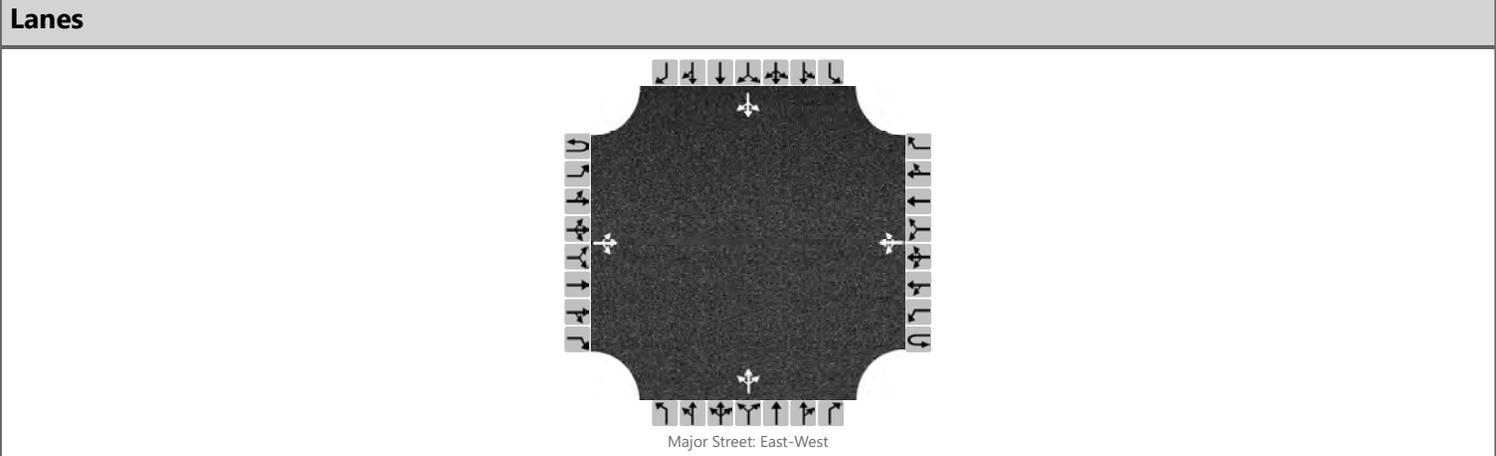
**Critical and Follow-up Headways**

Base Critical Headway (sec)																
Critical Headway (sec)																
Base Follow-Up Headway (sec)																
Follow-Up Headway (sec)																

**Delay, Queue Length, and Level of Service**

Flow Rate, v (veh/h)		16			0					43					19		
Capacity, c (veh/h)		1598			1584					819					882		
v/c Ratio		0.01			0.00					0.05					0.02		
95% Queue Length, Q <sub>95</sub> (veh)		0.0			0.0					0.2					0.1		
Control Delay (s/veh)		7.3			7.3					9.6					9.2		
Level of Service, LOS		A			A					A					A		
Approach Delay (s/veh)		3.0				0.0				9.6				9.2			
Approach LOS										A				A			

General Information				Site Information			
Analyst	Jerry Liu			Intersection	Locust Grove and Hubbard		
Agency/Co.	WHPacific			Jurisdiction			
Date Performed	9/15/2018			East/West Street	Hubbard R		
Analysis Year	2018			North/South Street	Locust Grove		
Time Analyzed	2018 Existing PM			Peak Hour Factor	0.90		
Intersection Orientation	East-West			Analysis Time Period (hrs)	0.25		
Project Description	Trilogy						



**Vehicle Volumes and Adjustments**

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Priority																
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	1	0
Configuration			LTR				LTR				LTR				LTR	
Volume, V (veh/h)		2	9	4		8	11	0		2	16	3		0	64	7
Percent Heavy Vehicles (%)		3				3				3	3	3		3	3	3
Proportion Time Blocked																
Percent Grade (%)									0				0			
Right Turn Channelized	No				No				No				No			
Median Type/Storage	Undivided															

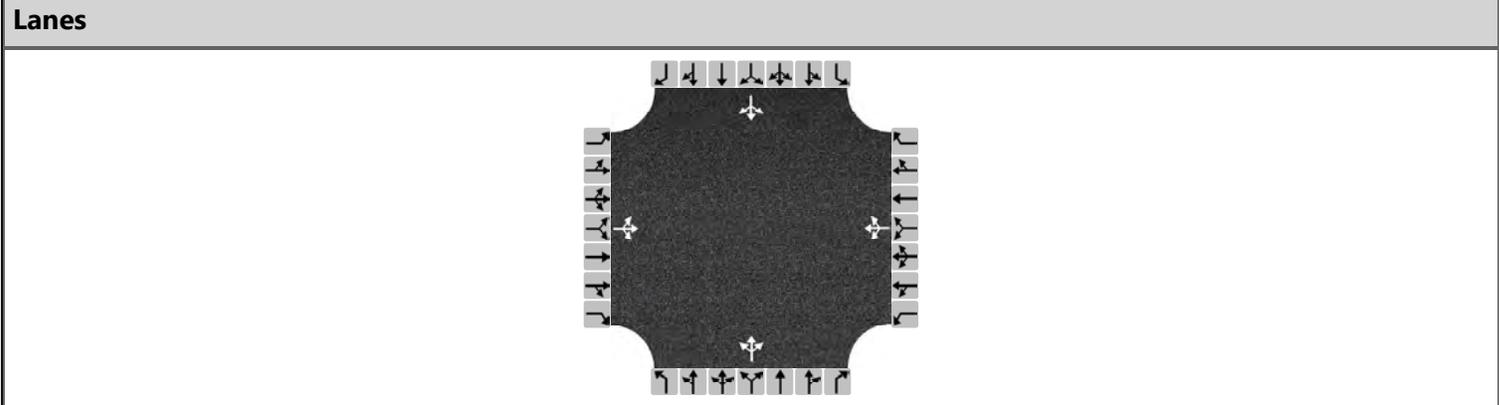
**Critical and Follow-up Headways**

Base Critical Headway (sec)																
Critical Headway (sec)																
Base Follow-Up Headway (sec)																
Follow-Up Headway (sec)																

**Delay, Queue Length, and Level of Service**

Flow Rate, v (veh/h)		2			9					23					79		
Capacity, c (veh/h)		1598			1596					861					854		
v/c Ratio		0.00			0.01					0.03					0.09		
95% Queue Length, Q <sub>95</sub> (veh)		0.0			0.0					0.1					0.3		
Control Delay (s/veh)		7.3			7.3					9.3					9.6		
Level of Service, LOS		A			A					A					A		
Approach Delay (s/veh)		0.9				3.1				9.3				9.6			
Approach LOS										A				A			

General Information		Site Information	
Analyst	Jerry Liu	Intersection	Locust Grove and Columbia
Agency/Co.	WHPacific	Jurisdiction	
Date Performed	9/15/2018	East/West Street	Columbia Rd
Analysis Year	2018	North/South Street	Locust Grove Rd
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.90
Time Analyzed	Existing AM		
Project Description	Trilogy		



**Vehicle Volume and Adjustments**

Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Movement												
Volume	61	211	4	4	71	15	4	41	7	18	16	16
% Thrus in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	LTR			LTR			LTR			LTR		
Flow Rate, v (veh/h)	307			100			58			56		
Percent Heavy Vehicles	3			3			3			3		

**Departure Headway and Service Time**

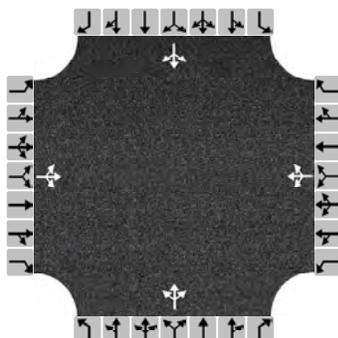
Initial Departure Headway, hd (s)	3.20			3.20			3.20			3.20		
Initial Degree of Utilization, x	0.273			0.089			0.051			0.049		
Final Departure Headway, hd (s)	4.39			4.49			4.90			4.85		
Final Degree of Utilization, x	0.374			0.125			0.079			0.075		
Move-Up Time, m (s)	2.0			2.0			2.0			2.0		
Service Time, ts (s)	2.39			2.49			2.90			2.85		

**Capacity, Delay and Level of Service**

Flow Rate, v (veh/h)	307			100			58			56		
Capacity	820			803			735			743		
95% Queue Length, Q <sub>95</sub> (veh)	1.7			0.4			0.3			0.2		
Control Delay (s/veh)	10.0			8.1			8.3			8.2		
Level of Service, LOS	A			A			A			A		
Approach Delay (s/veh)	10.0			8.1			8.3			8.2		
Approach LOS	A			A			A			A		
Intersection Delay, s/veh   LOS	9.3						A					

General Information		Site Information	
Analyst	Jerry Liu	Intersection	Locust Grove and Columbia
Agency/Co.	WHPacific	Jurisdiction	
Date Performed	9/15/2018	East/West Street	Columbia Rd
Analysis Year	2018	North/South Street	Locust Grove Rd
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.90
Time Analyzed	Existing PM		
Project Description	Trilogy		

## Lanes



## Vehicle Volume and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Movement												
Volume	15	93	4	10	259	19	2	11	6	16	58	68
% Thrus in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	LTR			LTR			LTR			LTR		
Flow Rate, v (veh/h)	124			320			21			158		
Percent Heavy Vehicles	3			3			3			3		

## Departure Headway and Service Time

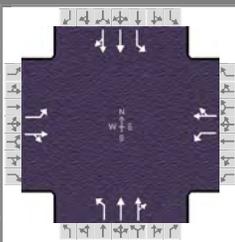
Initial Departure Headway, hd (s)	3.20			3.20			3.20			3.20		
Initial Degree of Utilization, x	0.111			0.284			0.019			0.140		
Final Departure Headway, hd (s)	4.78			4.53			5.07			4.77		
Final Degree of Utilization, x	0.165			0.402			0.030			0.209		
Move-Up Time, m (s)	2.0			2.0			2.0			2.0		
Service Time, ts (s)	2.78			2.53			3.07			2.77		

## Capacity, Delay and Level of Service

Flow Rate, v (veh/h)	124			320			21			158		
Capacity	752			795			711			755		
95% Queue Length, Q <sub>95</sub> (veh)	0.6			2.0			0.1			0.8		
Control Delay (s/veh)	8.7			10.5			8.2			9.0		
Level of Service, LOS	A			B			A			A		
Approach Delay (s/veh)	8.7			10.5			8.2			9.0		
Approach LOS	A			B			A			A		
Intersection Delay, s/veh   LOS	9.7						A					

# HCS7 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	WH Pacific			Duration, h	0.25		
Analyst	M Olson	Analysis Date	Sep 19, 2018	Area Type	Other		
Jurisdiction	ADA County	Time Period	AM	PHF	0.90		
Urban Street	Meridian Rd (SR 69)	Analysis Year	2025	Analysis Period	1 > 7:00		
Intersection	Meridian at Hubbard	File Name	C-Meridian_Hubb AM 2025 No-Build.xus				
Project Description	C-2025 AM - No Build						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	280	140	30	40	25	60	15	1530	25	85	360	45

Signal Information				Phase Diagrams										
Cycle, s	100.0	Reference Phase	2											
Offset, s	0	Reference Point	End											
Uncoordinated	No	Simult. Gap E/W	On											
Force Mode	Fixed	Simult. Gap N/S	On											
		Green	2.2	43.0	7.0	4.3	11.4	8.1						
		Yellow	4.0	4.0	4.0	4.0	4.0	4.0						
		Red	0.0	0.0	0.0	0.0	0.0	0.0						

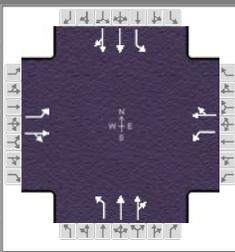
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	7	4	3	8	5	2	1	6
Case Number	2.0	4.0	2.0	4.0	2.0	4.0	2.0	4.0
Phase Duration, s	23.7	27.5	8.3	12.1	6.2	53.3	11.0	58.0
Change Period, (Y+R <sub>c</sub> ), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Max Allow Headway (MAH), s	3.0	3.0	3.0	3.0	2.9	0.0	2.9	0.0
Queue Clearance Time (g <sub>s</sub> ), s	19.2	11.0	4.5	7.6	3.0		7.4	
Green Extension Time (g <sub>e</sub> ), s	0.5	0.5	0.1	0.5	0.0	0.0	0.1	0.0
Phase Call Probability	1.00	1.00	0.71	1.00	0.37		0.93	
Max Out Probability	0.00	0.00	0.00	0.00	0.00		1.00	

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	7	4	14	3	8	18	5	2	12	1	6	16
Adjusted Flow Rate (v), veh/h	311	189		44	94		17	865	863	94	228	222
Adjusted Saturation Flow Rate (s), veh/h/ln	1767	1798		1767	1646		1682	1767	1756	1711	1796	1726
Queue Service Time (g <sub>s</sub> ), s	17.2	9.0		2.5	5.6		1.0	48.7	49.0	5.4	6.7	6.8
Cycle Queue Clearance Time (g <sub>c</sub> ), s	17.2	9.0		2.5	5.6		1.0	48.7	49.0	5.4	6.7	6.8
Green Ratio (g/C)	0.20	0.23		0.04	0.08		0.02	0.49	0.49	0.07	0.54	0.54
Capacity (c), veh/h	348	423		75	133		37	870	865	119	970	933
Volume-to-Capacity Ratio (X)	0.895	0.447		0.591	0.710		0.445	0.994	0.997	0.792	0.235	0.238
Back of Queue (Q), ft/ln (50th percentile)	189.4	96.5		28.4	58.9		11.3	647	651.9	75.3	64.5	63
Back of Queue (Q), veh/ln (50th percentile)	7.4	3.8		1.1	2.3		0.4	24.1	24.3	2.9	2.4	2.4
Queue Storage Ratio (RQ) (50th percentile)	3.16	0.00		0.32	0.00		0.08	0.00	0.00	0.46	0.00	0.00
Uniform Delay (d <sub>1</sub> ), s/veh	39.2	32.7		47.0	44.8		48.3	25.2	25.3	45.8	12.1	12.1
Incremental Delay (d <sub>2</sub> ), s/veh	3.3	0.3		2.7	2.6		3.1	29.0	29.9	20.3	0.6	0.6
Initial Queue Delay (d <sub>3</sub> ), s/veh	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (d), s/veh	42.5	33.0		49.7	47.4		51.3	54.2	55.2	66.1	12.7	12.7
Level of Service (LOS)	D	C		D	D		D	D	E	E	B	B
Approach Delay, s/veh / LOS	38.9		D	48.2		D	54.7		D	22.0		C
Intersection Delay, s/veh / LOS	45.6						D					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	2.29	B	2.31	B	1.95	B	1.89	B
Bicycle LOS Score / LOS	1.31	A	0.72	A	1.93	B	0.94	A

# HCS7 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	WH Pacific			Duration, h	0.25		
Analyst	M Olson	Analysis Date	Sep 19, 2018	Area Type	Other		
Jurisdiction	ADA County	Time Period	PM	PHF	0.90		
Urban Street	Meridian Rd (SR 69)	Analysis Year	2025	Analysis Period	1 > 7:00		
Intersection	Meridian at Hubbard	File Name	D-Meridian_Hubb PM 2025 No-Build.xus				
Project Description	2025 PM - No Build						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	140	50	55	50	60	170	50	850	50	50	1170	305

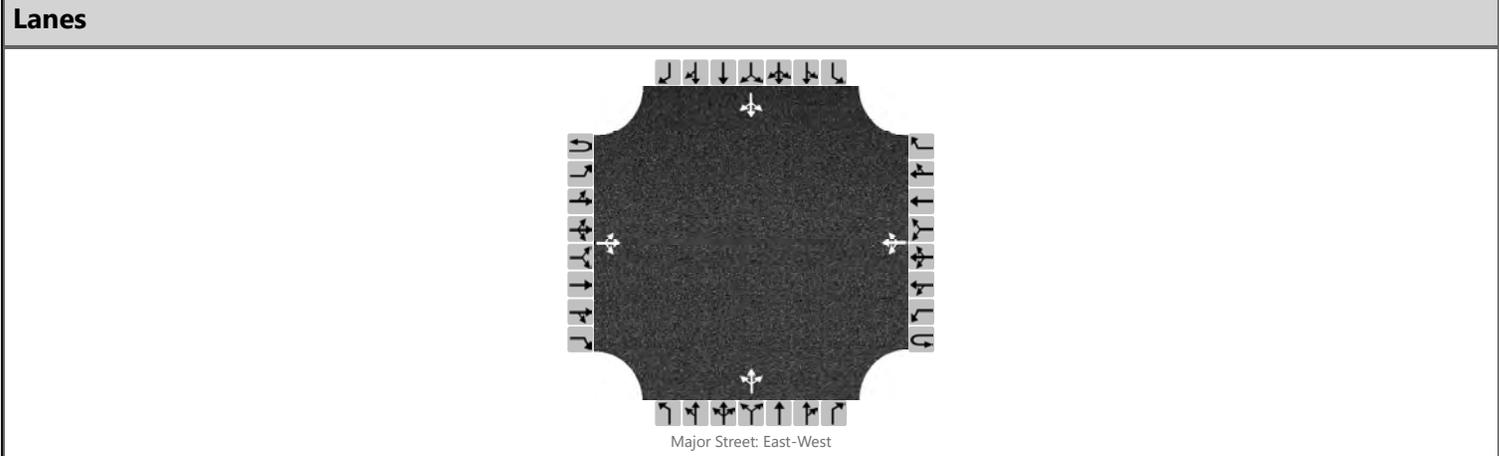
Signal Information				Phase Diagrams										
Cycle, s	125.0	Reference Phase	2											
Offset, s	0	Reference Point	End											
Uncoordinated	No	Simult. Gap E/W	On											
Force Mode	Fixed	Simult. Gap N/S	On											
		Green	5.2	69.0	5.1	3.9	21.8	0.0						
		Yellow	4.0	4.0	4.0	4.0	4.0	0.0						
		Red	0.0	0.0	0.0	0.0	0.0	0.0						

Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	7	4	3	8	5	2	1	6
Case Number	2.0	4.0	2.0	4.0	2.0	4.0	2.0	4.0
Phase Duration, s	17.0	33.7	9.1	25.8	9.3	73.0	9.2	72.9
Change Period, (Y+Rc), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Max Allow Headway (MAH), s	3.0	3.2	3.0	3.2	2.9	0.0	2.9	0.0
Queue Clearance Time (gs), s	12.8	9.0	5.9	21.1	6.1		6.0	
Green Extension Time (ge), s	0.3	0.7	0.1	0.7	0.1	0.0	0.1	0.0
Phase Call Probability	1.00	1.00	0.85	1.00	0.85		0.85	
Max Out Probability	0.00	0.00	0.00	0.00	0.00		0.00	

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	7	4	14	3	8	18	5	2	12	1	6	16
Adjusted Flow Rate (v), veh/h	156	117		56	256		56	505	495	56	836	803
Adjusted Saturation Flow Rate (s), veh/h/ln	1767	1696		1767	1638		1682	1767	1732	1711	1796	1669
Queue Service Time (gs), s	10.8	7.0		3.9	19.1		4.1	22.4	22.4	4.0	48.8	52.0
Cycle Queue Clearance Time (gc), s	10.8	7.0		3.9	19.1		4.1	22.4	22.4	4.0	48.8	52.0
Green Ratio (g/C)	0.10	0.24		0.04	0.17		0.04	0.55	0.55	0.04	0.55	0.55
Capacity (c), veh/h	184	403		73	286		71	975	956	71	990	921
Volume-to-Capacity Ratio (X)	0.846	0.290		0.766	0.895		0.783	0.518	0.518	0.778	0.844	0.873
Back of Queue (Q), ft/ln (50th percentile)	125.4	74		46.8	202.9		48.6	233.4	229.1	47.8	540.2	548.6
Back of Queue (Q), veh/ln (50th percentile)	4.9	2.9		1.8	7.9		1.8	8.7	8.5	1.8	20.5	20.8
Queue Storage Ratio (RQ) (50th percentile)	2.09	0.00		0.53	0.00		0.32	0.00	0.00	0.29	0.00	0.00
Uniform Delay (d1), s/veh	55.0	39.0		59.3	50.5		59.3	17.6	17.6	59.3	23.5	24.2
Incremental Delay (d2), s/veh	4.1	0.1		6.2	4.0		6.9	2.0	2.0	6.7	8.7	11.2
Initial Queue Delay (d3), s/veh	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Control Delay (d), s/veh	59.1	39.2		65.5	54.5		66.2	19.5	19.6	66.0	32.3	35.4
Level of Service (LOS)	E	D		E	D		E	B	B	E	C	D
Approach Delay, s/veh / LOS	50.5		D	56.4		E	22.0		C	34.9		C
Intersection Delay, s/veh / LOS	34.1						C					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	2.30	B	2.31	B	1.90	B	1.90	B
Bicycle LOS Score / LOS	0.94	A	1.00	A	1.36	A	1.89	B

General Information				Site Information			
Analyst	Jerry Liu			Intersection	Locust Grove and Hubbard		
Agency/Co.	WHPacific			Jurisdiction			
Date Performed	9/15/2018			East/West Street	Hubbard R		
Analysis Year	2025			North/South Street	Locust Grove		
Time Analyzed	2025 NoBuild AM			Peak Hour Factor	0.90		
Intersection Orientation	East-West			Analysis Time Period (hrs)	0.25		
Project Description	Trilogy						



**Vehicle Volumes and Adjustments**

Approach	Eastbound				Westbound				Northbound				Southbound				
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Movement	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12	
Priority																	
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	1	0	
Configuration			LTR				LTR				LTR				LTR		
Volume, V (veh/h)		100	125	25		10	100	10		10	290	5		5	40	15	
Percent Heavy Vehicles (%)		3				3				3	3	3		3	3	3	
Proportion Time Blocked																	
Percent Grade (%)										0				0			
Right Turn Channelized		No				No				No				No			
Median Type/Storage		Undivided															

**Critical and Follow-up Headways**

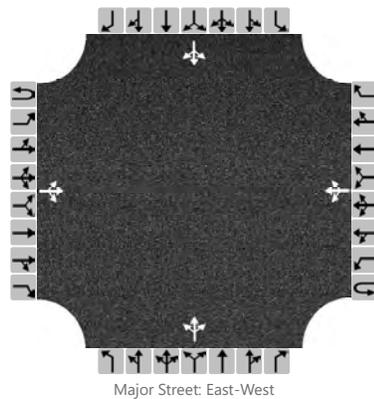
Base Critical Headway (sec)																	
Critical Headway (sec)																	
Base Follow-Up Headway (sec)																	
Follow-Up Headway (sec)																	

**Delay, Queue Length, and Level of Service**

Flow Rate, v (veh/h)		111			11					339					67		
Capacity, c (veh/h)		1457			1403					420					387		
v/c Ratio		0.08			0.01					0.81					0.17		
95% Queue Length, Q <sub>95</sub> (veh)		0.2			0.0					7.3					0.6		
Control Delay (s/veh)		7.7			7.6					40.9					16.2		
Level of Service, LOS		A			A					E					C		
Approach Delay (s/veh)		3.5				0.7				40.9				16.2			
Approach LOS										E				C			

General Information		Site Information	
Analyst	Jerry Liu	Intersection	Locust Grove and Hubbard
Agency/Co.	WHPacific	Jurisdiction	
Date Performed	9/15/2018	East/West Street	Hubbard R
Analysis Year	2025	North/South Street	Locust Grove
Time Analyzed	2025 NoBuild PM	Peak Hour Factor	0.90
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	Trilogy		

**Lanes**



**Vehicle Volumes and Adjustments**

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Priority																
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	1	0
Configuration			LTR				LTR				LTR				LTR	
Volume, V (veh/h)		25	100	25		70	240	10		20	100	10		5	235	20
Percent Heavy Vehicles (%)		3				3				3	3	3		3	3	3
Proportion Time Blocked																
Percent Grade (%)									0				0			
Right Turn Channelized	No				No				No				No			
Median Type/Storage	Undivided															

**Critical and Follow-up Headways**

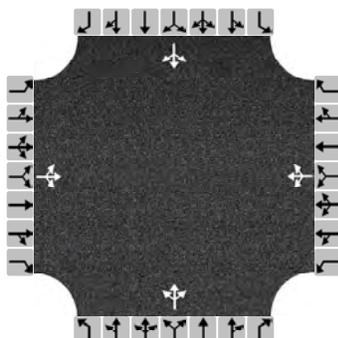
Base Critical Headway (sec)																
Critical Headway (sec)																
Base Follow-Up Headway (sec)																
Follow-Up Headway (sec)																

**Delay, Queue Length, and Level of Service**

Flow Rate, v (veh/h)		28			78						144					289
Capacity, c (veh/h)		1278			1437						295					378
v/c Ratio		0.02			0.05						0.49					0.77
95% Queue Length, Q <sub>95</sub> (veh)		0.1			0.2						2.5					6.3
Control Delay (s/veh)		7.9			7.6						28.2					39.7
Level of Service, LOS		A			A						D					E
Approach Delay (s/veh)		1.5			2.1					28.2			39.7			
Approach LOS										D			E			

General Information		Site Information	
Analyst	Jerry Liu	Intersection	Locust Grove and Columbia
Agency/Co.	WHPacific	Jurisdiction	
Date Performed	9/15/2018	East/West Street	Columbia Rd
Analysis Year	2025	North/South Street	Locust Grove Rd
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.90
Time Analyzed	2025 NoBuild AM		
Project Description	Trilogy		

## Lanes



## Vehicle Volume and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Movement												
Volume	140	510	15	15	150	30	30	335	35	45	60	40
% Thrus in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	LTR			LTR			LTR			LTR		
Flow Rate, v (veh/h)	739			217			444			161		
Percent Heavy Vehicles	3			3			3			3		

## Departure Headway and Service Time

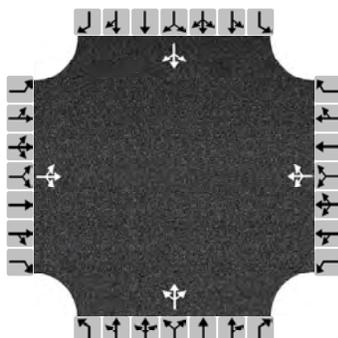
Initial Departure Headway, hd (s)	3.20			3.20			3.20			3.20		
Initial Degree of Utilization, x	0.657			0.193			0.395			0.143		
Final Departure Headway, hd (s)	6.86			7.52			6.97			7.83		
Final Degree of Utilization, x	1.408			0.453			0.861			0.350		
Move-Up Time, m (s)	2.0			2.0			2.0			2.0		
Service Time, ts (s)	4.86			5.52			4.97			5.83		

## Capacity, Delay and Level of Service

Flow Rate, v (veh/h)	739			217			444			161		
Capacity	525			479			516			460		
95% Queue Length, Q <sub>95</sub> (veh)	34.7			2.3			9.2			1.6		
Control Delay (s/veh)	214.5			16.6			39.4			15.0		
Level of Service, LOS	F			C			E			B		
Approach Delay (s/veh)	214.5			16.6			39.4			15.0		
Approach LOS	F			C			E			B		
Intersection Delay, s/veh   LOS	116.6						F					

General Information		Site Information	
Analyst	Jerry Liu	Intersection	Locust Grove and Columbia
Agency/Co.	WHPacific	Jurisdiction	
Date Performed	9/15/2018	East/West Street	Columbia Rd
Analysis Year	2025	North/South Street	Locust Grove Rd
Analysis Time Period (hrs)	0.25	Peak Hour Factor	0.90
Time Analyzed	2025 NoBuild PM		
Project Description	Trilogy		

## Lanes



## Vehicle Volume and Adjustments

Approach	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Movement												
Volume	35	225	15	35	560	40	15	90	30	40	210	170
% Thrus in Shared Lane												
Lane	L1	L2	L3	L1	L2	L3	L1	L2	L3	L1	L2	L3
Configuration	LTR			LTR			LTR			LTR		
Flow Rate, v (veh/h)	306			706			150			467		
Percent Heavy Vehicles	3			3			3			3		

## Departure Headway and Service Time

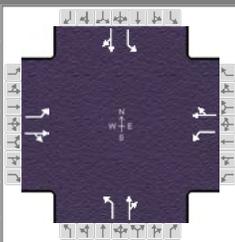
Initial Departure Headway, hd (s)	3.20			3.20			3.20			3.20		
Initial Degree of Utilization, x	0.272			0.627			0.133			0.415		
Final Departure Headway, hd (s)	7.76			7.29			8.45			7.13		
Final Degree of Utilization, x	0.659			1.429			0.352			0.924		
Move-Up Time, m (s)	2.0			2.0			2.0			2.0		
Service Time, ts (s)	5.76			5.29			6.45			5.13		

## Capacity, Delay and Level of Service

Flow Rate, v (veh/h)	306			706			150			467		
Capacity	464			494			426			505		
95% Queue Length, Q <sub>95</sub> (veh)	4.7			34.2			1.6			11.1		
Control Delay (s/veh)	24.5			225.2			16.0			50.2		
Level of Service, LOS	C			F			C			F		
Approach Delay (s/veh)	24.5			225.2			16.0			50.2		
Approach LOS	C			F			C			F		
Intersection Delay, s/veh   LOS	118.1						F					

# HCS7 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	WH Pacific			Duration, h	0.25		
Analyst	Olson	Analysis Date	9/26/2018	Area Type	Other		
Jurisdiction	ADA County	Time Period	AM	PHF	0.90		
Urban Street	E Columbia Rd	Analysis Year	2025	Analysis Period	1 > 7:00		
Intersection	Columbia and Locust Gr...	File Name	I-Columb_LocustGroV AM 2025 NoBuild.xus				
Project Description	2025 No Build AM						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand ( v ), veh/h	140	510	15	15	150	30	30	335	35	45	60	40

Signal Information				Phase Timings						Signal Diagram				
Cycle, s	60.0	Reference Phase	2	Green	1.5	28.5	18.0	0.0	0.0	0.0	1	2	3	4
Offset, s	0	Reference Point	End	Yellow	4.0	4.0	4.0	0.0	0.0	0.0	5	6	7	8
Uncoordinated	No	Simult. Gap E/W	On	Red	0.0	0.0	0.0	0.0	0.0	0.0				
Force Mode	Fixed	Simult. Gap N/S	On											

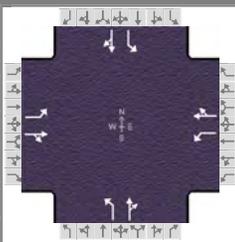
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		2	1	6		8		4
Case Number		6.3	1.0	4.0		6.0		6.0
Phase Duration, s		32.5	5.5	38.0		22.0		22.0
Change Period, ( Y+R <sub>c</sub> ), s		4.0	4.0	4.0		4.0		4.0
Max Allow Headway ( MAH ), s		0.0	3.0	0.0		3.0		3.0
Queue Clearance Time ( g <sub>s</sub> ), s			2.3			14.2		17.0
Green Extension Time ( g <sub>e</sub> ), s		0.0	0.0	0.0		1.1		1.0
Phase Call Probability			0.24			1.00		1.00
Max Out Probability			0.00			0.00		0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate ( v ), veh/h	156	583		17	200		33	411		50	111	
Adjusted Saturation Flow Rate ( s ), veh/h/ln	1173	1846		1767	1801		1272	1824		967	1731	
Queue Service Time ( g <sub>s</sub> ), s	4.8	14.6		0.3	3.3		1.2	12.2		2.9	2.9	
Cycle Queue Clearance Time ( g <sub>c</sub> ), s	5.0	14.6		0.3	3.3		3.9	12.2		15.0	2.9	
Green Ratio ( g/C )	0.48	0.48		0.53	0.57		0.30	0.30		0.30	0.30	
Capacity ( c ), veh/h	675	876		351	1019		445	549		217	521	
Volume-to-Capacity Ratio ( X )	0.231	0.666		0.047	0.196		0.075	0.748		0.230	0.213	
Back of Queue ( Q ), ft/ln ( 50 th percentile)	26.8	133.9		1.8	23		7.6	110		15.4	24.2	
Back of Queue ( Q ), veh/ln ( 50 th percentile)	1.0	5.2		0.1	0.9		0.3	4.3		0.6	0.9	
Queue Storage Ratio ( RQ ) ( 50 th percentile)	0.27	0.00		0.02	0.00		0.08	0.00		0.15	0.00	
Uniform Delay ( d <sub>1</sub> ), s/veh	9.6	12.1		9.0	6.4		17.1	18.9		25.6	15.7	
Incremental Delay ( d <sub>2</sub> ), s/veh	0.8	4.0		0.0	0.4		0.0	0.8		0.2	0.1	
Initial Queue Delay ( d <sub>3</sub> ), s/veh	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Control Delay ( d ), s/veh	10.4	16.1		9.0	6.8		17.1	19.7		25.8	15.7	
Level of Service ( LOS )	B	B		A	A		B	B		C	B	
Approach Delay, s/veh / LOS	14.9		B	7.0		A	19.5		B	18.9		B
Intersection Delay, s/veh / LOS	15.5						B					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	1.88	B	1.87	B	1.91	B	1.91	B
Bicycle LOS Score / LOS	1.71	B	0.85	A	1.22	A	0.75	A

# HCS7 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	WH Pacific			Duration, h	0.25		
Analyst	Olson	Analysis Date	9/26/2018	Area Type	Other		
Jurisdiction	ADA County	Time Period	AM	PHF	0.90		
Urban Street	E Columbia Rd	Analysis Year	2025	Analysis Period	1 > 7:00		
Intersection	Columbia and Locust Gr...	File Name	J-Columb_LocustGrove PM 2025 NoBuild.xus				
Project Description	2025 No Build PM						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand ( v ), veh/h	35	225	15	35	560	40	15	90	30	40	210	170

Signal Information				Signal Timing (s)								Signal Phases			
Cycle, s	60.0	Reference Phase	2	Green	2.9	27.3	17.8	0.0	0.0	0.0	1	2	3	4	
Offset, s	0	Reference Point	End	Yellow	4.0	4.0	4.0	0.0	0.0	0.0	5	6	7	8	
Uncoordinated	No	Simult. Gap E/W	On	Red	0.0	0.0	0.0	0.0	0.0	0.0					
Force Mode	Fixed	Simult. Gap N/S	On												

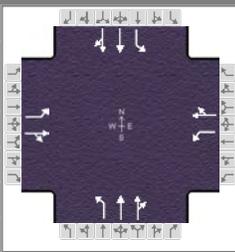
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		2	1	6		8		4
Case Number		6.3	1.0	4.0		6.0		6.0
Phase Duration, s		31.3	6.9	38.2		21.8		21.8
Change Period, ( Y+R <sub>c</sub> ), s		4.0	4.0	4.0		4.0		4.0
Max Allow Headway ( MAH ), s		0.0	3.0	0.0		3.1		3.1
Queue Clearance Time ( g <sub>s</sub> ), s			2.6			16.7		15.7
Green Extension Time ( g <sub>e</sub> ), s		0.0	0.0	0.0		1.1		1.1
Phase Call Probability			0.48			1.00		1.00
Max Out Probability			0.00			0.00		0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate ( v ), veh/h	39	267		39	667		17	133		44	422	
Adjusted Saturation Flow Rate ( s ), veh/h/ln	764	1835		1767	1834		957	1776		1246	1717	
Queue Service Time ( g <sub>s</sub> ), s	2.2	5.6		0.6	14.8		1.0	3.4		1.7	13.7	
Cycle Queue Clearance Time ( g <sub>c</sub> ), s	10.2	5.6		0.6	14.8		14.7	3.4		5.1	13.7	
Green Ratio ( g/C )	0.46	0.46		0.54	0.57		0.30	0.30		0.30	0.30	
Capacity ( c ), veh/h	365	834		603	1043		187	529		421	511	
Volume-to-Capacity Ratio ( X )	0.106	0.320		0.065	0.639		0.089	0.252		0.106	0.826	
Back of Queue ( Q ), ft/ln ( 50 th percentile)	9.4	48.5		4.1	113.8		5.2	29.6		10.6	118.6	
Back of Queue ( Q ), veh/ln ( 50 th percentile)	0.4	1.9		0.2	4.4		0.2	1.2		0.4	4.6	
Queue Storage Ratio ( RQ ) ( 50 th percentile)	0.09	0.00		0.04	0.00		0.05	0.00		0.11	0.00	
Uniform Delay ( d <sub>1</sub> ), s/veh	14.6	10.4		7.1	8.8		26.4	16.0		17.9	19.6	
Incremental Delay ( d <sub>2</sub> ), s/veh	0.6	1.0		0.0	3.0		0.1	0.1		0.0	1.3	
Initial Queue Delay ( d <sub>3</sub> ), s/veh	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Control Delay ( d ), s/veh	15.1	11.5		7.1	11.8		26.5	16.1		17.9	20.9	
Level of Service ( LOS )	B	B		A	B		C	B		B	C	
Approach Delay, s/veh / LOS	11.9		B	11.5		B	17.2		B	20.6		C
Intersection Delay, s/veh / LOS	14.7						B					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	1.89	B	1.87	B	1.91	B	1.91	B
Bicycle LOS Score / LOS	0.99	A	1.65	B	0.74	A	1.26	A

# HCS7 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	WH Pacific			Duration, h	0.25		
Analyst	M Olson	Analysis Date	Sep 19, 2018	Area Type	Other		
Jurisdiction	ADA County	Time Period	AM	PHF	0.90		
Urban Street	Meridian Rd (SR 69)	Analysis Year	2025	Analysis Period	1 > 7:00		
Intersection	Meridian at Hubbard	File Name	E-Meridian_Hubb AM 2025 With Project.xus				
Project Description	2025 AM - With Project						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand ( v ), veh/h	280	140	30	47	25	88	15	1530	28	94	360	45

Signal Information				Signal Timing (s)													
Cycle, s	105.0	Reference Phase	2	Green	2.3	1.7	49.9	4.7	11.9	10.5	Yellow	4.0	4.0	4.0	4.0	4.0	4.0
Offset, s	0	Reference Point	End	Red	0.0	0.0	0.0	0.0	0.0	0.0	Force Mode	Fixed	Simult. Gap E/W	On	Simult. Gap N/S	On	

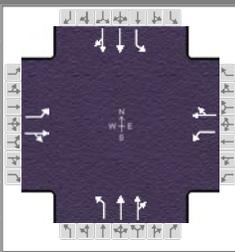
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	7	4	3	8	5	2	1	6
Case Number	2.0	4.0	2.0	4.0	2.0	4.0	2.0	4.0
Phase Duration, s	24.6	30.4	8.7	14.5	6.3	53.9	12.1	59.6
Change Period, ( Y+R <sub>c</sub> ), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Max Allow Headway ( MAH ), s	3.1	3.2	3.1	3.2	2.9	0.0	2.9	0.0
Queue Clearance Time ( g <sub>s</sub> ), s	20.0	11.2	5.1	9.9	3.0		8.3	
Green Extension Time ( g <sub>e</sub> ), s	0.5	0.6	0.1	0.6	0.0	0.0	0.1	0.0
Phase Call Probability	1.00	1.00	0.78	1.00	0.38		0.95	
Max Out Probability	0.00	0.00	0.00	0.00	0.00		0.00	

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	7	4	14	3	8	18	5	2	12	1	6	16
Adjusted Flow Rate ( v ), veh/h	311	189		52	126		17	867	864	104	228	222
Adjusted Saturation Flow Rate ( s ), veh/h/ln	1767	1798		1767	1627		1682	1767	1755	1711	1796	1726
Queue Service Time ( g <sub>s</sub> ), s	18.0	9.2		3.1	7.9		1.0	49.9	49.9	6.3	7.2	7.3
Cycle Queue Clearance Time ( g <sub>c</sub> ), s	18.0	9.2		3.1	7.9		1.0	49.9	49.9	6.3	7.2	7.3
Green Ratio ( g/C )	0.20	0.25		0.04	0.10		0.02	0.48	0.48	0.08	0.53	0.53
Capacity ( c ), veh/h	346	451		79	162		37	840	834	131	952	915
Volume-to-Capacity Ratio ( X )	0.898	0.418		0.661	0.774		0.450	1.032	1.036	0.796	0.240	0.242
Back of Queue ( Q ), ft/ln ( 50 th percentile)	205.1	102		36.1	84		11.9	731.5	735.3	71.2	71.1	69.4
Back of Queue ( Q ), veh/ln ( 50 th percentile)	8.0	4.0		1.4	3.3		0.4	27.3	27.4	2.7	2.7	2.6
Queue Storage Ratio ( RQ ) ( 50 th percentile)	3.42	0.00		0.41	0.00		0.08	0.00	0.00	0.43	0.00	0.00
Uniform Delay ( d <sub>1</sub> ), s/veh	41.2	32.9		49.4	46.1		50.7	27.5	27.5	47.7	13.3	13.3
Incremental Delay ( d <sub>2</sub> ), s/veh	3.4	0.2		3.5	3.0		3.1	39.6	40.9	4.1	0.6	0.6
Initial Queue Delay ( d <sub>3</sub> ), s/veh	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Control Delay ( d ), s/veh	44.6	33.1		52.9	49.1		53.9	67.2	68.5	51.8	13.9	13.9
Level of Service ( LOS )	D	C		D	D		D	F	F	D	B	B
Approach Delay, s/veh / LOS	40.3		D	50.2		D	67.7		E	21.0		C
Intersection Delay, s/veh / LOS	53.4						D					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	2.29	B	2.31	B	1.90	B	1.90	B
Bicycle LOS Score / LOS	1.31	A	0.78	A	1.93	B	0.95	A

# HCS7 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	WH Pacific			Duration, h	0.25		
Analyst	M Olson	Analysis Date	Sep 19, 2018	Area Type	Other		
Jurisdiction	ADA County	Time Period	PM	PHF	0.90		
Urban Street	Meridian Rd (SR 69)	Analysis Year	2025	Analysis Period	1 > 7:00		
Intersection	Meridian at Hubbard	File Name	F-Meridian_Hubb PM 2025 With Project.xus				
Project Description	2025 PM - With Project						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand ( v ), veh/h	140	50	55	55	60	184	50	850	58	74	1170	305

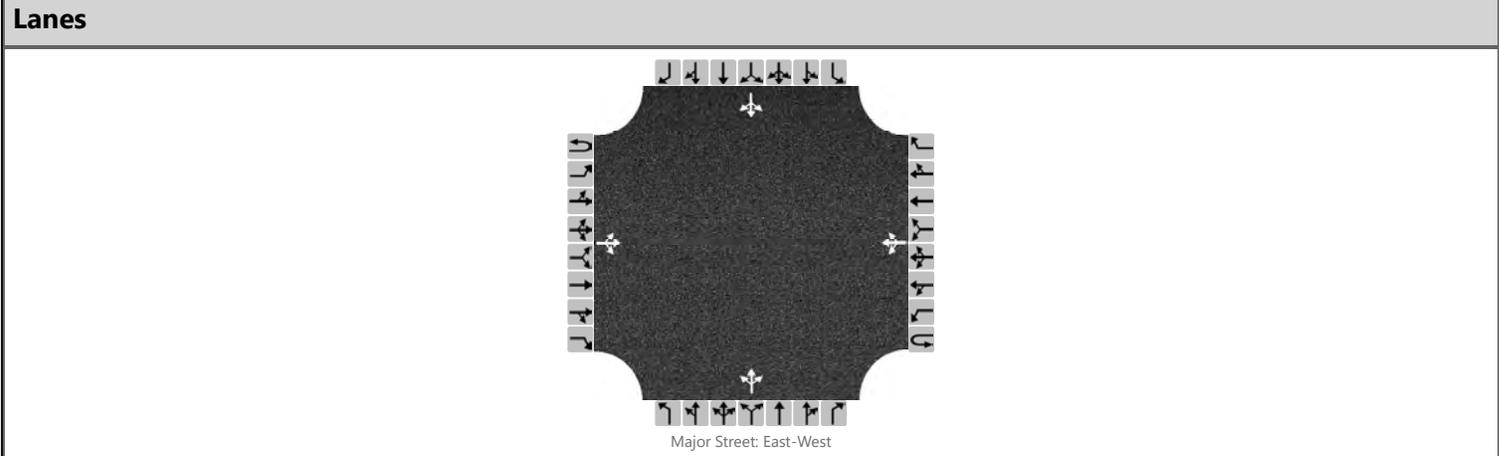
Signal Information				Signal Timing (s)													
Cycle, s	100.0	Reference Phase	2	Green	4.7	1.4	48.4	4.9	1.7	18.8	Yellow	4.0	0.0	4.0	4.0	4.0	4.0
Offset, s	0	Reference Point	End	Red	0.0	0.0	0.0	0.0	0.0	0.0	Red	0.0	0.0	0.0	0.0	0.0	0.0
Uncoordinated	No	Simult. Gap E/W	On														
Force Mode	Fixed	Simult. Gap N/S	On														

Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	7	4	3	8	5	2	1	6
Case Number	2.0	4.0	2.0	4.0	2.0	4.0	2.0	4.0
Phase Duration, s	14.6	28.5	8.9	22.8	8.7	52.4	10.1	53.8
Change Period, ( Y+R <sub>c</sub> ), s	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0
Max Allow Headway ( MAH ), s	3.1	3.3	3.1	3.3	2.9	0.0	2.9	0.0
Queue Clearance Time ( g <sub>s</sub> ), s	10.6	7.6	5.4	18.1	5.3		6.7	
Green Extension Time ( g <sub>e</sub> ), s	0.1	0.8	0.0	0.7	0.1	0.0	0.1	0.0
Phase Call Probability	0.99	1.00	0.82	1.00	0.79		0.90	
Max Out Probability	0.08	0.00	0.00	0.00	0.00		0.00	

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	7	4	14	3	8	18	5	2	12	1	6	16
Adjusted Flow Rate ( v ), veh/h	156	117		61	271		56	510	499	82	836	803
Adjusted Saturation Flow Rate ( s ), veh/h/ln	1767	1696		1767	1634		1682	1767	1726	1711	1796	1669
Queue Service Time ( g <sub>s</sub> ), s	8.6	5.6		3.4	16.1		3.3	21.0	21.0	4.7	43.6	46.5
Cycle Queue Clearance Time ( g <sub>c</sub> ), s	8.6	5.6		3.4	16.1		3.3	21.0	21.0	4.7	43.6	46.5
Green Ratio ( g/C )	0.37	0.25		0.31	0.19		0.31	0.48	0.48	0.32	0.50	0.50
Capacity ( c ), veh/h	187	416		87	308		79	855	836	105	895	832
Volume-to-Capacity Ratio ( X )	0.830	0.280		0.705	0.881		0.700	0.597	0.597	0.783	0.934	0.965
Back of Queue ( Q ), ft/ln ( 50 th percentile)	104.6	57.6		40.2	173.2		37	218.1	213.6	54	525.7	551.4
Back of Queue ( Q ), veh/ln ( 50 th percentile)	4.1	2.3		1.6	6.8		1.4	8.1	8.0	2.0	19.9	20.9
Queue Storage Ratio ( RQ ) ( 50 th percentile)	1.74	0.00		0.46	0.00		0.25	0.00	0.00	0.33	0.00	0.00
Uniform Delay ( d <sub>1</sub> ), s/veh	43.8	30.6		46.8	39.5		46.9	18.7	18.7	46.3	23.5	24.2
Incremental Delay ( d <sub>2</sub> ), s/veh	7.4	0.1		3.9	5.2		4.1	3.1	3.1	4.7	17.7	23.8
Initial Queue Delay ( d <sub>3</sub> ), s/veh	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Control Delay ( d ), s/veh	51.2	30.7		50.7	44.7		51.1	21.8	21.8	51.0	41.3	48.1
Level of Service ( LOS )	D	C		D	D		D	C	C	D	D	D
Approach Delay, s/veh / LOS	42.4		D	45.8		D	23.3		C	44.9		D
Intersection Delay, s/veh / LOS	38.0						D					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	2.29	B	2.30	B	1.90	B	1.90	B
Bicycle LOS Score / LOS	0.94	A	1.04	A	1.37	A	1.91	B

General Information				Site Information			
Analyst	Jerry Liu			Intersection	Locust Grove and Hubbard		
Agency/Co.	WHPacific			Jurisdiction			
Date Performed	9/15/2018			East/West Street	Hubbard R		
Analysis Year	2025			North/South Street	Locust Grove		
Time Analyzed	2025 Build AM			Peak Hour Factor	0.90		
Intersection Orientation	East-West			Analysis Time Period (hrs)	0.25		
Project Description	Trilogy						



**Vehicle Volumes and Adjustments**

Approach	Eastbound				Westbound				Northbound				Southbound				
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Movement	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12	
Priority																	
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	1	0	
Configuration			LTR				LTR				LTR				LTR		
Volume, V (veh/h)		138	132	25		20	104	10		10	315	40		5	55	21	
Percent Heavy Vehicles (%)		3				3				3	3	3		3	3	3	
Proportion Time Blocked																	
Percent Grade (%)										0				0			
Right Turn Channelized		No			No					No			No				
Median Type/Storage		Undivided															

**Critical and Follow-up Headways**

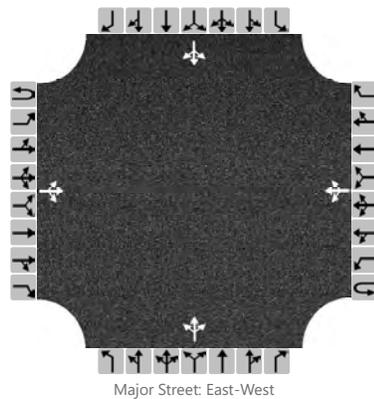
Base Critical Headway (sec)																
Critical Headway (sec)																
Base Follow-Up Headway (sec)																
Follow-Up Headway (sec)																

**Delay, Queue Length, and Level of Service**

Flow Rate, v (veh/h)		153			22					405					90	
Capacity, c (veh/h)		1451			1394					363						
v/c Ratio		0.11			0.02					1.12						
95% Queue Length, Q <sub>95</sub> (veh)		0.4			0.0					15.2						
Control Delay (s/veh)		7.8			7.6					116.2						
Level of Service, LOS		A			A					F						
Approach Delay (s/veh)		4.1			1.2					116.2						
Approach LOS										F						

General Information		Site Information	
Analyst	Jerry Liu	Intersection	Locust Grove and Hubbard
Agency/Co.	WHPacific	Jurisdiction	
Date Performed	9/15/2018	East/West Street	Hubbard R
Analysis Year	2025	North/South Street	Locust Grove
Time Analyzed	2025 Build PM	Peak Hour Factor	0.90
Intersection Orientation	East-West	Analysis Time Period (hrs)	0.25
Project Description	Trilogy		

**Lanes**



**Vehicle Volumes and Adjustments**

Approach	Eastbound				Westbound				Northbound				Southbound				
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Movement	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12	
Priority																	
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	1	0	
Configuration			LTR				LTR				LTR				LTR		
Volume, V (veh/h)		47	107	25		105	252	10		20	120	30		5	285	41	
Percent Heavy Vehicles (%)		3				3				3	3	3		3	3	3	
Proportion Time Blocked																	
Percent Grade (%)										0				0			
Right Turn Channelized		No			No					No			No				
Median Type/Storage		Undivided															

**Critical and Follow-up Headways**

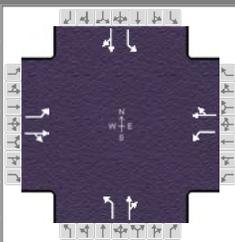
Base Critical Headway (sec)																
Critical Headway (sec)																
Base Follow-Up Headway (sec)																
Follow-Up Headway (sec)																

**Delay, Queue Length, and Level of Service**

Flow Rate, v (veh/h)		52			117					188					369	
Capacity, c (veh/h)		1264			1427										304	
v/c Ratio		0.04			0.08										1.22	
95% Queue Length, Q <sub>95</sub> (veh)		0.1			0.3										16.5	
Control Delay (s/veh)		8.0			7.7										159.2	
Level of Service, LOS		A			A										F	
Approach Delay (s/veh)		2.3			2.8									159.2		
Approach LOS													F			

# HCS7 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	WH Pacific			Duration, h	0.25		
Analyst	Olson	Analysis Date	9/26/2018	Area Type	Other		
Jurisdiction	ADA County	Time Period	AM	PHF	0.90		
Urban Street	E Columbia Rd	Analysis Year	2025	Analysis Period	1 > 7:00		
Intersection	Columbia and Locust Gr...	File Name	K-Columb_LocustGro AM 2025 WithProj.xus				
Project Description	2025 With Project AM						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand ( v ), veh/h	140	510	15	15	150	30	30	392	35	45	79	40

Signal Information				Timing (s)						Signal Phases				
Cycle, s	60.0	Reference Phase	2	Green	1.5	26.5	20.1	0.0	0.0	0.0	1	2	3	4
Offset, s	0	Reference Point	End	Yellow	4.0	4.0	4.0	0.0	0.0	0.0	5	6	7	8
Uncoordinated	No	Simult. Gap E/W	On	Red	0.0	0.0	0.0	0.0	0.0	0.0				
Force Mode	Fixed	Simult. Gap N/S	On											

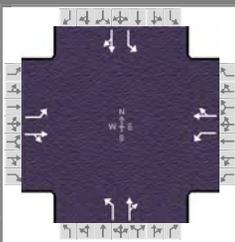
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		2	1	6		8		4
Case Number		6.3	1.0	4.0		6.0		6.0
Phase Duration, s		30.5	5.5	35.9		24.1		24.1
Change Period, ( Y+R <sub>c</sub> ), s		4.0	4.0	4.0		4.0		4.0
Max Allow Headway ( MAH ), s		0.0	3.0	0.0		3.0		3.0
Queue Clearance Time ( g <sub>s</sub> ), s			2.3			16.0		18.9
Green Extension Time ( g <sub>e</sub> ), s		0.0	0.0	0.0		1.2		1.2
Phase Call Probability			0.24			1.00		1.00
Max Out Probability			0.00			0.00		0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate ( v ), veh/h	156	583		17	200		33	474		50	132	
Adjusted Saturation Flow Rate ( s ), veh/h/ln	1173	1846		1767	1801		1248	1829		912	1750	
Queue Service Time ( g <sub>s</sub> ), s	5.1	15.5		0.3	3.5		1.2	14.0		3.1	3.3	
Cycle Queue Clearance Time ( g <sub>c</sub> ), s	5.3	15.5		0.3	3.5		4.3	14.0		16.9	3.3	
Green Ratio ( g/C )	0.44	0.44		0.50	0.53		0.33	0.33		0.33	0.33	
Capacity ( c ), veh/h	635	814		311	958		473	612		216	586	
Volume-to-Capacity Ratio ( X )	0.245	0.716		0.054	0.209		0.071	0.775		0.232	0.226	
Back of Queue ( Q ), ft/ln ( 50 th percentile)	29.7	152.2		2	26.6		7.3	123.6		15.4	27.1	
Back of Queue ( Q ), veh/ln ( 50 th percentile)	1.2	5.9		0.1	1.0		0.3	4.8		0.6	1.1	
Queue Storage Ratio ( RQ ) ( 50 th percentile)	0.30	0.00		0.02	0.00		0.07	0.00		0.15	0.00	
Uniform Delay ( d <sub>1</sub> ), s/veh	10.9	13.7		10.3	7.4		15.9	17.9		25.4	14.4	
Incremental Delay ( d <sub>2</sub> ), s/veh	0.9	5.4		0.0	0.5		0.0	0.8		0.2	0.1	
Initial Queue Delay ( d <sub>3</sub> ), s/veh	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Control Delay ( d ), s/veh	11.8	19.1		10.3	7.9		15.9	18.7		25.6	14.4	
Level of Service ( LOS )	B	B		B	A		B	B		C	B	
Approach Delay, s/veh / LOS	17.5	B		8.1	A		18.5	B		17.5	B	
Intersection Delay, s/veh / LOS	16.6						B					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	1.89	B	1.87	B	1.90	B	1.90	B
Bicycle LOS Score / LOS	1.71	B	0.85	A	1.33	A	0.79	A

# HCS7 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency	WH Pacific			Duration, h	0.25		
Analyst	Olson	Analysis Date	9/26/2018	Area Type	Other		
Jurisdiction	ADA County	Time Period	PM	PHF	0.90		
Urban Street	E Columbia Rd	Analysis Year	2025	Analysis Period	1 > 7:00		
Intersection	Columbia and Locust Gr...	File Name	L-Columb_LocustGro PM 2025 WithProj.xus				
Project Description	2025 With Project PM						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand ( v ), veh/h	35	225	15	35	560	40	15	127	30	40	273	170

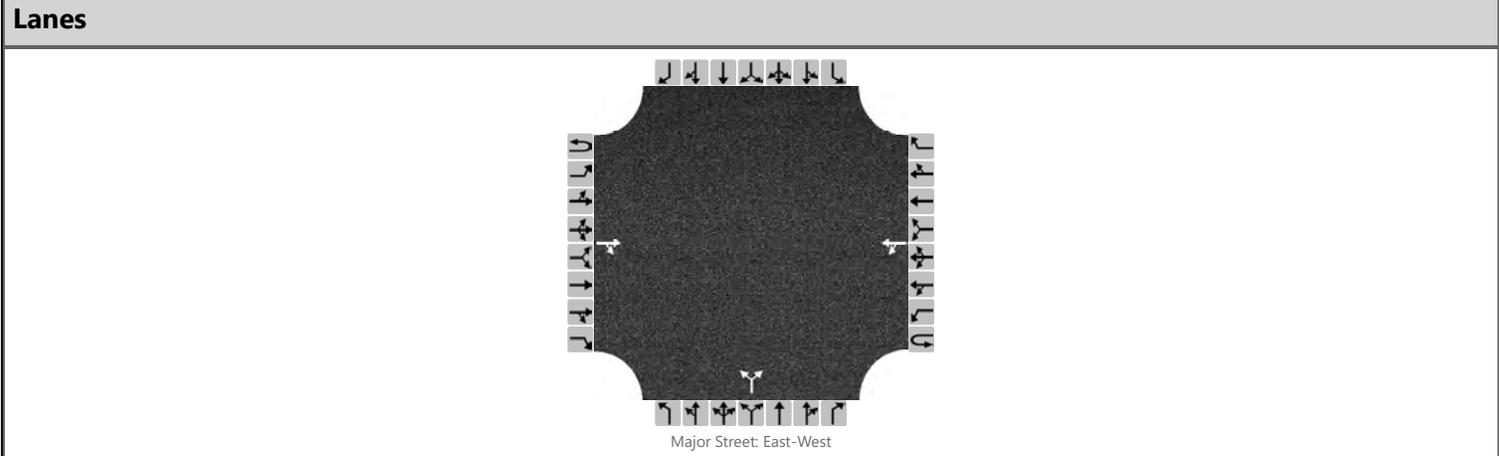
Signal Information				Signal Timing (s)								Signal Phases						
Cycle, s	60.0	Reference Phase	2	Green	2.9	25.1	20.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Offset, s	0	Reference Point	End	Yellow	4.0	4.0	4.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Uncoordinated	No	Simult. Gap E/W	On	Red	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Force Mode	Fixed	Simult. Gap N/S	On															

Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		2	1	6		8		4
Case Number		6.3	1.0	4.0		6.0		6.0
Phase Duration, s		29.1	6.9	35.9		24.1		24.1
Change Period, ( Y+R <sub>c</sub> ), s		4.0	4.0	4.0		4.0		4.0
Max Allow Headway ( MAH ), s		0.0	3.0	0.0		3.0		3.0
Queue Clearance Time ( g <sub>s</sub> ), s			2.7			18.7		17.8
Green Extension Time ( g <sub>e</sub> ), s		0.0	0.0	0.0		1.3		1.3
Phase Call Probability			0.48			1.00		1.00
Max Out Probability			0.00			0.00		0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate ( v ), veh/h	39	267		39	667		17	174		44	492	
Adjusted Saturation Flow Rate ( s ), veh/h/ln	764	1835		1767	1834		897	1794		1201	1736	
Queue Service Time ( g <sub>s</sub> ), s	2.4	5.9		0.7	16.1		1.1	4.3		1.7	15.8	
Cycle Queue Clearance Time ( g <sub>c</sub> ), s	11.7	5.9		0.7	16.1		16.7	4.3		5.9	15.8	
Green Ratio ( g/C )	0.42	0.42		0.50	0.53		0.33	0.33		0.33	0.33	
Capacity ( c ), veh/h	320	765		554	974		186	602		438	582	
Volume-to-Capacity Ratio ( X )	0.122	0.349		0.070	0.684		0.090	0.290		0.101	0.846	
Back of Queue ( Q ), ft/ln ( 50 th percentile)	10.7	54.2		4.7	135.6		5.2	36.5		10.1	134	
Back of Queue ( Q ), veh/ln ( 50 th percentile)	0.4	2.1		0.2	5.3		0.2	1.4		0.4	5.2	
Queue Storage Ratio ( RQ ) ( 50 th percentile)	0.11	0.00		0.05	0.00		0.05	0.00		0.10	0.00	
Uniform Delay ( d <sub>1</sub> ), s/veh	17.2	11.9		8.3	10.4		26.2	14.7		16.8	18.5	
Incremental Delay ( d <sub>2</sub> ), s/veh	0.8	1.3		0.0	3.9		0.1	0.1		0.0	1.3	
Initial Queue Delay ( d <sub>3</sub> ), s/veh	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Control Delay ( d ), s/veh	18.0	13.2		8.3	14.3		26.3	14.8		16.9	19.8	
Level of Service ( LOS)	B	B		A	B		C	B		B	B	
Approach Delay, s/veh / LOS	13.8		B	13.9		B	15.8		B	19.6		B
Intersection Delay, s/veh / LOS	15.9						B					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	1.89	B	1.87	B	1.90	B	1.90	B
Bicycle LOS Score / LOS	0.99	A	1.65	B	0.80	A	1.37	A

General Information				Site Information			
Analyst	Jerry Liu			Intersection	WesEntrance at Hubbard Rd		
Agency/Co.	WHPacific			Jurisdiction			
Date Performed	9/15/2018			East/West Street	Hubbard R		
Analysis Year	2025			North/South Street	West Entrance		
Time Analyzed	2025 AM			Peak Hour Factor	0.90		
Intersection Orientation	East-West			Analysis Time Period (hrs)	0.25		
Project Description	Trilogy						



**Vehicle Volumes and Adjustments**

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Priority																
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0
Configuration				TR		LT					LR					
Volume, V (veh/h)			250	9		10	125			28		45				
Percent Heavy Vehicles (%)						3				3		3				
Proportion Time Blocked																
Percent Grade (%)									0							
Right Turn Channelized	No				No				No				No			
Median Type/Storage	Undivided															

**Critical and Follow-up Headways**

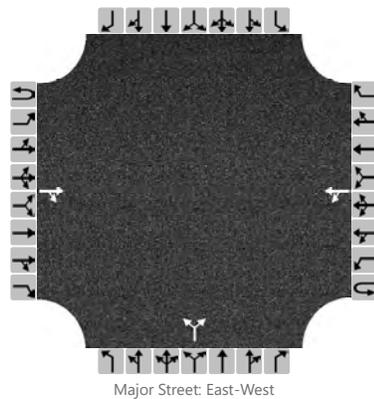
Base Critical Headway (sec)						4.1					7.1		6.2			
Critical Headway (sec)						4.13					7.13		6.23			
Base Follow-Up Headway (sec)						2.2					3.5		3.3			
Follow-Up Headway (sec)						2.23					3.53		3.33			

**Delay, Queue Length, and Level of Service**

Flow Rate, v (veh/h)						11					81					
Capacity, c (veh/h)						1267					642					
v/c Ratio						0.01					0.13					
95% Queue Length, Q <sub>95</sub> (veh)						0.0					0.4					
Control Delay (s/veh)						7.9					11.4					
Level of Service, LOS						A					B					
Approach Delay (s/veh)					0.6				11.4							
Approach LOS									B							

General Information				Site Information			
Analyst	Jerry Liu			Intersection	WesEntrance at Hubbard Rd		
Agency/Co.	WHPacific			Jurisdiction			
Date Performed	9/15/2018			East/West Street	Hubbard R		
Analysis Year	2025			North/South Street	West Entrance		
Time Analyzed	2025 PM			Peak Hour Factor	0.90		
Intersection Orientation	East-West			Analysis Time Period (hrs)	0.25		
Project Description	Trilogy						

**Lanes**



**Vehicle Volumes and Adjustments**

Approach	Eastbound				Westbound				Northbound				Southbound			
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R
Movement	1U	1	2	3	4U	4	5	6		7	8	9		10	11	12
Priority																
Number of Lanes	0	0	1	0	0	0	1	0		0	1	0		0	0	0
Configuration				TR		LT					LR					
Volume, V (veh/h)			150	32		33	280			19		29				
Percent Heavy Vehicles (%)						3				3		3				
Proportion Time Blocked																
Percent Grade (%)									0							
Right Turn Channelized	No				No				No				No			
Median Type/Storage	Undivided															

**Critical and Follow-up Headways**

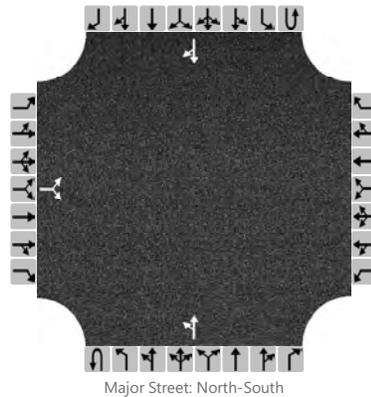
Base Critical Headway (sec)						4.1					7.1		6.2			
Critical Headway (sec)						4.13					7.13		6.23			
Base Follow-Up Headway (sec)						2.2					3.5		3.3			
Follow-Up Headway (sec)						2.23					3.53		3.33			

**Delay, Queue Length, and Level of Service**

Flow Rate, v (veh/h)						37						53				
Capacity, c (veh/h)						1361						606				
v/c Ratio						0.03						0.09				
95% Queue Length, Q <sub>95</sub> (veh)						0.1						0.3				
Control Delay (s/veh)						7.7						11.5				
Level of Service, LOS						A						B				
Approach Delay (s/veh)					1.0				11.5							
Approach LOS									B							

General Information		Site Information	
Analyst	Jerry Liu	Intersection	East Entr at Locust Gro
Agency/Co.	WHPacific	Jurisdiction	
Date Performed	9/15/2018	East/West Street	East Entrance
Analysis Year	2025	North/South Street	Locust Grove Rd
Time Analyzed	2025 AM	Peak Hour Factor	0.90
Intersection Orientation	North-South	Analysis Time Period (hrs)	0.25
Project Description	Trilogy		

**Lanes**



**Vehicle Volumes and Adjustments**

Approach	Eastbound				Westbound				Northbound				Southbound				
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R	
Movement																	
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6	
Number of Lanes		0	1	0		0	0	0	0	0	1	0	0	0	1	0	
Configuration			LR							LT						TR	
Volume, V (veh/h)		60		7						3	305				75	25	
Percent Heavy Vehicles (%)		3		3						3							
Proportion Time Blocked																	
Percent Grade (%)		0															
Right Turn Channelized		No					No					No					
Median Type/Storage		Undivided															

**Critical and Follow-up Headways**

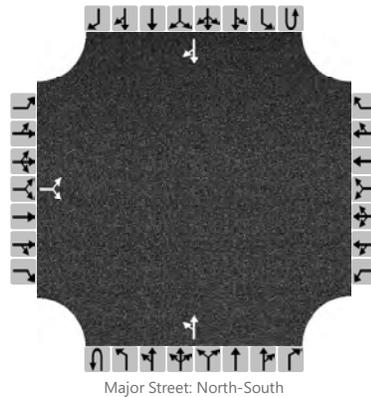
Base Critical Headway (sec)																
Critical Headway (sec)																
Base Follow-Up Headway (sec)																
Follow-Up Headway (sec)																

**Delay, Queue Length, and Level of Service**

Flow Rate, v (veh/h)			75							3						
Capacity, c (veh/h)			595							1471						
v/c Ratio			0.13							0.00						
95% Queue Length, Q <sub>95</sub> (veh)			0.4							0.0						
Control Delay (s/veh)			11.9							7.5						
Level of Service, LOS			B							A						
Approach Delay (s/veh)		11.9										0.1				
Approach LOS		B														

General Information		Site Information	
Analyst	Jerry Liu	Intersection	East Entr at Locust Gro
Agency/Co.	WHPacific	Jurisdiction	
Date Performed	9/15/2018	East/West Street	East Entrance
Analysis Year	2025	North/South Street	Locust Grove Rd
Time Analyzed	2025 PM	Peak Hour Factor	0.90
Intersection Orientation	North-South	Analysis Time Period (hrs)	0.25
Project Description	Trilogy		

**Lanes**



**Vehicle Volumes and Adjustments**

Approach	Eastbound				Westbound				Northbound				Southbound						
	U	L	T	R	U	L	T	R	U	L	T	R	U	L	T	R			
Movement																			
Priority		10	11	12		7	8	9	1U	1	2	3	4U	4	5	6			
Number of Lanes		0	1	0		0	0	0	0	0	1	0	0	0	1	0			
Configuration			LR							LT						TR			
Volume, V (veh/h)		40		5						8	130				330	85			
Percent Heavy Vehicles (%)		3		3						3									
Proportion Time Blocked																			
Percent Grade (%)		0																	
Right Turn Channelized		No					No					No							
Median Type/Storage		Undivided																	

**Critical and Follow-up Headways**

Base Critical Headway (sec)		7.1		6.2						4.1							
Critical Headway (sec)		6.43		6.23						4.13							
Base Follow-Up Headway (sec)		3.5		3.3						2.2							
Follow-Up Headway (sec)		3.53		3.33						2.23							

**Delay, Queue Length, and Level of Service**

Flow Rate, v (veh/h)			50							9							
Capacity, c (veh/h)			488							1094							
v/c Ratio			0.10							0.01							
95% Queue Length, Q <sub>95</sub> (veh)			0.3							0.0							
Control Delay (s/veh)			13.2							8.3							
Level of Service, LOS			B							A							
Approach Delay (s/veh)		13.2								0.6							
Approach LOS		B															

## SIGNAL WARRANT ANALYSIS

### Introduction

- The Signal Warrant Analysis Spreadsheets are a tool for assisting traffic engineers when evaluating the need for a traffic signal installation
  - The filled spreadsheets can be used as part of the supporting documents for the signal warrant evaluation
- Note: This templates are a useful resource, but it remains necessary to apply engineering judgment and to consider specific environmental, traffic, geometric, and operational conditions

### Instructions

<i>Fill in "Orange" areas only</i>	
<i>Automated cells based on in Input Data in "orange" cells</i>	
<i>General Information</i>	<p>Fill in below the general information including:</p> <p>District, County (drop-down menu)</p> <p>City, Engineer, Date</p> <p>Major and Minor Street with corresponding number of lanes and speed limits</p>
<i>Enter Eight Hour Volumes</i>	Any 8 hours of an average day. Major-street and minor-street volumes shall be for the same 8 hours; however, the 8 hours satisfied in Condition A shall <b>not</b> be required to be the same 8 hours satisfied in Condition B <b>for 80% columns only</b> . On the minor street, the higher volume shall not be required to be on the same approach during each of the 8 hours.
<i>Enter Four Hour Volumes</i>	Any 4 hours of an average day. Vehicles per hour on the major street (total of both approaches) and the corresponding vehicles per hour on the higher-volume minor-street approach (one direction only, not required to be on the same approach during each of the 4 hours)
<i>Enter Pedestrian Volumes (4-hr)</i>	Pedestrians per hour crossing the major street (total of all crossings)
<i>Enter Peak Hour Volumes</i>	<p>Vehicular: Any four consecutive 15-minute periods of an average day</p> <p>Pedestrian: Any four consecutive 15-minute periods of an average day representing the vehicles per hour on the major street (total of both approaches) and the corresponding pedestrians per hour crossing the major street (total of all crossings)</p>

Form 750-020-01  
TRAFFIC ENGINEERING  
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**Input Data**

City:	<b>Kuna</b>	Engineer:	<b>R Beckman</b>		
County:		Date:	<b>September 24, 2018</b>		
District:					
Major Street:	<b>Columbia Rd</b>	# Lanes:	<b>1</b>	Major Approach Speed:	<b>50</b>
Minor Street:	<b>Locust Grove</b>	# Lanes:	<b>1</b>	Minor Approach Speed:	<b>50</b>

Eight Hour Volumes (Condition A)		
Hours	Major Street (total of both approaches)	Minor Street (one direction only)
12:00 PM	893	353
1:00 PM	1667	659
2:00 PM	774	306
3:00 PM	1250	494
4:00 PM	714	282
5:00 PM	893	353
6:00 PM	714	282
7:00 PM	833	329

Eight Hour Volumes (Condition B)		
Hours	Major Street (total of both approaches)	Minor Street (one direction only)

Highest Four Hour Vehicular Volumes		
Hours	Major Street (total of both approaches)	Minor Street (one direction only)
12:00 PM	893	353
1:00 PM	1667	659
2:00 PM	774	306
3:00 PM	1250	494

Highest Four Hour Pedestrian Volumes		
Hours	Major Street (total of both approaches)	Pedestrian Crossings on Major Street

Vehicular Peak Hour Volumes			
Peak Hour	Major Street (total of both approaches)	Minor Street (one direction only)	Total Entering Volume
5:00 PM	893	353	1465

Pedestrian Peak Hour Volumes		
Peak Hour	Major Street (total of both approaches)	Pedestrian Crossing Volumes on Major Street



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**Condition B - Interruption of Continuous Traffic**

Condition B is intended for application where Condition A is not satisfied and the traffic volume on a major street is so heavy that traffic on the minor intersecting street suffers excessive delay or conflict in entering or crossing the major street.

Applicable:  Yes  No

100% Satisfied:  Yes  No

80% Satisfied:  Yes  No

70% Satisfied:  Yes  No

Number of Lanes for moving traffic on each approach		Vehicles per hour on major-street (total of both approaches)			Vehicles per hour on minor-street (one direction only)		
Major	Minor	100% <sup>a</sup>	80% <sup>b</sup>	70% <sup>c</sup>	100% <sup>a</sup>	80% <sup>b</sup>	70% <sup>c</sup>
1	1	750	600	525	75	60	53
2 or more	1	900	720	630	75	60	53
2 or more	2 or more	900	720	630	100	80	70
1	2 or more	750	600	525	100	80	70

<sup>a</sup> Basic Minimum hourly volume  
<sup>b</sup> Used for combination of Conditions A and B after adequate trial of other remedial measures  
<sup>c</sup> May be used when the major-street speed exceeds 40 mph or in an isolated community with a population of less than 10,000

Record 8 highest hours and the corresponding major-street and minor-street volumes in the Instructions Sheet.

Eight Highest Hours								
Street								
Major								
Minor								

Existing Volumes

State of Florida Department of Transportation  
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Form 750-020-01  
TRAFFIC ENGINEERING  
10/15

City: **Kuna**  
County: \_\_\_\_\_  
District: \_\_\_\_\_

Engineer: **R Beckman**  
Date: **September 24, 2018**

Major Street: **Columbia Rd** Lanes: **1** Major Approach Speed: **50**  
Minor Street: **Locust Grove** Lanes: **1** Minor Approach Speed: **50**

MUTCD Electronic Reference to Chapter 4: <http://mutcd.fhwa.dot.gov/pdfs/2009r1r2/part4.pdf>

**Volume Level Criteria**

1. Is the posted speed or 85th-percentile of major street > 40 mph (70 km/h)?  Yes  No
  2. Is the intersection in a built-up area of an isolated community with a population < 10,000?  Yes  No
- "70%" volume level **may** be used if Question 1 **or** 2 above is answered "Yes"  Yes  No

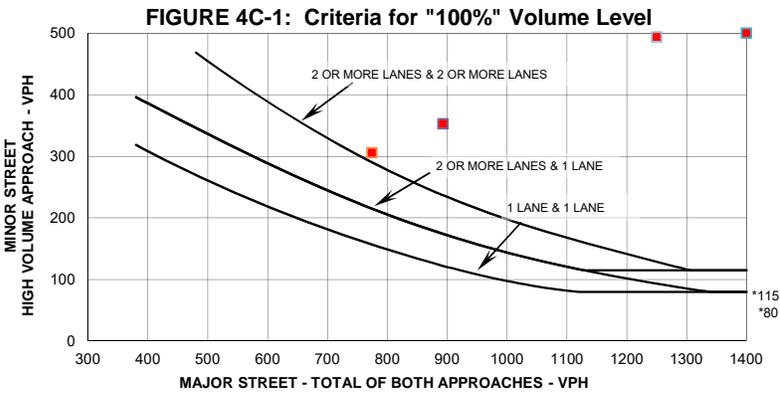
**WARRANT 2 - FOUR-HOUR VEHICULAR VOLUME**

If all four points lie above the appropriate line, then the warrant is satisfied. Applicable:  Yes  No  
Satisfied:  Yes  No

Plot four volume combinations on the applicable figure below.

**100% Volume Level**

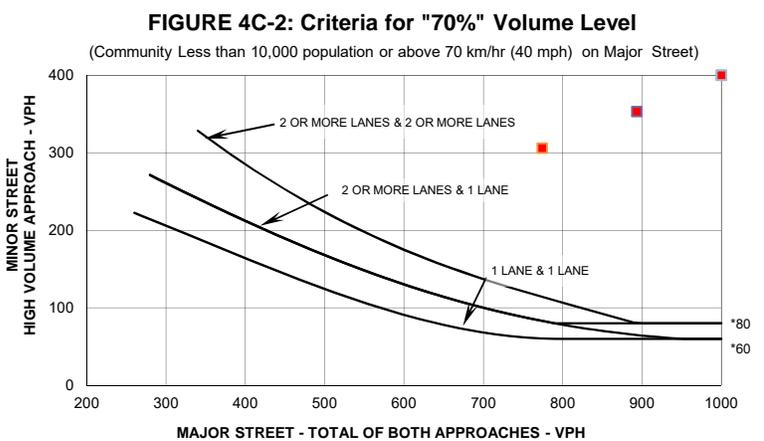
Four Highest Hours	Volumes	
	Major Street	Minor Street
12:00 PM	893	353
1:00 PM	1667	659
2:00 PM	774	306
3:00 PM	1250	494



\* Note: 115 vph applies as the lower threshold volume for a minor street approach with two or more lanes and 80 vph applies as the lower threshold volume threshold for a minor street approach with one lane.

**70% Volume Level**

Four Highest Hours	Volumes	
	Major Street	Minor Street
12:00 PM	893	353
1:00 PM	1667	659
2:00 PM	774	306
3:00 PM	1250	494



\* Note: 80 vph applies as the lower threshold volume for a minor street approach with two or more lanes and 60 vph applies as the lower threshold volume threshold for a minor street approach with one lane.

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TRAFFIC ENGINEERING  
10/15

City: **Kuna**  
County: \_\_\_\_\_  
District: \_\_\_\_\_

Engineer: **R Beckman**  
Date: **September 24, 2018**

Major Street: **Columbia Rd** Lanes: **1** Major Approach Speed: **50**  
Minor Street: **Locust Grove** Lanes: **1** Minor Approach Speed: **50**

MUTCD Electronic Reference to Chapter 4: <http://mutcd.fhwa.dot.gov/pdfs/2009r1r2/part4.pdf>

**Volume Level Criteria**

- 1. Is the posted speed or 85th-percentile of major street > 40 mph (70 km/h)?  Yes  No
  - 2. Is the intersection in a built-up area of an isolated community with a population < 10,000?  Yes  No
- "70%" volume level **may** be used if Question 1 or 2 above is answered "Yes"  70%  100%

**WARRANT 3 - PEAK HOUR**

If all three criteria are fulfilled **or** the plotted point lies above the appropriate line, then the warrant is satisfied. Applicable:  Yes  No  
Satisfied:  Yes  No

Unusual condition justifying use of warrant:

\_\_\_\_\_

Record hour when criteria are fulfilled and the corresponding delay or volume in boxes provided.

Peak Hour 100% Volume		
Time	Major Vol.	Minor Vol.

Peak Hour 70% Volume		
Time	Major Vol.	Minor Vol.
5:00 PM	893	353

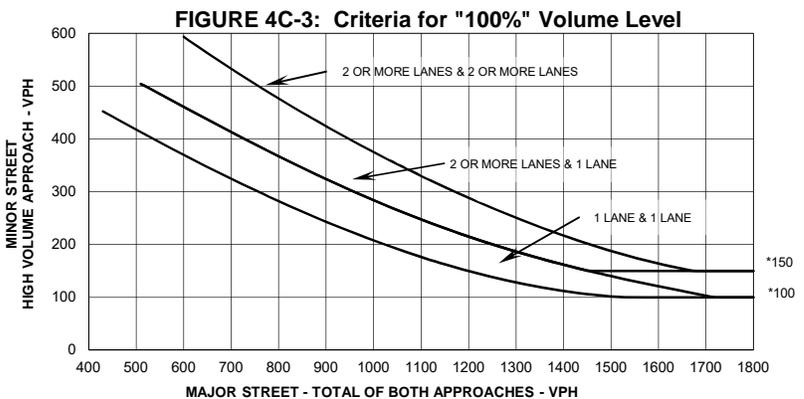
**Criteria**

1. Delay on Minor Approach *(vehicle-hours)		
Approach Lanes	1	2
Delay Criteria*	4.0	5.0
Delay*		
Fulfilled?:	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No

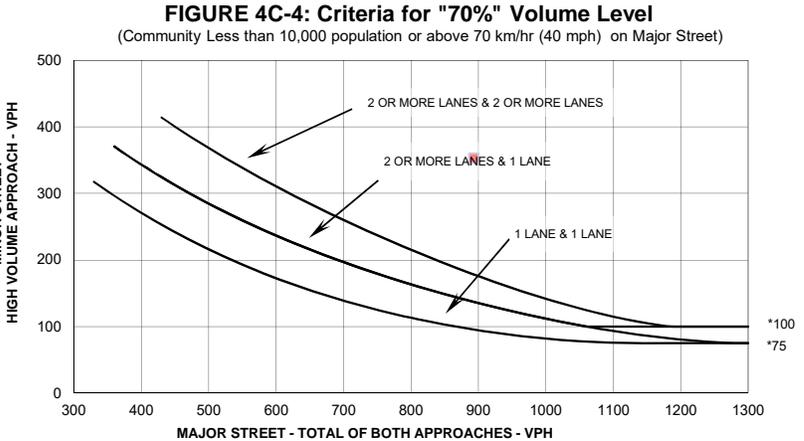
2. Volume on Minor Approach One-Direction *(vehicles per hour)		
Approach Lanes	1	2
Volume Criteria*	100	150
Volume*		
Fulfilled?:	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No

3. Total Intersection Entering Volume *(vehicles per hour)		
No. of Approaches	3	4
Volume Criteria*	650	800
Volume*		
Fulfilled?:	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No

Plot volume combination on the applicable figure below.



\* Note: 150 vph applies as the lower threshold volume for a minor street approach with two or more lanes and 100 vph applies as the lower threshold volume threshold for a minor street approach with one lane.



\* Note: 100 vph applies as the lower threshold volume for a minor street approach with two or more lanes and 75 vph applies as the lower threshold volume threshold for a minor street approach with one lane.

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TRAFFIC ENGINEERING  
10/15

City: **Kuna**  
County: \_\_\_\_\_  
District: \_\_\_\_\_

Engineer: **R Beckman**  
Date: **September 24, 2018**

Major Street: **Columbia Rd** Lanes: **1** Major Approach Speed: **50**  
Minor Street: **Locust Grove** Lanes: **1** Minor Approach Speed: **50**

MUTCD Electronic Reference to Chapter 4: <http://mutcd.fhwa.dot.gov/pdfs/2009r1r2/part4.pdf>

**Volume Level Criteria**

- 1. Is the posted speed or 85th-percentile of major street > 40 mph (70 km/h)?  Yes  No
  - 2. Is the intersection in a built-up area of an isolated community with a population < 10,000?  Yes  No
- "70%" volume level **may** be used if Question 1 or 2 above is answered "Yes"  70%  100%

**WARRANT 4 - PEDESTRIAN VOLUME**

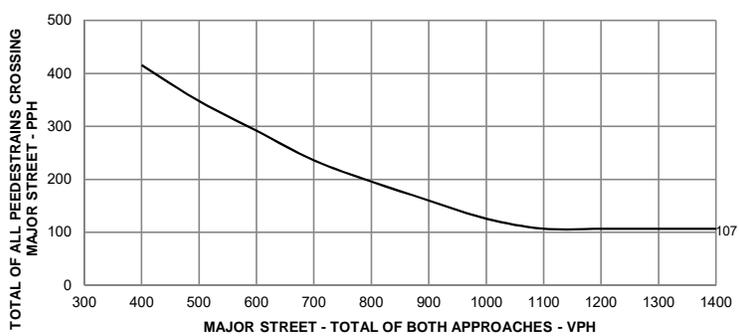
For each of any 4 hours of an average day, the plotted points lie above the appropriate line, then the warrant is satisfied.

Applicable:  Yes  No  
Satisfied:  Yes  No

Plot four volume combinations on the applicable figure below.

**Figure 4C-5. Criteria for "100%" Volume Level**

Four Highest Hours	Volumes	
	Major Street	Pedestrian Total

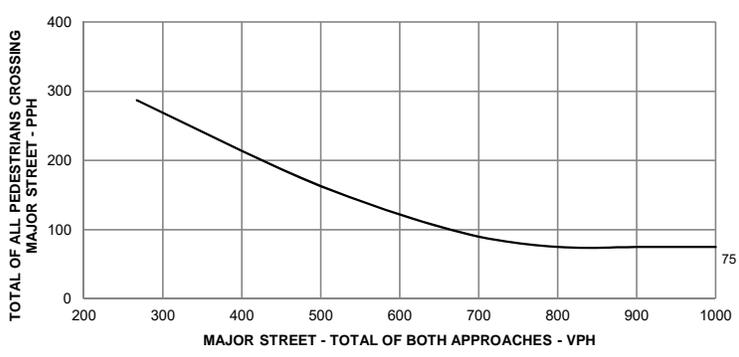


\* Note: 107 pph applies as the lower threshold volume

**70% Volume Level**

Four Highest Hours	Volumes	
	Major Street	Pedestrian Total

**Figure 4C-6 Criteria for "70%" Volume Level**



\* Note: 75 pph applies as the lower threshold volume

**WARRANT 4 - PEDESTRIAN VOLUME**

For 1 hour (any four consecutive 15-minute periods) of an average day, the plotted point falls above the appropriate line, then the warrant is satisfied.

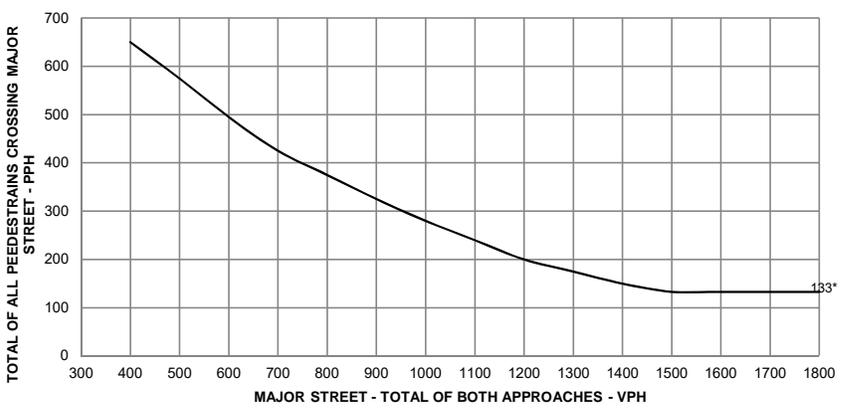
Applicable:  Yes  No  
Satisfied:  Yes  No

Plot one volume combination on the applicable figure below.

**100% Volume Level**

Peak Hour	Volumes	
	Major Street	Pedestrian Total

**Figure 4C-7. Criteria for "100%" Volume Level - Peak Hour**



\* Note: 133 pph applies as the lower threshold volume

**70% Volume Level**

Peak Hour	Volumes	
	Major Street	Pedestrian Total

**Figure 4C-8 Criteria for "70%" Volume Level - Peak Hour**



\* Note: 93 pph applies as the lower threshold volume

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City: **Kuna**  
County:  
District:

Engineer: **R Beckman**  
Date: **September 24, 2018**

Major Street: **Columbia Rd** Lanes: **1** Major Approach Speed: **50**  
Minor Street: **Locust Grove** Lanes: **1** Minor Approach Speed: **50**

MUTCD Electronic Reference to Chapter 4: <http://mutcd.fhwa.dot.gov/pdfs/2009r1r2/part4.pdf>

**WARRANT 5 - SCHOOL CROSSING**

Record hours where criteria are fulfilled and the corresponding volume or gap frequency in the boxes provided. The warrant is satisfied if all three of the criteria are fulfilled.

Applicable:  Yes  No  
Satisfied:  Yes  No

Criteria			Fulfilled?	
			Yes	No
1. There are a minimum of 20 students crossing the major street during the highest crossing hour.	Students:	Hour:		
2. There are fewer adequate gaps in the major street traffic stream during the period when the children are using the established school crossing than the number of minutes in the same period.	Minutes:	Gaps:		
3. The nearest traffic signal along the major street is located more than 300 ft. (90 m) away, or the nearest signal is within 300 ft. (90 m) but the proposed traffic signal will not restrict the progressive movement of traffic.				

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TRAFFIC ENGINEERING  
10/15

City: **Kuna**  
County: \_\_\_\_\_  
District: \_\_\_\_\_

Engineer: **R Beckman**  
Date: **September 24, 2018**

Major Street: **Columbia Rd** Lanes: **1** Major Approach Speed: **50**  
Minor Street: **Locust Grove** Lanes: **1** Minor Approach Speed: **50**

MUTCD Electronic Reference to Chapter 4: <http://mutcd.fhwa.dot.gov/pdfs/2009r1r2/part4.pdf>

**WARRANT 6 - COORDINATED SIGNAL SYSTEM**

*Indicate if the criteria are fulfilled in the boxes provided. The warrant is satisfied if either criterion is fulfilled. This warrant should not be applied when the resulting signal spacing would be less than 300 m (1,000 ft.).*

Applicable:  Yes  No  
Satisfied:  Yes  No

Criteria	Fulfilled?	
	Yes	No
1. On a one-way street or a street that has traffic predominately in one direction, the adjacent signals are so far apart that they do not provide the necessary degree of vehicle platooning.		
2. On a two-way street, adjacent signals do not provide the necessary degree of platooning, and the proposed and adjacent signals will collectively provide a progressive operation.		



State of Florida Department of Transportation  
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City: <b>Kuna</b>	Engineer: <b>R Beckman</b>
County: _____	Date: <b>September 24, 2018</b>
District: _____	
Major Street: <b>Columbia Rd</b>	Lanes: <b>1</b> Major Approach Speed: <b>50</b>
Minor Street: <b>Locust Grove</b>	Lanes: <b>1</b> Minor Approach Speed: <b>50</b>

MUTCD Electronic Reference to Chapter 4: <http://mutcd.fhwa.dot.gov/pdfs/2009r1r2/part4.pdf>

**WARRANT 8 - ROADWAY NETWORK**

Record hours where criteria are fulfilled, and the corresponding volume or other information in the boxes provided. The warrant is satisfied if at least one of the criteria is fulfilled and if all intersecting routes have one or more of the Major Route characteristics listed.

Applicable:  Yes  No  
Satisfied:  Yes  No

Criteria						Met?		Fulfilled?		
						Yes	No	Yes	No	
1.	Both of the criteria to the right are met.	a. Total entering volume of at least 1,000 veh/hr during a typical weekday peak hour.	Entering Volume:							
		b. Five-year projected volumes that satisfy one or more of Warrants 1, 2, or 3.	Warrant:	1	2	3				
			Satisfied?:							
2.	Total entering volume at least 1,000 veh/hr for each of any 5 hrs of a non-normal business day (Sat. or Sun.)					← Hour				
						← Volume				

Characteristics of Major Routes						Met?		Fulfilled?	
						Yes	No	Yes	No
1.	Part of the street or highway system that serves as the principal roadway network for through traffic flow.	Major Street:							
		Minor Street:							
2.	Rural or suburban highway outside of, entering, or traversing a city.	Major Street:							
		Minor Street:							
3.	Appears as a major route on an official plan.	Major Street:							
		Minor Street:							

State of Florida Department of Transportation  
**TRAFFIC SIGNAL WARRANT SUMMARY**

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TRAFFIC ENGINEERING  
10/15

City:           Kuna            
County:                             
District:                           

Engineer:           R Beckman            
Date:           September 24, 2018          

Major Street:           Columbia Rd           Lanes:           1           Major Approach Speed:           50            
Minor Street:           Locust Grove           Lanes:           1           Minor Approach Speed:           50          

MUTCD Electronic Reference to Chapter 4: <http://mutcd.fhwa.dot.gov/pdfs/2009r1r2/part4.pdf>

**Approach Lane Criteria**

1. How many approach lanes are there at the track crossing?  
If there is 1 lane, use Figure 4C-9 and if there are 2 or more, use Figure 4C-10.

1       2 or more  
 Fig 4C-9       Fig 4C-10

**WARRANT 9 - INTERSECTION NEAR A GRADE CROSSING**

*This signal warrant should be applied only after adequate consideration has been given to other alternatives or after a trial of an alternative has failed to alleviate the safety concerns associated with the grade crossing.*

Indicate if both criteria are fulfilled in the boxes provided. The warrant is satisfied if both criteria are met.

Applicable:  Yes       No  
Satisfied:  Yes       No

Criteria	Fulfilled?	
	Yes	No
1. A grade crossing exists on an approach controlled by a STOP or YIELD sign and the center of the track nearest to the intersection is within 140 feet of the stop line or yield line on the approach; and	<input type="checkbox"/>	<input type="checkbox"/>
2. During the highest traffic volume hour during which the rail uses the crossing, the plotted point falls above the applicable curve for the existing combination of approach lanes over the track and the distance D (clear storage distance).	<input type="checkbox"/>	<input type="checkbox"/>

Use the following tables (4C-2, 4C-3, and 4C-4 to appropriately adjust the minor-street approach volume).

**Inputs**

Occurrences of Rail traffic per day  
% of High Occupancy Buses on Minor-Street Approach  
Enter D (feet)  
% of Tractor-Trailer Trucks on Minor-Street Approach

**Adjustment Factors from Tables**

                            
          1.00            
                            
          0.50          

**Table 4C-2. Adjustment Factor for Daily Frequency of Rail Traffic**

Rail Traffic per Day	Adjustment Factor
1	0.67
2	0.91
3 to 5	1.00
6 to 8	1.18
9 to 11	1.25
12 or more	1.33

**Table 4C-3. Adjustment Factor for Percentage of High-Occupancy Buses**

% of High-Occupancy Buses* on Minor Street Approach	Adjustment Factor
0%	1.00
2%	1.09
4%	1.19
6% or more	1.32

\* A high-occupancy bus is defined as a bus occupied by at least 20 people

**Table 4C-4. Adjustment Factor for Percentage of Tractor-Trailer Trucks**

% of Tractor-Trailer Trucks on Minor-Street Approach	Adjustment Factor	
	D less than 70 feet	D of 70 feet or more
0% to 2.5%	0.50	0.50
2.6% to 7.5%	0.75	0.75
7.6% to 12.5%	1.00	1.00
12.6% to 17.5%	2.30	1.15
17.6% to 22.5%	2.70	1.35
22.6% to 27.5%	3.28	1.64
More than 27.5%	4.18	2.09

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Input the major and minor street volumes before adjustment factors are applied

1 Approach Lane		

D (ft) Major Vol. Minor Vol.

After adjustment factors are applied

1 Approach Lane w/Factors		

D (ft) Major Vol. Minor Vol.

Input D and the major and minor street volumes before adjustment factors are applied

2 or more Approach Lanes		

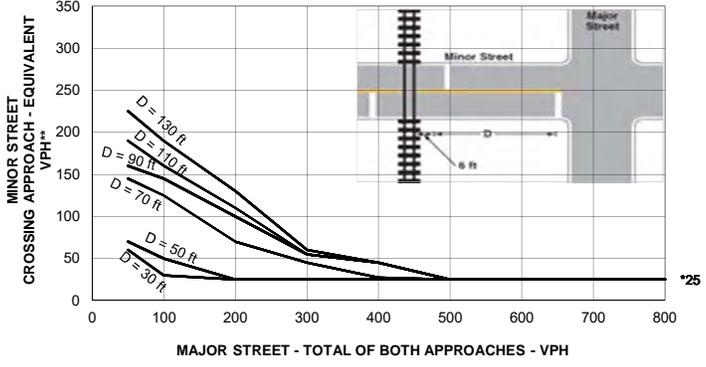
D (ft) Major Vol. Minor Vol.

After adjustment factors are applied

2+ Approach Lane w/Factors		

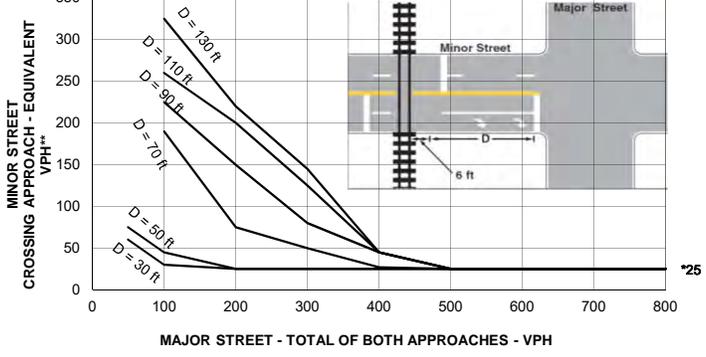
D (ft) Major Vol. Minor Vol.

FIGURE 4C-9: Criteria for 1 Approach Lane at the Track Crossing



\* Note: 25 vph applies as the lower threshold volume  
\* \*Note: VPH after applying the adjustment factors in Tables 4C-2, 4C, and or 4C-4, if appropriate

FIGURE 4C-10: Criteria for 2+ Approach Lanes at Track Crossing



\* Note: 25 vph applies as the lower threshold volume  
\* \*Note: VPH after applying the adjustment factors in Tables 4C-2, 4C, and or 4C-4, if appropriate



## SIGNAL WARRANT ANALYSIS

### Introduction

- The Signal Warrant Analysis Spreadsheets are a tool for assisting traffic engineers when evaluating the need for a traffic signal installation
  - The filled spreadsheets can be used as part of the supporting documents for the signal warrant evaluation
- Note: This templates are a useful resource, but it remains necessary to apply engineering judgment and to consider specific environmental, traffic, geometric, and operational conditions

### Instructions

<i>Fill in "Orange" areas only</i>	
<i>Automated cells based on in Input Data in "orange" cells</i>	
<i>General Information</i>	<p>Fill in below the general information including:</p> <ul style="list-style-type: none"> <li>District, County (drop-down menu)</li> <li>City, Engineer, Date</li> <li>Major and Minor Street with corresponding number of lanes and speed limits</li> </ul>
<i>Enter Eight Hour Volumes</i>	Any 8 hours of an average day. Major-street and minor-street volumes shall be for the same 8 hours; however, the 8 hours satisfied in Condition A shall <b>not</b> be required to be the same 8 hours satisfied in Condition B <b>for 80% columns only</b> . On the minor street, the higher volume shall not be required to be on the same approach during each of the 8 hours.
<i>Enter Four Hour Volumes</i>	Any 4 hours of an average day. Vehicles per hour on the major street (total of both approaches) and the corresponding vehicles per hour on the higher-volume minor-street approach (one direction only, not required to be on the same approach during each of the 4 hours)
<i>Enter Pedestrian Volumes (4-hr)</i>	Pedestrians per hour crossing the major street (total of all crossings)
<i>Enter Peak Hour Volumes</i>	<p>Vehicular: Any four consecutive 15-minute periods of an average day</p> <p>Pedestrian: Any four consecutive 15-minute periods of an average day representing the vehicles per hour on the major street (total of both approaches) and the corresponding pedestrians per hour crossing the major street (total of all crossings)</p>

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**Input Data**

City: **Kuna**  
 County:   
 District:   
 Engineer: **R Beckman**  
 Date: **September 24, 2018**

Major Street: **Locust Grove** # Lanes: **1** Major Approach Speed: **50**  
 Minor Street: **Hubbard Rd** # Lanes: **1** Minor Approach Speed: **45**

Eight Hour Volumes (Condition A)		
Hours	Major Street (total of both approaches)	Minor Street (one direction only)
12:00 PM	150	123
1:00 PM	160	132
2:00 PM	235	193
3:00 PM	240	197
4:00 PM	390	320
5:00 PM	390	320
6:00 PM	240	197
7:00 PM	155	127

Eight Hour Volumes (Condition B)		
Hours	Major Street (total of both approaches)	Minor Street (one direction only)

Highest Four Hour Vehicular Volumes		
Hours	Major Street (total of both approaches)	Minor Street (one direction only)
3:00 PM	240	197
4:00 PM	390	320
5:00 PM	390	320
6:00 PM	240	197

Highest Four Hour Pedestrian Volumes		
Hours	Major Street (total of both approaches)	Pedestrian Crossings on Major Street

Vehicular Peak Hour Volumes			
Peak Hour	Major Street (total of both approaches)	Minor Street (one direction only)	Total Entering Volume
5:00 PM	390	320	860

Pedestrian Peak Hour Volumes		
Peak Hour	Major Street (total of both approaches)	Pedestrian Crossing Volumes on Major Street



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**Condition B - Interruption of Continuous Traffic**

Condition B is intended for application where Condition A is not satisfied and the traffic volume on a major street is so heavy that traffic on the minor intersecting street suffers excessive delay or conflict in entering or crossing the major street.

Applicable:  Yes  No  
 100% Satisfied:  Yes  No  
 80% Satisfied:  Yes  No  
 70% Satisfied:  Yes  No

Number of Lanes for moving traffic on each approach		Vehicles per hour on major-street (total of both approaches)			Vehicles per hour on minor-street (one direction only)		
Major	Minor	100% <sup>a</sup>	80% <sup>b</sup>	70% <sup>c</sup>	100% <sup>a</sup>	80% <sup>b</sup>	70% <sup>c</sup>
1	1	750	600	525	75	60	53
2 or more	1	900	720	630	75	60	53
2 or more	2 or more	900	720	630	100	80	70
1	2 or more	750	600	525	100	80	70

<sup>a</sup> Basic Minimum hourly volume  
<sup>b</sup> Used for combination of Conditions A and B after adequate trial of other remedial measures  
<sup>c</sup> May be used when the major-street speed exceeds 40 mph or in an isolated community with a population of less than 10,000

Record 8 highest hours and the corresponding major-street and minor-street volumes in the Instructions Sheet.

Eight Highest Hours								
Street								
Major								
Minor								

Existing Volumes

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Form 750-020-01  
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City: **Kuna**  
County: \_\_\_\_\_  
District: \_\_\_\_\_

Engineer: **R Beckman**  
Date: **September 24, 2018**

Major Street: **Locust Grove** Lanes: **1** Major Approach Speed: **50**  
Minor Street: **Hubbard Rd** Lanes: **1** Minor Approach Speed: **45**

MUTCD Electronic Reference to Chapter 4: <http://mutcd.fhwa.dot.gov/pdfs/2009r1r2/part4.pdf>

**Volume Level Criteria**

1. Is the posted speed or 85th-percentile of major street > 40 mph (70 km/h)?  Yes  No
  2. Is the intersection in a built-up area of an isolated community with a population < 10,000?  Yes  No
- "70%" volume level **may** be used if Question 1 **or** 2 above is answered "Yes"  Yes  No

**WARRANT 2 - FOUR-HOUR VEHICULAR VOLUME**

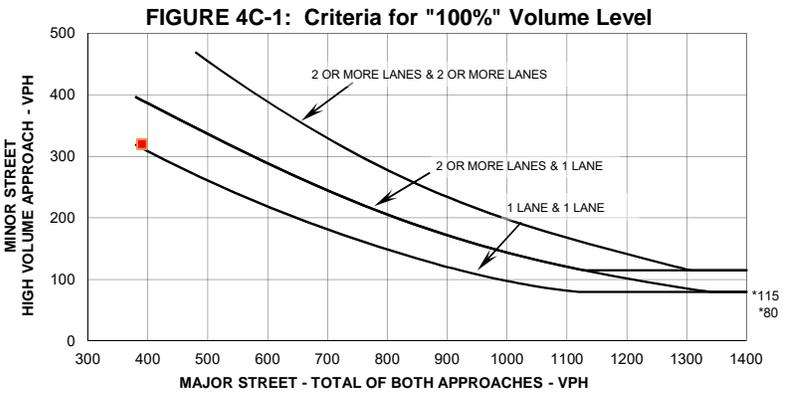
*If all four points lie above the appropriate line, then the warrant is satisfied.*

Applicable:  Yes  No  
Satisfied:  Yes  No

Plot four volume combinations on the applicable figure below.

**100% Volume Level**

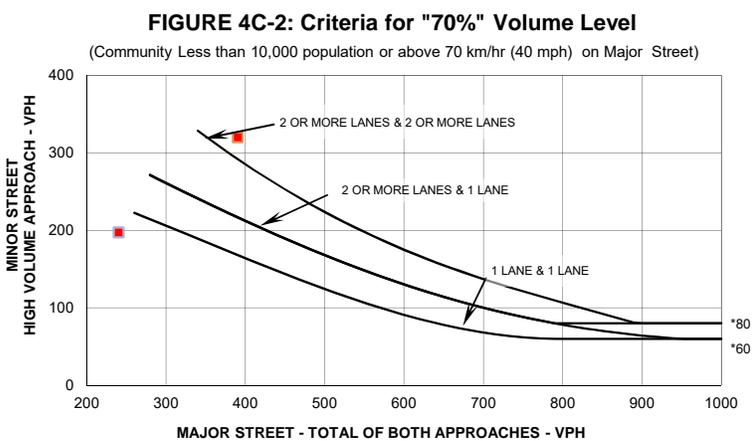
Four Highest Hours	Volumes	
	Major Street	Minor Street
3:00 PM	240	197
4:00 PM	390	320
5:00 PM	390	320
6:00 PM	240	197



\* Note: 115 vph applies as the lower threshold volume for a minor street approach with two or more lanes and 80 vph applies as the lower threshold volume threshold for a minor street approach with one lane.

**70% Volume Level**

Four Highest Hours	Volumes	
	Major Street	Minor Street
3:00 PM	240	197
4:00 PM	390	320
5:00 PM	390	320
6:00 PM	240	197



\* Note: 80 vph applies as the lower threshold volume for a minor street approach with two or more lanes and 60 vph applies as the lower threshold volume threshold for a minor street approach with one lane.

State of Florida Department of Transportation  
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City: **Kuna**  
County: \_\_\_\_\_  
District: \_\_\_\_\_

Engineer: **R Beckman**  
Date: **September 24, 2018**

Major Street: **Locust Grove** Lanes: **1** Major Approach Speed: **50**  
Minor Street: **Hubbard Rd** Lanes: **1** Minor Approach Speed: **45**

MUTCD Electronic Reference to Chapter 4: <http://mutcd.fhwa.dot.gov/pdfs/2009r1r2/part4.pdf>

**Volume Level Criteria**

- 1. Is the posted speed or 85th-percentile of major street > 40 mph (70 km/h)?  Yes  No
  - 2. Is the intersection in a built-up area of an isolated community with a population < 10,000?  Yes  No
- "70%" volume level **may** be used if Question 1 or 2 above is answered "Yes"  70%  100%

**WARRANT 3 - PEAK HOUR**

If all three criteria are fulfilled **or** the plotted point lies above the appropriate line, then the warrant is satisfied. Applicable:  Yes  No  
Satisfied:  Yes  No

Unusual condition justifying use of warrant:

\_\_\_\_\_

Record hour when criteria are fulfilled and the corresponding delay or volume in boxes provided.

Peak Hour 100% Volume		
Time	Major Vol.	Minor Vol.

Peak Hour 70% Volume		
Time	Major Vol.	Minor Vol.
5:00 PM	390	320

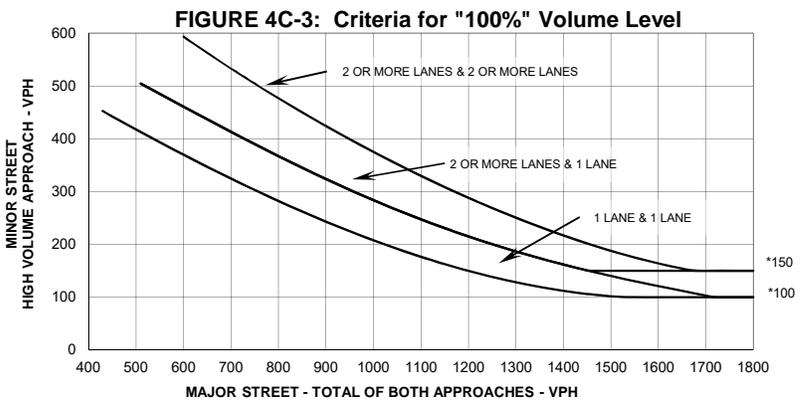
**Criteria**

1. Delay on Minor Approach (vehicle-hours)		
Approach Lanes	1	2
Delay Criteria*	4.0	5.0
Delay*		
Fulfilled?:	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No

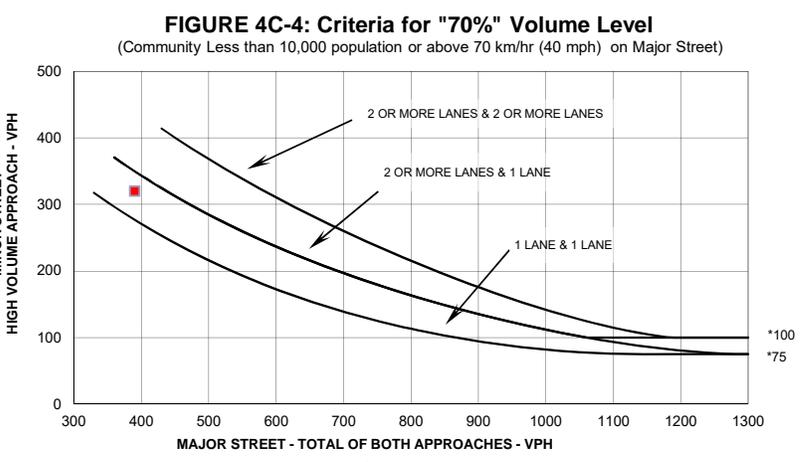
2. Volume on Minor Approach One-Direction (vehicles per hour)		
Approach Lanes	1	2
Volume Criteria*	100	150
Volume*		
Fulfilled?:	<input type="checkbox"/> Yes	<input checked="" type="checkbox"/> No

3. Total Intersection Entering Volume (vehicles per hour)		
No. of Approaches	3	4
Volume Criteria*	650	800
Volume*		
Fulfilled?:	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> No

Plot volume combination on the applicable figure below.



\* Note: 150 vph applies as the lower threshold volume for a minor street approach with two or more lanes and 100 vph applies as the lower threshold volume threshold for a minor street approach with one lane.



\* Note: 100 vph applies as the lower threshold volume for a minor street approach with two or more lanes and 75 vph applies as the lower threshold volume threshold for a minor street approach with one lane.

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City: **Kuna**  
County: \_\_\_\_\_  
District: \_\_\_\_\_

Engineer: **R Beckman**  
Date: **September 24, 2018**

Major Street: **Locust Grove** Lanes: **1** Major Approach Speed: **50**  
Minor Street: **Hubbard Rd** Lanes: **1** Minor Approach Speed: **45**

MUTCD Electronic Reference to Chapter 4: <http://mutcd.fhwa.dot.gov/pdfs/2009r1r2/part4.pdf>

**Volume Level Criteria**

- 1. Is the posted speed or 85th-percentile of major street > 40 mph (70 km/h)?  Yes  No
  - 2. Is the intersection in a built-up area of an isolated community with a population < 10,000?  Yes  No
- "70%" volume level **may** be used if Question 1 or 2 above is answered "Yes"  70%  100%

**WARRANT 4 - PEDESTRIAN VOLUME**

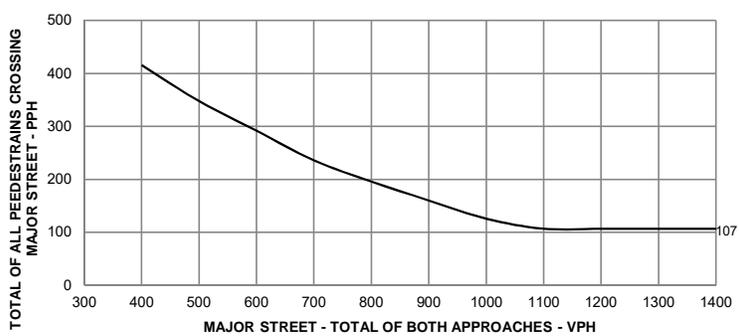
For each of any 4 hours of an average day, the plotted points lie above the appropriate line, then the warrant is satisfied. Applicable:  Yes  No  
Satisfied:  Yes  No

Plot four volume combinations on the applicable figure below.

**Figure 4C-5. Criteria for "100%" Volume Level**

**100% Volume Level**

Four Highest Hours	Volumes	
	Major Street	Pedestrian Total

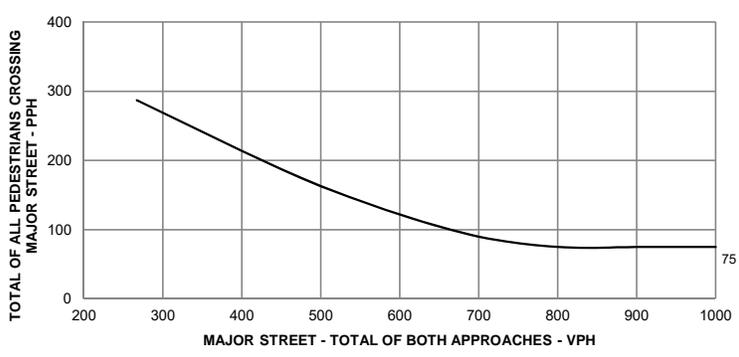


\* Note: 107 pph applies as the lower threshold volume

**70% Volume Level**

Four Highest Hours	Volumes	
	Major Street	Pedestrian Total

**Figure 4C-6 Criteria for "70%" Volume Level**



\* Note: 75 pph applies as the lower threshold volume

**WARRANT 4 - PEDESTRIAN VOLUME**

For 1 hour (any four consecutive 15-minute periods) of an average day, the plotted point falls above the appropriate line, then the warrant is satisfied.

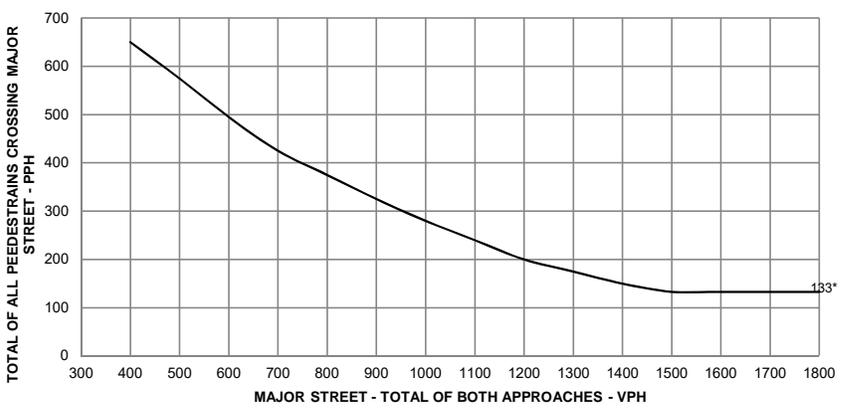
Applicable:  Yes  No  
Satisfied:  Yes  No

Plot one volume combination on the applicable figure below.

**100% Volume Level**

Peak Hour	Volumes	
	Major Street	Pedestrian Total

**Figure 4C-7. Criteria for "100%" Volume Level - Peak Hour**

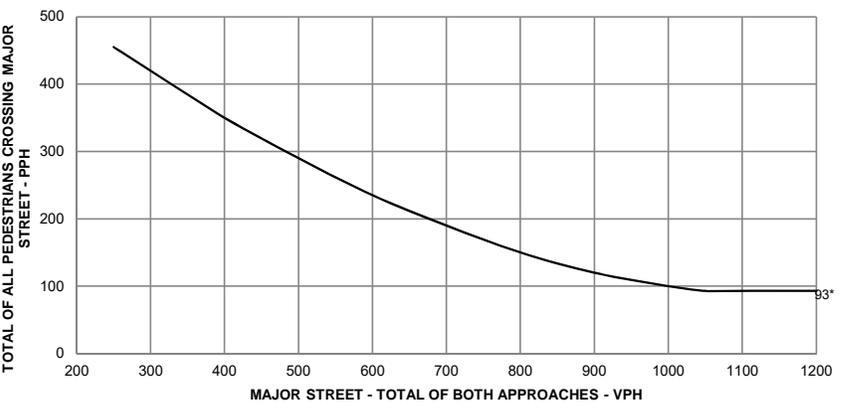


\* Note: 133 pph applies as the lower threshold volume

**70% Volume Level**

Peak Hour	Volumes	
	Major Street	Pedestrian Total

**Figure 4C-8 Criteria for "70%" Volume Level - Peak Hour**



\* Note: 93 pph applies as the lower threshold volume

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City: **Kuna**  
County:  
District:

Engineer: **R Beckman**  
Date: **September 24, 2018**

Major Street: **Locust Grove** Lanes: **1** Major Approach Speed: **50**  
Minor Street: **Hubbard Rd** Lanes: **1** Minor Approach Speed: **45**

MUTCD Electronic Reference to Chapter 4: <http://mutcd.fhwa.dot.gov/pdfs/2009r1r2/part4.pdf>

**WARRANT 5 - SCHOOL CROSSING**

*Record hours where criteria are fulfilled and the corresponding volume or gap frequency in the boxes provided. The warrant is satisfied if all three of the criteria are fulfilled.*

Applicable:  Yes  No  
Satisfied:  Yes  No

Criteria				Fulfilled?	
				Yes	No
1. There are a minimum of 20 students crossing the major street during the highest crossing hour.	Students:	Hour:			
2. There are fewer adequate gaps in the major street traffic stream during the period when the children are using the established school crossing than the number of minutes in the same period.	Minutes:	Gaps:			
3. The nearest traffic signal along the major street is located more than 300 ft. (90 m) away, or the nearest signal is within 300 ft. (90 m) but the proposed traffic signal will not restrict the progressive movement of traffic.					

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City: **Kuna**  
County: \_\_\_\_\_  
District: \_\_\_\_\_

Engineer: **R Beckman**  
Date: **September 24, 2018**

Major Street: **Locust Grove** Lanes: **1** Major Approach Speed: **50**  
Minor Street: **Hubbard Rd** Lanes: **1** Minor Approach Speed: **45**

MUTCD Electronic Reference to Chapter 4: <http://mutcd.fhwa.dot.gov/pdfs/2009r1r2/part4.pdf>

**WARRANT 6 - COORDINATED SIGNAL SYSTEM**

*Indicate if the criteria are fulfilled in the boxes provided. The warrant is satisfied if either criterion is fulfilled. This warrant should not be applied when the resulting signal spacing would be less than 300 m (1,000 ft.).*

Applicable:  Yes  No  
Satisfied:  Yes  No

Criteria	Fulfilled?	
	Yes	No
1. On a one-way street or a street that has traffic predominately in one direction, the adjacent signals are so far apart that they do not provide the necessary degree of vehicle platooning.		
2. On a two-way street, adjacent signals do not provide the necessary degree of platooning, and the proposed and adjacent signals will collectively provide a progressive operation.		

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City: **Kuna**  
County: \_\_\_\_\_  
District: \_\_\_\_\_

Engineer: **R Beckman**  
Date: **September 24, 2018**

Major Street: **Locust Grove**  
Minor Street: **Hubbard Rd**

Lanes: **1** Major Approach Speed: **50**  
Lanes: **1** Minor Approach Speed: **45**

MUTCD Electronic Reference to Chapter 4: <http://mutcd.fhwa.dot.gov/pdfs/2009r1r2/part4.pdf>

**WARRANT 7 - CRASH EXPERIENCE**

Record hours where criteria are fulfilled, the corresponding volume, and other information in the boxes provided. The warrant is satisfied if all three of the criteria are fulfilled.

Applicable:  Yes  No  
Satisfied:  Yes  No

Criteria	Hour	Volume		Met?		Fulfilled?	
		Major	Minor	Yes	No	Yes	No
1. One of the warrants to the right is met.	Warrant 1, Condition A (80% satisfied)						
	Warrant 1, Condition B (80% satisfied)						
	Warrant 4, Pedestrian Volume at 80% of volume requirements: # ped/hr for four (4) hours or # ped/hr for one (1) hour.						
2. Adequate trial of other remedial measure has failed to reduce crash frequency.	Measure tried:						
3. Five or more reported crashes, of types susceptible to correction by signal, have occurred within a 12-month period.	Observed Crash Types:			Number of crashes per 12 months:			



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City: Kuna  
County: \_\_\_\_\_  
District: \_\_\_\_\_

Engineer: R Beckman  
Date: September 24, 2018

Major Street: Locust Grove Lanes: 1 Major Approach Speed: 50  
Minor Street: Hubbard Rd Lanes: 1 Minor Approach Speed: 45

MUTCD Electronic Reference to Chapter 4: <http://mutcd.fhwa.dot.gov/pdfs/2009r1r2/part4.pdf>

**Approach Lane Criteria**

1. How many approach lanes are there at the track crossing?  
If there is 1 lane, use Figure 4C-9 and if there are 2 or more, use Figure 4C-10.

1     2 or more  
 Fig 4C-9     Fig 4C-10

**WARRANT 9 - INTERSECTION NEAR A GRADE CROSSING**

*This signal warrant should be applied only after adequate consideration has been given to other alternatives or after a trial of an alternative has failed to alleviate the safety concerns associated with the grade crossing.*

Indicate if both criteria are fulfilled in the boxes provided. The warrant is satisfied if both criteria are met.

Applicable:  Yes     No  
Satisfied:  Yes     No

Criteria	Fulfilled?	
	Yes	No
1. A grade crossing exists on an approach controlled by a STOP or YIELD sign and the center of the track nearest to the intersection is within 140 feet of the stop line or yield line on the approach; and	<input type="checkbox"/>	<input type="checkbox"/>
2. During the highest traffic volume hour during which the rail uses the crossing, the plotted point falls above the applicable curve for the existing combination of approach lanes over the track and the distance D (clear storage distance).	<input type="checkbox"/>	<input type="checkbox"/>

Use the following tables (4C-2, 4C-3, and 4C-4 to appropriately adjust the minor-street approach volume).

**Inputs**

Occurrences of Rail traffic per day  
% of High Occupancy Buses on Minor-Street Approach  
Enter D (feet)  
% of Tractor-Trailer Trucks on Minor-Street Approach

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**Adjustment Factors from Tables**

\_\_\_\_\_  
1.00  
\_\_\_\_\_  
0.50

**Table 4C-2. Adjustment Factor for Daily Frequency of Rail Traffic**

Rail Traffic per Day	Adjustment Factor
1	0.67
2	0.91
3 to 5	1.00
6 to 8	1.18
9 to 11	1.25
12 or more	1.33

**Table 4C-3. Adjustment Factor for Percentage of High-Occupancy Buses**

% of High-Occupancy Buses* on Minor Street Approach	Adjustment Factor
0%	1.00
2%	1.09
4%	1.19
6% or more	1.32

\* A high-occupancy bus is defined as a bus occupied by at least 20 people

**Table 4C-4. Adjustment Factor for Percentage of Tractor-Trailer Trucks**

% of Tractor-Trailer Trucks on Minor-Street Approach	Adjustment Factor	
	D less than 70 feet	D of 70 feet or more
0% to 2.5%	0.50	0.50
2.6% to 7.5%	0.75	0.75
7.6% to 12.5%	1.00	1.00
12.6% to 17.5%	2.30	1.15
17.6% to 22.5%	2.70	1.35
22.6% to 27.5%	3.28	1.64
More than 27.5%	4.18	2.09

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Input the major and minor street volumes before adjustment factors are applied

1 Approach Lane		

D (ft) Major Vol. Minor Vol.

After adjustment factors are applied

1 Approach Lane w/Factors		

D (ft) Major Vol. Minor Vol.

Input D and the major and minor street volumes before adjustment factors are applied

2 or more Approach Lanes		

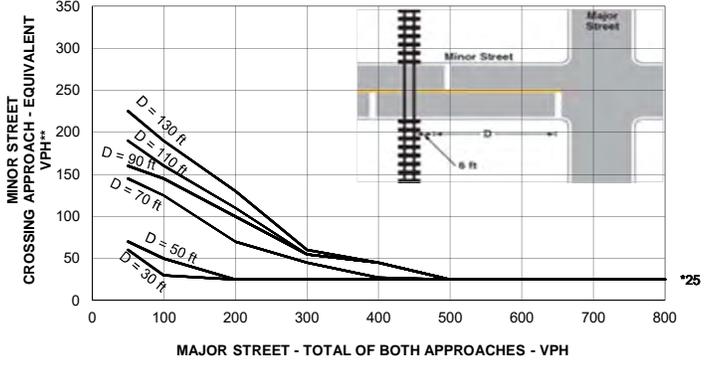
D (ft) Major Vol. Minor Vol.

After adjustment factors are applied

2+ Approach Lane w/Factors		

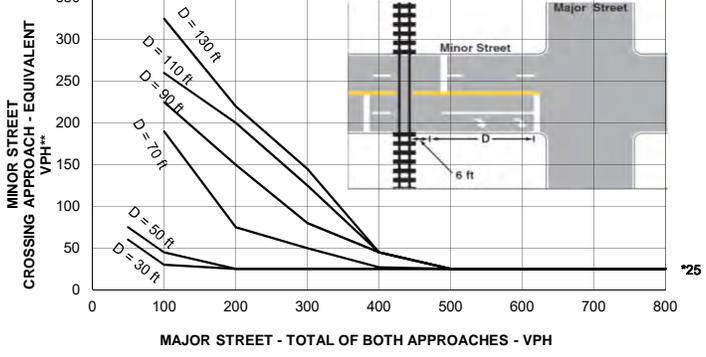
D (ft) Major Vol. Minor Vol.

FIGURE 4C-9: Criteria for 1 Approach Lane at the Track Crossing



\* Note: 25 vph applies as the lower threshold volume  
\* \*Note: VPH after applying the adjustment factors in Tables 4C-2, 4C, and/or 4C-4, if appropriate

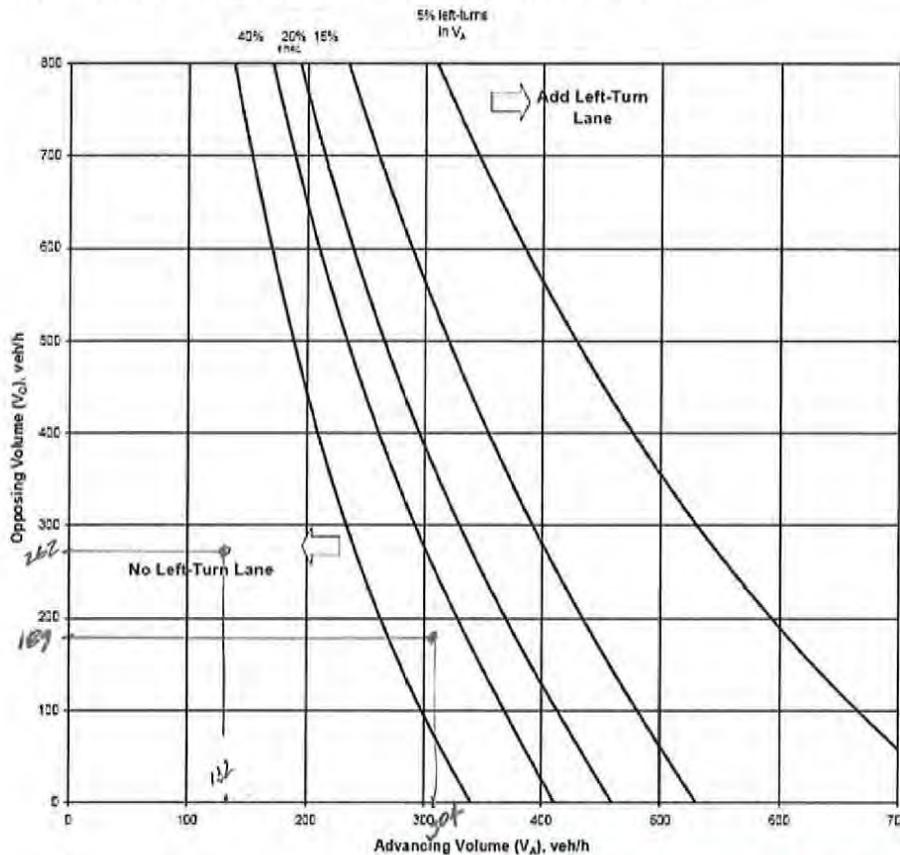
FIGURE 4C-10: Criteria for 2+ Approach Lanes at Track Crossing



\* Note: 25 vph applies as the lower threshold volume  
\* \*Note: VPH after applying the adjustment factors in Tables 4C-2, 4C, and/or 4C-4, if appropriate



**Figure 2 – Left-Turn Lane Guidelines for Two-Lane Roads, 45 mph**



NO LEFT TURN REDD => HUBBARD RD

The following data are required:

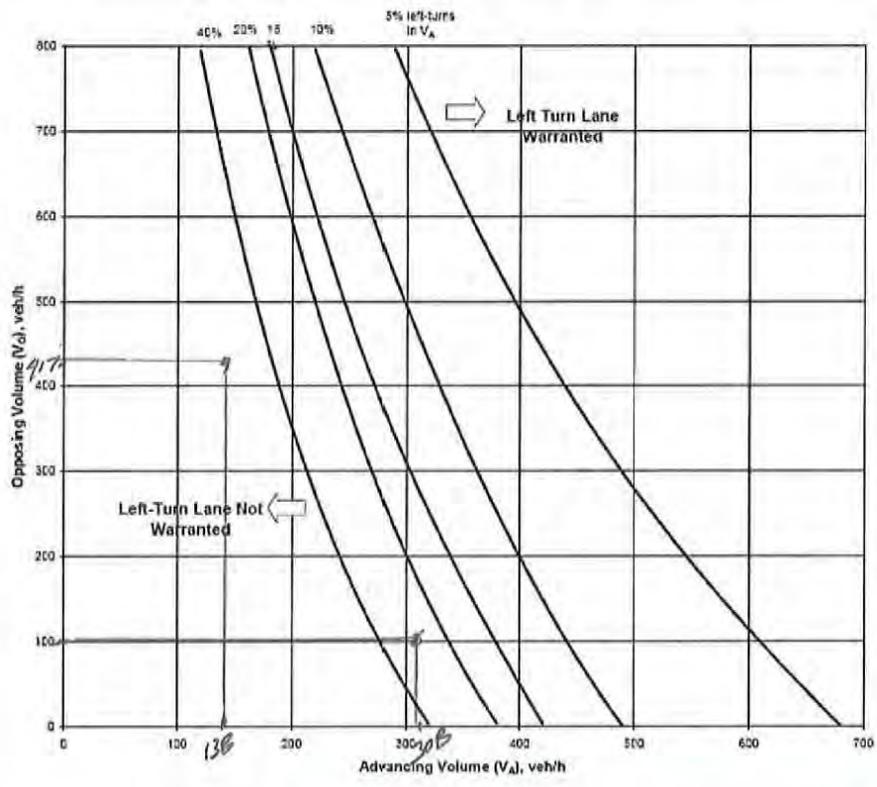
1. Opposing Volume (veh/hr) - VO - The opposing volume is to include only the right-turn and through movements in the opposite direction of the left turning vehicle.
2. Advancing Volume (veh/hr) - VA - The advancing volume is to include the right-turn, left-turn and through movements in the same direction as the left turning vehicle.
3. Operating Speed (mph) - The greatest of anticipated operating speed, measured 85th percentile speed or posted speed.
4. Percentage of left turns in VA 24/304 = 7.9%

Left-turn lane is not needed for left-turn volume less than 10 vph. However, criteria other than volume, such as crash experience, may be used to justify a left-turn lane.

The appropriate trend line is identified on the basis of the percentage of left-turns in the advancing volume, rounded up to the nearest percentage trend line. If the advancing and opposing volume combination intersects above or to the right of this trend line, a left-turn lane is appropriate.

Source: NCHRP Report 279 and 457

Figure 3 – Left-Turn Lane Guidelines for Two-Lane Roads, 50 mph



The following data are required:

*NO LEFT TURN REQ'D EXCEPT GRAVE*

1. Opposing Volume (veh/hr) - VO - The opposing volume is to include only the right-turn and through movements in the opposite direction of the left-turning vehicle.
2. Advancing Volume (veh/hr) - VA - The advancing volume is to include the right-turn, left-turn and through movements in the same direction as the left-turning vehicle.
3. Operating Speed (mph) - The greatest of anticipated operating speed, measured 85th percentile speed or posted speed.
4. Percentage of left-turns in VA *3/308 = 1%*

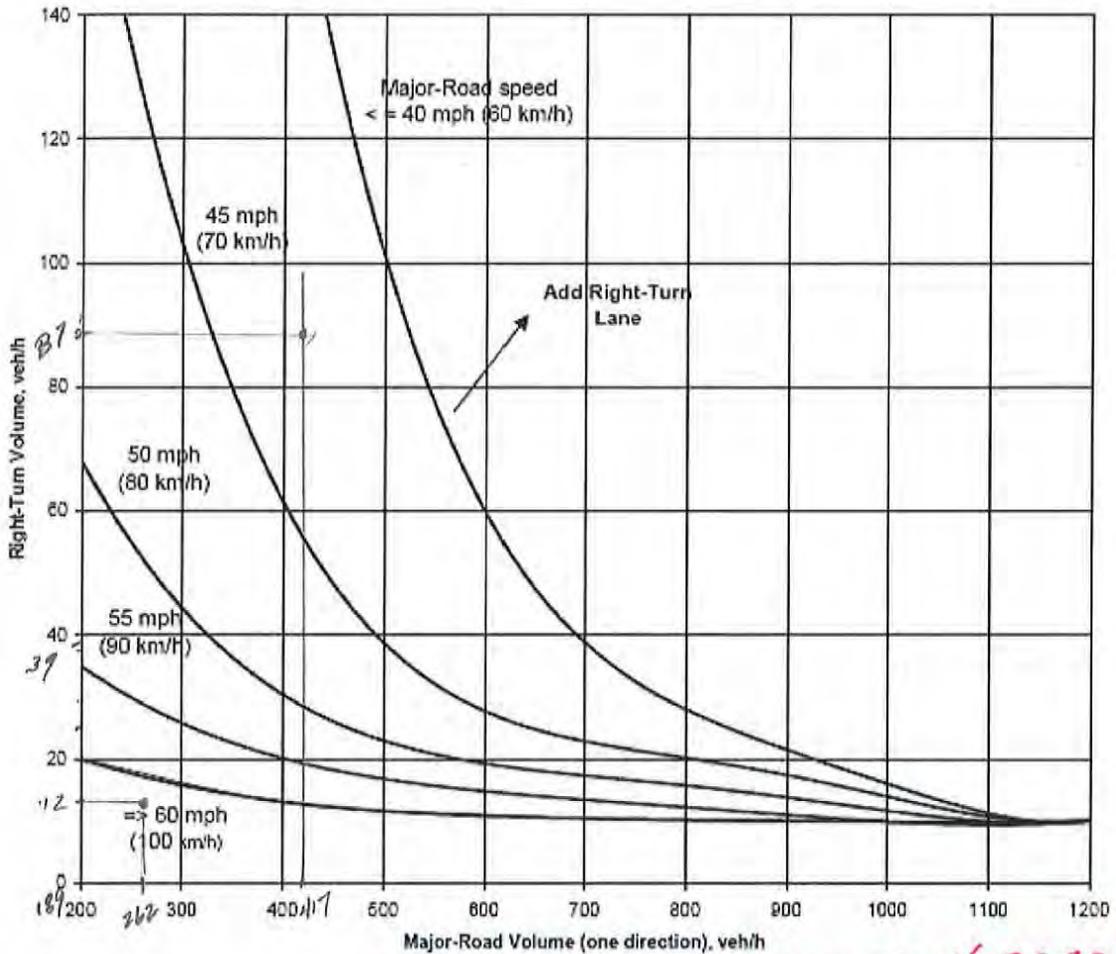
Left-turn lane is not needed for left-turn volume less than 10 vph. However, criteria other than volume, such as crash experience, may be used to justify a left-turn lane.

The appropriate trend line is identified on the basis of the percentage of left-turns in the advancing volume, rounded up to the nearest percentage trend line. If the advancing and opposing volume combination intersects above or to the right of this trend line, a left-turn lane is appropriate.

Source: NCHRP Report 279 and 457

*Hubbard RD  
 Locust Grove SD*

Figure 6 – Right-Turn Lane Guidelines for Two-Lane Roadways



*NO RIGHT TURN LANE => HUBBARD RD  
 RIGHT TURN LANE NEEDED (PT) => LOCUST GROVE*

The following data are required:

1. Advancing Volume (veh/hr) - The advancing volume is to include the right-turn, left-turn and through movements in the same direction as the right-turning vehicle.
2. Right-Turning Volume (veh/hr) - The right-turning volume is the number of advancing vehicles turning right.
3. Operating Speed (mph) - The greatest of anticipated operating speed, measured 85th percentile speed or posted speed.

Note: Right-turn lane is not needed for right-turn volume less than 10 vph. However, criteria other than volume, e.g. crash experience, may be used to justify a right-turn lane.

If the combination of major road approach volume and right-turn volume intersects above or to the right of the speed trend line corresponding to the major road operating speed, then a right-turn lane is appropriate.

Source: NCHRP Report 279 and 457

**Jane Suggs**

---

**From:** Sub Name Mail <subnamemail@adaweb.net>  
**Sent:** Friday, September 07, 2018 3:02 PM  
**To:** Jane Suggs  
**Cc:** Gregory Carter; Cara Duskey  
**Subject:** Ledgestone Subdivision Name Reservation

September 7, 2018

Jane Suggs, WHPacific  
Greg Carter, Idaho Survey Group

RE: Subdivision Name Reservation: **LEDGESTONE SUBDIVISION**

At your request, I will reserve the name **Ledgestone Subdivision** for your project. I can honor this reservation only as long as your project is in the approval process. Final approval can only take place when the final plat is recorded.

This reservation is available for the project as long as it is in the approval process unless the project is terminated by the client, the jurisdiction or the conditions of approval have not been met, in which case the name can be re-used by someone else.

Sincerely,



**Jerry L. Hastings, PLS 5359**  
*County Surveyor*  
**Deputy Clerk Recorder**  
Ada County Development Services  
200 W. Front St., Boise, ID 83702  
(208) 287-7912 office  
(208) 287-7909 fax

---

**From:** Jane Suggs [mailto:JSuggs@whpacific.com]  
**Sent:** Tuesday, September 04, 2018 8:39 AM  
**To:** Sub Name Mail  
**Cc:** Cara Duskey  
**Subject:** Ledgestone Subdivision Name Reservation

Hi Glen,

Using the info listed previously (below and in **bold**), we'd like to request the subdivision name:

**Ledgestone Subdivision**

Still has "stone", but it is not the leading word.

Thank you,  
Jane

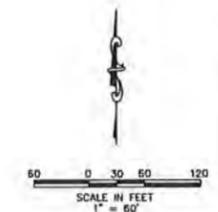
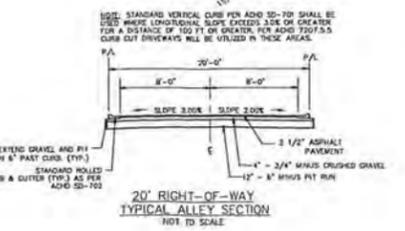
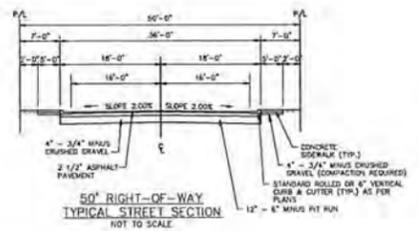
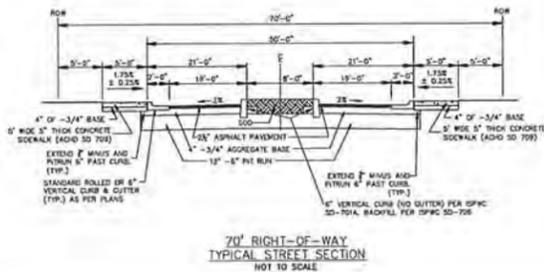
Jane Suggs | WHPacific, Inc.





LINE TABLE		
LINE	LENGTH	BEARING
L1	25.00	N0°34'13"E
L2	31.00	S0°34'13"W
L3	15.51	S89°25'47"E
L4	42.45	N42°31'14"W
L5	62.00	N89°25'47"W
L6	44.88	N55°34'36"W

- PLAT LEGEND**
- LOT NUMBER
  - LOT AREA
  - BLOCK NUMBER
  - FLOW ARROW
  - FIRE HYDRANT
  - CATCH BASIN
  - STREET NAME
  - BOUNDARY
  - LOT LINES
  - ROAD CENTERLINE
  - RIGHT OF WAY
  - EASEMENT
  - SEWER
  - CURB GUTTER SW
  - SEWER LINE
  - WATER LINE
  - STORM DRAIN LINE
  - PRESSURE IRRIGATION
  - GRAVITY IRRIGATION
  - PHASE LINE
  - FEMA FLOOD HAZARD ZONE A



REVISIONS

NO.	DATE	DESCRIPTION

**PRELIMINARY PLAT**  
LEDGESTONE SUBDIVISION  
TRILOGY DEVELOPMENT, INC.

**PP-2**



**Bailey Engineering, Inc.**  
CIVIL ENGINEERING | PLANNING | CADD  
400 N. RANDOLPH LAKE  
BOULEVARD, SUITE 200  
BOULEVARD, SUITE 200  
PROJECT: 2018-058 DATE: 10-01-2018

Parcel Table with columns: Lot, Area, Perimeter. Rows include Block 1 Lot 1 through Block 2 Lot 24.

Parcel Table with columns: Lot, Area, Perimeter. Rows include Block 3 Lot 1 through Block 4 Lot 24.

Parcel Table with columns: Lot, Area, Perimeter. Rows include Block 5 Lot 1 through Block 6 Lot 24.

Parcel Table with columns: Lot, Area, Perimeter. Rows include Block 7 Lot 1 through Block 8 Lot 24.

Parcel Table with columns: Lot, Area, Perimeter. Rows include Block 9 Lot 1 through Block 10 Lot 24.

Parcel Table with columns: Lot, Area, Perimeter. Rows include Block 11 Lot 1 through Block 12 Lot 24.

Parcel Table with columns: Lot, Area, Perimeter. Rows include Block 13 Lot 1 through Block 14 Lot 24.

Parcel Table with columns: Lot, Area, Perimeter. Rows include Block 15 Lot 1 through Block 16 Lot 24.

Curve Table with columns: Curve #, Radius, Length, Chord, Bearing, Delta. Rows include Curve C1 through C119.

Curve Table with columns: Curve #, Radius, Length, Chord, Bearing, Delta. Rows include Curve C120 through C239.

Curve Table with columns: Curve #, Radius, Length, Chord, Bearing, Delta. Rows include Curve C240 through C359.

Curve Table with columns: Curve #, Radius, Length, Chord, Bearing, Delta. Rows include Curve C360 through C479.



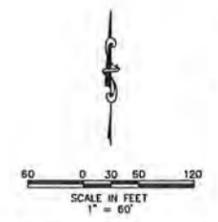
Project information including: REVISED NO. DATE DESCRIPTION, Bailey Engineering, Inc. Civil Engineering | Planning | CADD, and LEGSTONE SUBDIVISION TRILGY DEVELOPMENT, INC.



**PLAT LEGEND**

LOT NUMBER	○
LOT AREA	○
BLOCK NUMBER	○
FLOOR AREA	○
FIRE HYDRANT	○
CATCH BASIN	○
STREET NAME	○
BOUNDARY	○
LOT LINES	○
ROAD CENTERLINE	○
RIGHT OF WAY	○
EASEMENT	○
SEWER	○
CURB OUTER SW	○
SEWER LINE	○
WATER LINE	○
STORM DRAIN LINE	○
PRESSURE PIPES	○
GRANITE PAVEMENT	○
PHASE LINE	○
FEMA FLOOD HAZARD ZONE A	○

BOISE PROJECT BOARD  
OF CONTROL EASEMENT  
25' SOUTH AND 37'  
NORTH FROM MASON  
CREEK CENTERLINE



REVISION	NO. DATE DESCRIPTION

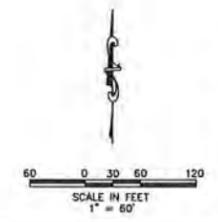
**PROFESSIONAL ENGINEER**  
NRT522  
CONSTRUCTION  
STATE OF IDAHO

**Bailey Engineering, Inc.**  
Civil Engineering | Planning | CADD  
1024 BROOKDALE  
BOISE, ID 83718  
TEL: 208-333-8113  
WWW.BAILEYENGINEERING.COM

DRAWN BY: JAV/WH CHECKED BY: DVID A. WALEY P.E. PROJECT: 2018-008 DATE: 10-01-2018

**CONCEPTUAL ENGINEERING PLAN**  
**LEDGESTONE SUBDIVISION**  
TRILOGY DEVELOPMENT, INC. **PP-4**

LOT NUMBER	①
LOT AREA	②
BLOCK NUMBER	③
FLOW ARROW	④
FIRE HYDRANT	⑤
CATCH BASIN	⑥
STREET NAME	⑦
BOUNDARY	⑧
LOT LINES	⑨
ROAD CENTERLINE	⑩
RIGHT OF WAY	⑪
EASEMENT	⑫
SEWER	⑬
STORM DRAIN SW	⑭
SEWER LINE	⑮
WATER LINE	⑯
STORM DRAIN LINE	⑰
PRESSURE DRAINAGE	⑱
GRAVITY IRRIGATION	⑲
PHASE LINE	⑳
FEMA FLOOD HAZARD ZONE A	㉑



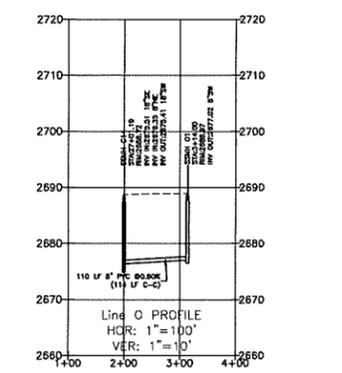
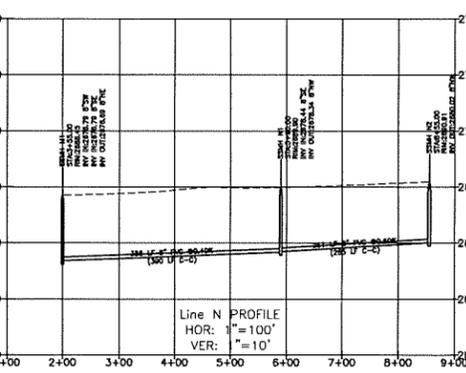
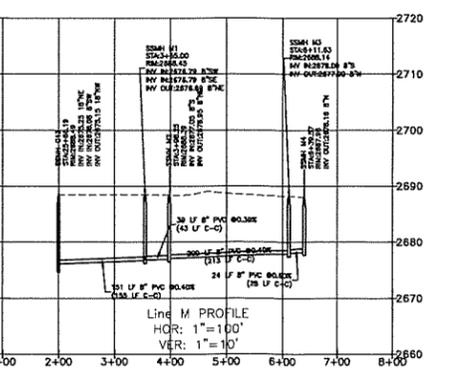
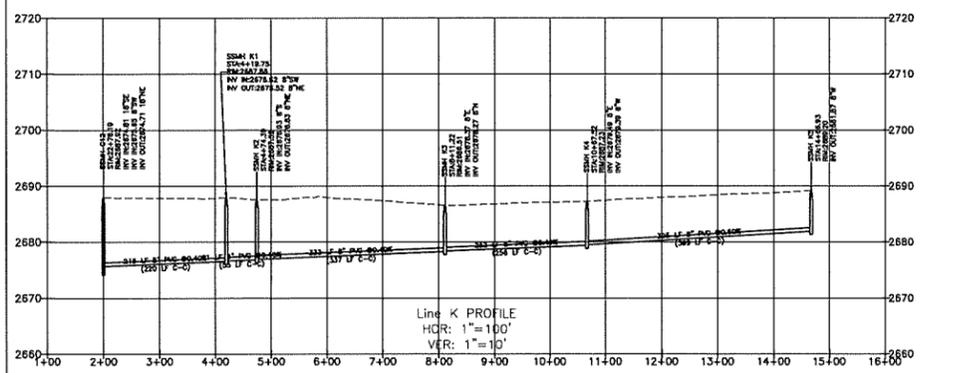
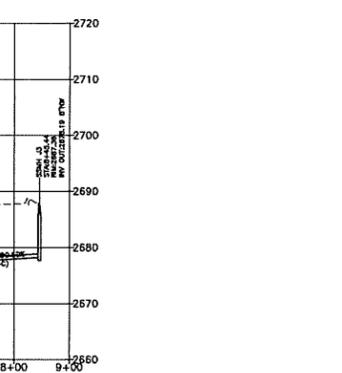
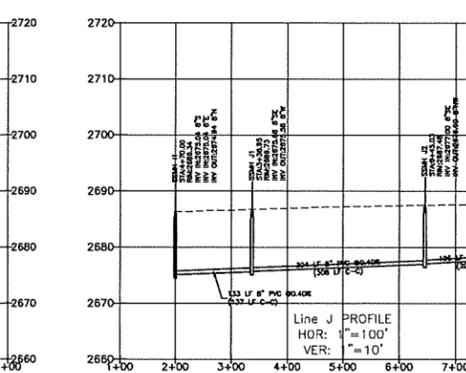
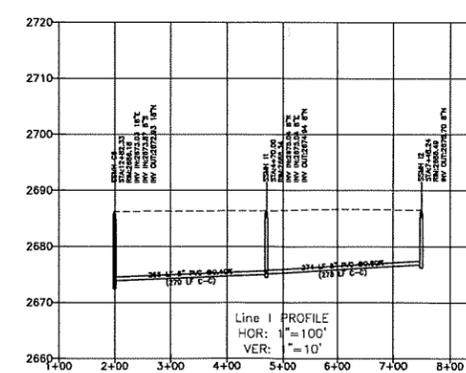
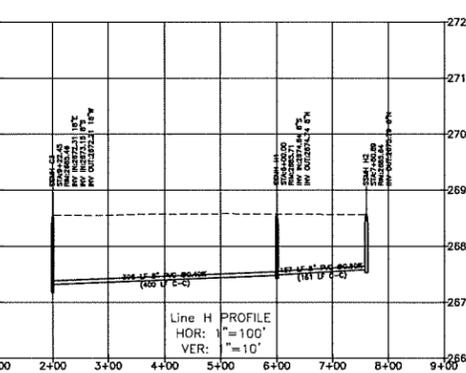
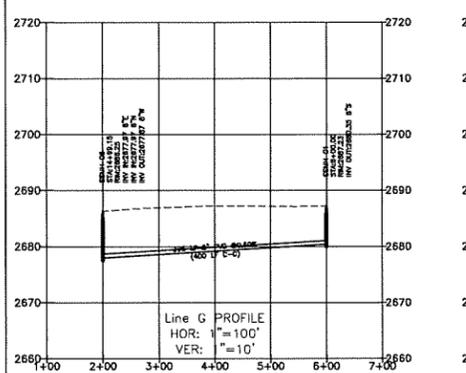
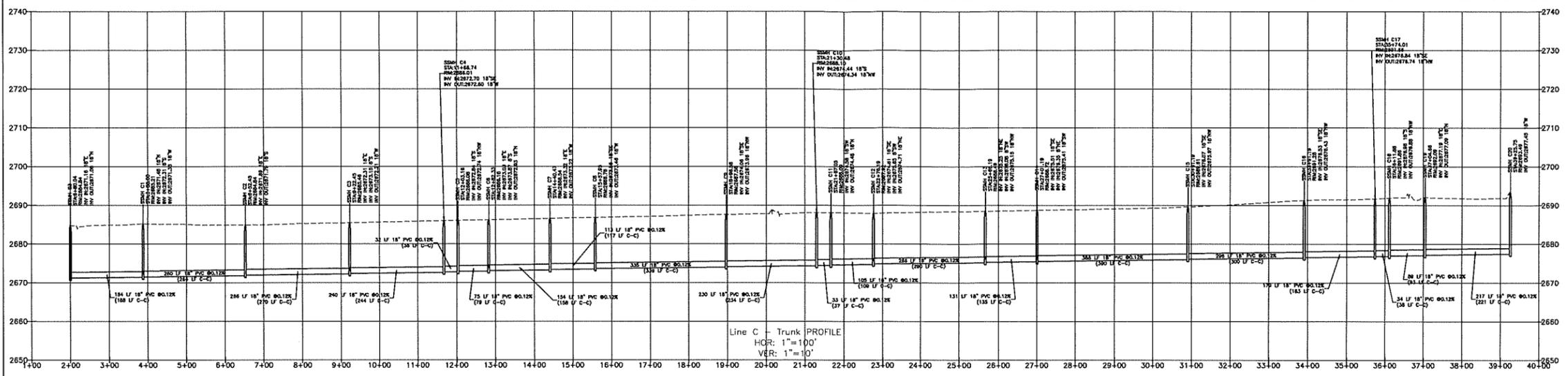
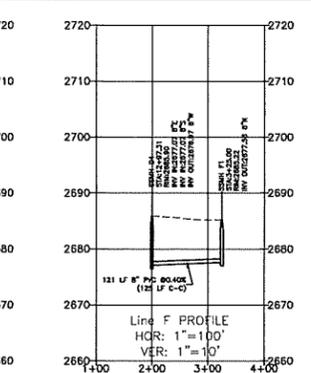
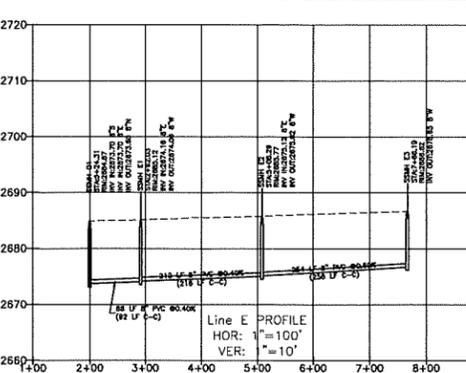
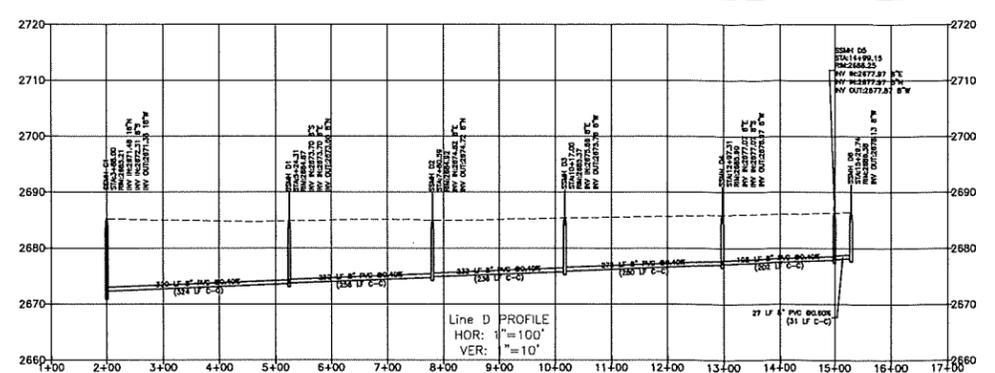
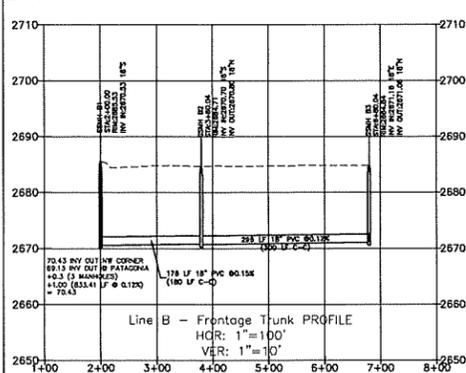
REVISED	DATE	DESCRIPTION

**David A. Bailey**  
 CIVIL ENGINEER | PLANNING | CADD  
 6424 N. KENOSHA AVE. | TEL: 224-844-0111  
 SUITE C, 60114 | CHICAGO, IL 60631 | www.baileyeng.com

PROJECT: C2018-006 | DATE: 10-01-2018

**CONCEPTUAL SEWER PLAN**  
**LEDGESTONE SUBDIVISION**  
**TRILOGY DEVELOPMENT, INC.**

PP-5



REVISION	NO.	DATE	DESCRIPTION

**David A. Bailey**  
Professional Engineer  
No. 15529  
State of Illinois

**Bailey Engineering, Inc.**  
Civil Engineering | Planning | CADD  
4201 BROOKSIDE LANE  
ROSELAND, IL 60511  
TEL: 262-530-0213  
WWW.BAILEYENGINEERING.COM

DRAWN BY: AD | CHECKED BY: DRD A. BAILEY P.E. | PROJECT: C2018-008 | DATE: 10-01-2018

**CONCEPTUAL SEWER PROFILES**  
LEDGESTONE SUBDIVISION  
TRILOGY DEVELOPMENT, INC.

PP-6



# Neighborhood Meeting Certification

CITY OF KUNA PLANNING & ZONING \* 763 W. Avalon, Kuna, Idaho, 83634 \* www.kunacity.id.gov \* (208) 922-5274 \* Fax: (208) 922-5989

## GENERAL INFORMATION:

You must conduct a neighborhood meeting prior to application for variance, conditional use, zoning ordinance map amendment, expansion or extension of a nonconforming use, and/or a subdivision. Please see Section 8-7A-3 of the Kuna City Code or ask one of our planners for more information on neighborhood meetings.

The meeting must be held either on a weekend between 10 a.m. and 7 p.m., or a weekday between 6 p.m. and 8 p.m. Meetings cannot be conducted on holidays, holiday weekends, or the day before or after a holiday or holiday weekend. The meeting must be held at one of the following locations:

- The Subject Property;
- The nearest available public meeting place (Examples include fire stations, libraries and community centers);
- An office space within a 1-mile radius of the subject property.

The meeting cannot take place more than 2 months prior to acceptance of the application and the application will not be accepted before the neighborhood meeting is conducted. You are required to send written notification of your meeting, allowing a reasonable amount of time before your meeting for property owners to plan to attend. Contacting and/or meeting individually with residents will not fulfill Neighborhood Meeting requirements.

You may request a list of the people you need to invite to the neighborhood meeting from our department. This list includes property owners within 300 feet of the subject property. Once you have held your neighborhood meeting, please complete this certification form and include it with your application.

**Please Note: The neighborhood meeting must be conducted in one location for attendance by all neighboring residents. Contacting and/or meeting individually with residents does not comply with the neighborhood meeting requirements.**

**Please include a copy of the sign-in sheet for your neighborhood meeting, so we have written record of who attended your meeting and the letter of intent sent to each recipient. In addition, provide any concerns that may have been addressed by individuals that attended the meeting.**

Description of proposed project: single family subdivision with parks, pathways and open space

Date and time of neighborhood meeting: Monday, July 9, 2018 at 6 pm

Location of neighborhood meeting: on-site

## SITE INFORMATION:

Location: Quarter: \_\_\_\_\_ Section: 18 Township: 2N Range: 1E Total Acres: 60.85

Subdivision Name: Ledgestone Subdivision Lot: \_\_\_\_\_ Block: \_\_\_\_\_

Site Address: Hubbard Road and Locust Grove Rd. Tax Parcel Number(s): S1418121126  
S1418123400

Please make sure to include **all** parcels & addresses included in your proposed use.

## CURRENT PROPERTY OWNER:

Name: TJ Johnson

Address: 2425 N. Locust Grove Road City: Kuna State: ID Zip: 83634

## CONTACT PERSON (Mail recipient and person to call with questions):

Name: Jane Suggs Business (if applicable): WHPacific

Address: 2141 W. Airport Way, Suite 104 City: Boise State: ID Zip: 83705



**PROPOSED USE:**

I request a neighborhood meeting list for the following proposed use of my property (check all that apply):

<b>Application Type</b>	<b>Brief Description</b>
Annexation	<u>annex 60.85 acres into Kuna</u>
Re-zone	<u>rezone property from Ada County zone RR to Kuna zone R-6</u>
Subdivision (Sketch Plat and/or Prelim. Plat)	<u>Preliminary Plat for 253 buildable lots and 44 common lots</u>
Special Use	_____
Variance	_____
Expansion of Extension of a Nonconforming Use	_____
Zoning Ordinance Map Amendment	_____

**APPLICANT:**

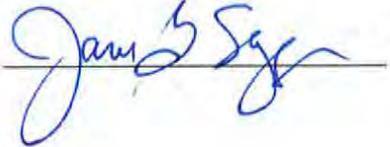
Name: Jane Suggs / WHPacific

Address: 2141 W. Airport Way, Suite 104

City: Boise State: ID Zip: 83705

Telephone: \_\_\_\_\_ Fax: \_\_\_\_\_

I certify that a neighborhood meeting was conducted at the time and location noted on this form and in accord with Section 5-1 A-2 of the Kuna City Code

Signature: (Applicant)  Date 10/2/13



June 27, 2018

Subject: New Subdivision on a portion of the TJ Johnson property  
located west of Locust Grove and south of Mason Creek

Dear Neighbor,

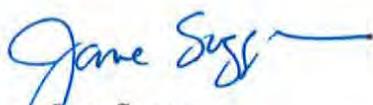
You and your family are invited to attend a neighborhood meeting to discuss a new subdivision proposed for a portion of the TJ Johnson properties shown on the back of this letter

The developer is proposing to annex, rezone and subdivide approximately 59 acres for single family homes, along with open space, a neighborhood park and over 2000 feet of regional pathway along Mason Creek.

Our meeting will be held on **Monday, July 9, 2018, at 6 pm at the project site. Please meet me at the site where Hubbard Road crosses Mason Creek, between Meridian Road and Locust Grove Road.**

If you have any questions about the meeting or the proposed development project, please do not hesitate to call me at 208-275-8729 or e-mail me at [jsuggs@whpacific.com](mailto:jsuggs@whpacific.com).

Sincerely,

  
Jane Suggs  
Planner

TJ Johnson property Subdivision in Kuna  
**Neighborhood Meeting**  
Monday, July 9, 2018  
6 pm

Name Address Email / Phone

option?

- 1. Phil + Margie Schroeder Phil@heritagewriti.com
- 2. DAN & SHIRLEY BROTHAUS d.grothaus@ATT.net
- 3. Way Kahn chuckedhiatt1@outlook.com
- 4. Charles & Hiatt CHUCKEHIATT@OUTLOOK.COM
- 5. Beatty mgbud Beatty@gmail.com
- 6. Stagg Josette Roser Staggjm@gmail.com

- 7. \_\_\_\_\_
- 8. \_\_\_\_\_
- 9. \_\_\_\_\_
- 10. \_\_\_\_\_
- 11. \_\_\_\_\_
- 12. \_\_\_\_\_
- 13. \_\_\_\_\_
- 14. \_\_\_\_\_
- 15. \_\_\_\_\_
- 16. \_\_\_\_\_
- 17. \_\_\_\_\_
- 18. \_\_\_\_\_
- 19. \_\_\_\_\_
- 20. \_\_\_\_\_

Bruce and Robyn Garrett  
2933 N. Locust Grove Rd.  
Kuna, Idaho

Concerning 18-06-AN Annexation & 18-08-s (Preliminary Plat)-Ledgestone Subdivision

Dear Council Members and Planning Division,

We are opposed to having the proposed annexation and plan for the subdivision.

We moved to this area for the specific reason that it was a rural area, and was in the county. We are very concerned that the annexation of this property nulls the county plan of 10 acre properties, and will negatively affect our local rural community.

We enjoy the quiet and peace that living here has afforded us. We have enjoyed being part of the Kuna small town atmosphere. Residents of Kuna are proud of their community, and have moved here for the rural, small town atmosphere.

The proposed plan does not belong in a rural community. Real estate sales are all about location. Row houses in this area will not attract young professionals as it is so far away from the city center. This proposal of apartments, townhouses, and row houses will attract a transient community. This is the type of community that we don't want here. We are afraid this is more a plan of greed, not need.

We don't have the infrastructure to sustain this type of rapid growth. The amount of traffic this proposal will bring to this area cannot be sustained by the current roads, school system, or our police department. Since Patagonia has gone in, we have had a major increase in traffic, and have had three crashes through our fence.

We understand that the sale of farm properties is inevitable, but we are asking that the Planning Commission and the Council Members consider zoning this area more appropriately in order to preserve our rural community.

Sincerely,

*Bruce and Robyn Garrett*  
Bruce and Robyn Garrett

received  
12.11.18

RECEIVED  
12.7.18

---

**JOHN BROWNING- 880 E HUBBARD ROAD, KUNA, IDAHO 83634**

December 7, 2018

Kuna Planning and Zoning Department  
751 W 4th Street  
Kuna, Idaho 83634

Concerning:18-06-AN / 18-08-S  
Dear Chairman of Kuna Planning and Zoning Commission,

Please consider this letter my testimony on the above zoning and annexation request. I am unable to attend this hearing due to prior commitments at this time.

I know that this site would make a great location for a new subdivision and nice addition to the Kuna City. I am concerned about couple issues. The first is the density being requested. The 253 future lots is 4.21 units to the acre. The size of these lots creates a development with very small lots. The small lots in its self is not the issue, the issue is the site improvements the developer is willing to provide for these future home owners. I believe this site should at least match the amenities that other developers in the area have provided in their projects. I would not approve this project without a development agreement that includes these amenities. I have listed some of these below.

- Landscaped setback and buffer on Hubbard that mirrors the already existing projects on Hubbard.
- Limited accesses to Hubbard into the development. ( No more than one and landscaped with water amenities as the existing projects on Hubbard now have.)
- Walking path that matches the walking path of the other developments.
- Pool and Clubhouses for the use of the homeowners in the development.
- Home owners Association that is managed professional by entirety other than the developer. The Association should handle street parking, landscaping issues, funding for common area maintenance and the general upscale homogeneity.

Sincerely yours,

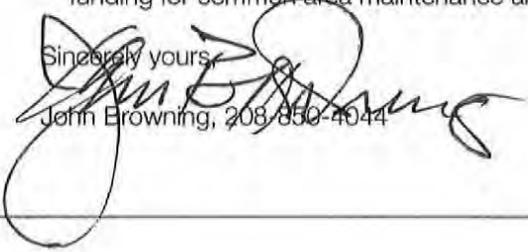
  
John Browning, 208-850-4044

Exhibit  
A3

**Jace Hellman**

---

**From:** Jane Suggs <JSuggs@whpacific.com>  
**Sent:** Monday, January 28, 2019 11:51 AM  
**To:** Jace Hellman  
**Cc:** Wendy Howell; Shawn Brownlee  
**Subject:** Ledgestone curb, gutter and sidewalk

Hi Jace,

So glad Ledgestone got recommended for approval Tuesday night!

There was some question about the curb, gutter and sidewalk for Hubbard and Locust Grove. We have consulted with our engineer and we can construct curb and gutter on Hubbard and Locust Grove and we will move the 8' sidewalk to just inside the right-of-way.

However, please note that this requirement is not only inconsistent with ACHD requirements, but the curb, gutter and 8' sidewalk in the right-of-way is inconsistent with other projects on arterials. For example, see Saranda, Memory Ranch, Springhill, and Grayhawk.

Thanks,  
Jane

**Jane Suggs** | Planner  
**WHPacific, Inc.** | 2141 W Airport Way, Suite 104, Boise, ID 83705  
Direct 208.275.8729 | Mobile 208.602.6941 | Fax 208.342.5353 | [jsuggs@whpacific.com](mailto:jsuggs@whpacific.com)

*Enhancing Client Satisfaction through Creative, Exceptional Service*





**Jace Hellman**

---

**From:** Jace Hellman  
**Sent:** Monday, October 22, 2018 4:34 PM  
**To:** ACHD; Ada County Engineer; Adam Ingram; Attorney Icloud; Becky Rone - Kuna USPS Addressing; Bob Bachman; Boise Project Board of Control (TRitthaler@boiseproject.org); Cable One t.v.; Central District Health Dept. CDHD; COMPASS; DEQ (Alicia.martin@deq.idaho.gov); Eric Adolfson; Idaho Power; Idaho Power; Idaho Power Easements 1; 'Idaho Power Easments 2'; Intermountain Gas; J&M Sanitation - Chad Gordon; jmcdaniel@adaweb.net; Julie Stanely - Regional Address Mgmt.; Ken Couch: Idaho Transportation Department; Kuna Postmaster - Marc C. Boyer; Kuna School District; Kuna School District; Kuna School District; Megan Leatherman; Nampa Meridian Irrigation District; New York Irrigation; Paul Stevens; Perry Palmer; Planning Mgr: Ada County Development Services; Terry Gammel  
**Subject:** Kuna Planning and Zoning Request for Comment - Case No. 18-06-AN (Annexation) & 18-8-S (Preliminary Plat) - LedgeStone Subdivision  
**Attachments:** Agency Packet 10.22.18.pdf

October 22, 2018

Notice is hereby given by the City of Kuna that the following action(s) are under consideration:

<b>File Number &amp; Case Name:</b>	18-06-AN (Annexation) & 18-08-S (Preliminary Plat) - LedgeStone Subdivision.
<b>Project Description</b>	On behalf of Trilogy Development, Inc., Jane Suggs, with WHPacific, requests approval to annex an approximate 60.85-acre parcel into Kuna City Limits with an R-8 zoning designation and to subdivide the 60.85-acres into 298 total lots, consisting of 253 single family buildable lots, and 45 Common lots. The subject site is located at East Hubbard Road, Kuna, ID 83634, within Section 18, Township 2 North, Range 1 East; (APN#'s S1418121126 & S1418123400).
<b>Site Location</b>	East Hubbard Road, Kuna, ID 83634 (East Hubbard Road and North Locust Grove Road).
<b>Representative</b>	<b>WHPacific</b> Jane Suggs 2141 Airport Way, Suite 104 Boise, ID 83705 208-275-8729 <a href="mailto:jsuggs@whpacific.com">jsuggs@whpacific.com</a>
<b>Public Hearing Date</b>	Tuesday, <b>December 11, 2018</b> 6:00 pm Kuna City Hall is located at 751 W. 4 <sup>th</sup> Street, Kuna, ID 83634



<b>Staff Contact</b>	Jace Hellman, Planner II <a href="mailto:jhellman@kunaaid.gov">jhellman@kunaaid.gov</a> Phone: 208.922.5274 Fax: 208.922.5989
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Enclosed is information to assist you with your consideration and response. All comments as to how this action may affect the service(s) your agency provides, is greatly appreciated. Please contact staff with any questions. **If your agency needs different or additional information to review and provide comments please notify our office and they will be sent to you.** If your agency needs additional time for review, please let our office know as soon as possible. *No response within 15 business days will indicate you have no objection or comments for this project.*

Thank you!

Jace Hellman  
 Planner II  
 751 W 4<sup>th</sup> St  
 Kuna, ID 83634  
[jhellman@kunaaid.gov](mailto:jhellman@kunaaid.gov)





STATE OF IDAHO  
DEPARTMENT OF ENVIRONMENTAL QUALITY  
BOISE REGIONAL OFFICE  
1445 North Orchard Street • Boise, ID 83706-2239 • (208) 373-0550

*DEQ Response to Request for Environmental Comment*

Date: November 1, 2018  
Agency Requesting Comments: City of Kuna  
Date Request Received: October 22, 2018  
Applicant/Description: LedgeStone Subdivision 18-06-AN & 18-8-S

Thank you for the opportunity to respond to your request for comment. While DEQ does not review projects on a project-specific basis, we attempt to provide the best review of the information provided. DEQ encourages agencies to review and utilize the Idaho Environmental Guide to assist in addressing project-specific conditions that may apply. This guide can be found at <http://www.deq.idaho.gov/ieg/>.

The following information does not cover every aspect of this project; however, we have the following general comments to use as appropriate:

**1. Air Quality**

- Please review IDAPA 58.01.01 for all rules on Air Quality, especially those regarding fugitive dust (58.01.01.651), trade waste burning (58.01.01.600-617), and odor control plans (58.01.01.776).

The property owner, developer, and their contractor(s) must ensure that reasonable controls to prevent fugitive dust from becoming airborne are utilized during all phases of construction activities per IDAPA 58.01.01.651.

Per IDAPA 58.01.01.600-617, the open burning of any construction waste is prohibited. The property owner, developer, and their contractor(s) are responsible for ensuring no prohibited open burning occurs during construction.

For questions, contact David Luft, Air Quality Manager, at 373-0550.

**2. Wastewater and Recycled Water**

- DEQ recommends verifying that there is adequate sewer to serve this project prior to approval. Please contact the sewer provider for a capacity statement, declining balance report, and willingness to serve this project.
- IDAPA 58.01.16 and IDAPA 58.01.17 are the sections of Idaho rules regarding wastewater and recycled water. Please review these rules to determine whether this or future projects will require DEQ approval. IDAPA 58.01.03 is the section of Idaho rules regarding subsurface disposal of wastewater. Please review this rule to determine whether this or future projects will require permitting by the district health department.

All projects for construction or modification of wastewater systems require preconstruction approval. Recycled water projects and subsurface disposal projects require separate permits as well.



Page 2 of 4

- *DEQ recommends that projects be served by existing approved wastewater collection systems or a centralized community wastewater system whenever possible. Please contact DEQ to discuss potential for development of a community treatment system along with best management practices for communities to protect ground water.*
- *DEQ recommends that cities and counties develop and use a comprehensive land use management plan, which includes the impacts of present and future wastewater management in this area. Please schedule a meeting with DEQ for further discussion and recommendations for plan development and implementation.*

*For questions, contact Todd Crutcher, Engineering Manager, at 373-0550.*

### **3. Drinking Water**

- *DEQ recommends verifying that there is adequate water to serve this project prior to approval. Please contact the water provider for a capacity statement, declining balance report, and willingness to serve this project.*
- *IDAPA 58.01.08 is the section of Idaho rules regarding public drinking water systems. Please review these rules to determine whether this or future projects will require DEQ approval.*

*All projects for construction or modification of public drinking water systems require preconstruction approval.*

- *DEQ recommends verifying if the current and/or proposed drinking water system is a regulated public drinking water system (refer to the DEQ website at <http://www.deq.idaho.gov/water-quality/drinking-water.aspx>). For non-regulated systems, DEQ recommends annual testing for total coliform bacteria, nitrate, and nitrite.*
- *If any private wells will be included in this project, we recommend that they be tested for total coliform bacteria, nitrate, and nitrite prior to use and retested annually thereafter.*
- *DEQ recommends using an existing drinking water system whenever possible or construction of a new community drinking water system. Please contact DEQ to discuss this project and to explore options to both best serve the future residents of this development and provide for protection of ground water resources.*
- *DEQ recommends cities and counties develop and use a comprehensive land use management plan which addresses the present and future needs of this area for adequate, safe, and sustainable drinking water. Please schedule a meeting with DEQ for further discussion and recommendations for plan development and implementation.*

*For questions, contact Todd Crutcher, Engineering Manager at 373-0550.*

### **4. Surface Water**

- *A DEQ short-term activity exemption (STAE) from this office is required if the project will involve de-watering of ground water during excavation and discharge back into surface water, including a description of the water treatment from this process to prevent excessive sediment and turbidity from entering surface water.*
- *Please contact DEQ to determine whether this project will require a National Pollution Discharge Elimination System (NPDES) Permit. If this project disturbs more than one acre, a stormwater permit from EPA may be required.*

- *If this project is near a source of surface water, DEQ requests that projects incorporate construction best management practices (BMPs) to assist in the protection of Idaho's water resources. Additionally, please contact DEQ to identify BMP alternatives and to determine whether this project is in an area with Total Maximum Daily Load stormwater permit conditions.*
- *The Idaho Stream Channel Protection Act requires a permit for most stream channel alterations. Please contact the Idaho Department of Water Resources (IDWR), Western Regional Office, at 2735 Airport Way, Boise, or call 208-334-2190 for more information. Information is also available on the IDWR website at: <http://www.idwr.idaho.gov/WaterManagement/StreamsDams/Streams/AlterationPermit/AlterationPermit.htm>*
- *The Federal Clean Water Act requires a permit for filling or dredging in waters of the United States. Please contact the US Army Corps of Engineers, Boise Field Office, at 10095 Emerald Street, Boise, or call 208-345-2155 for more information regarding permits.*

*For questions, contact Lance Holloway, Surface Water Manager, at 373-0550.*

#### **5. Hazardous Waste And Ground Water Contamination**

- **Hazardous Waste.** *The types and number of requirements that must be complied with under the federal Resource Conservation and Recovery Act (RCRA) and the Idaho Rules and Standards for Hazardous Waste (IDAPA 58.01.05) are based on the quantity and type of waste generated. Every business in Idaho is required to track the volume of waste generated, determine whether each type of waste is hazardous, and ensure that all wastes are properly disposed of according to federal, state, and local requirements.*
- *No trash or other solid waste shall be buried, burned, or otherwise disposed of at the project site. These disposal methods are regulated by various state regulations including Idaho's Solid Waste Management Regulations and Standards, Rules and Regulations for Hazardous Waste, and Rules and Regulations for the Prevention of Air Pollution.*
- **Water Quality Standards.** *Site activities must comply with the Idaho Water Quality Standards (IDAPA 58.01.02) regarding hazardous and deleterious-materials storage, disposal, or accumulation adjacent to or in the immediate vicinity of state waters (IDAPA 58.01.02.800); and the cleanup and reporting of oil-filled electrical equipment (IDAPA 58.01.02.849); hazardous materials (IDAPA 58.01.02.850); and used-oil and petroleum releases (IDAPA 58.01.02.851 and 852).*

*Petroleum releases must be reported to DEQ in accordance with IDAPA 58.01.02.851.01 and 04. Hazardous material releases to state waters, or to land such that there is likelihood that it will enter state waters, must be reported to DEQ in accordance with IDAPA 58.01.02.850.*

- **Ground Water Contamination.** *DEQ requests that this project comply with Idaho's Ground Water Quality Rules (IDAPA 58.01.11), which states that "No person shall cause or allow the release, spilling, leaking, emission, discharge, escape, leaching, or disposal of a contaminant into the environment in a manner that causes a ground water quality standard to be exceeded, injures a beneficial use of ground water, or is not in accordance with a permit, consent order or applicable best management practice, best available method or best practical method."*

Page 4 of 4

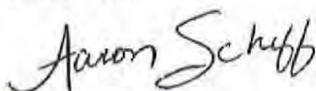
*For questions, contact Albert Crawshaw, Waste & Remediation Manager, at 373-0550.*

**6. Additional Notes**

- *If an underground storage tank (UST) or an aboveground storage tank (AST) is identified at the site, the site should be evaluated to determine whether the UST is regulated by DEQ. EPA regulates ASTs. UST and AST sites should be assessed to determine whether there is potential soil and ground water contamination. Please call DEQ at 373-0550, or visit the DEQ website (<http://www.deq.idaho.gov/waste-mgmt-remediation/storage-tanks.aspx>) for assistance.*
- *If applicable to this project, DEQ recommends that BMPs be implemented for any of the following conditions: wash water from cleaning vehicles, fertilizers and pesticides, animal facilities, composted waste, and ponds. Please contact DEQ for more information on any of these conditions.*

*We look forward to working with you in a proactive manner to address potential environmental impacts that may be within our regulatory authority. If you have any questions, please contact me, or any our technical staff at 208-373-0550.*

Sincerely,



Aaron Scheff  
[aaron.scheff@deq.idaho.gov](mailto:aaron.scheff@deq.idaho.gov)  
Regional Administrator  
Boise Regional Office  
Idaho Department of Environmental Quality

ec: TRIM 2018AEK169



# CENTRAL DISTRICT HEALTH DEPARTMENT

## Environmental Health Division

- Return to:
- ACZ
  - Boise
  - Eagle
  - Garden City
  - Kuna
  - Meridian
  - Star

Rezone # 18-06-AM

Conditional Use # \_\_\_\_\_

Preliminary / Final / Short Plat 18-08-S

Ledgestone

**RECEIVED**  
**NOV 02 2018**  
**CITY OF KUNA**

- 1. We have No Objections to this Proposal.
- 2. We recommend Denial of this Proposal.
- 3. Specific knowledge as to the exact type of use must be provided before we can comment on this Proposal.
- 4. We will require more data concerning soil conditions on this Proposal before we can comment.
- 5. Before we can comment concerning individual sewage disposal, we will require more data concerning the depth of:
  - high seasonal ground water
  - waste flow characteristics
  - bedrock from original grade
  - other \_\_\_\_\_
- 6. This office may require a study to assess the impact of nutrients and pathogens to receiving ground waters and surface waters.
- 7. This project shall be reviewed by the Idaho Department of Water Resources concerning well construction and water availability.
- 8. After written approval from appropriate entities are submitted, we can approve this proposal for:
  - central sewage
  - community sewage system
  - community water well
  - interim sewage
  - central water
  - individual sewage
  - individual water
- 9. The following plan(s) must be submitted to and approved by the Idaho Department of Environmental Quality:
  - central sewage
  - community sewage system
  - community water
  - sewage dry lines
  - central water
- 10. This Department would recommend deferral until high seasonal ground water can be determined if other considerations indicate approval.
- 11. If restroom facilities are to be installed, then a sewage system MUST be installed to meet Idaho State Sewage Regulations.
- 12. We will require plans be submitted for a plan review for any:
  - food establishment
  - swimming pools or spas
  - child care center
  - beverage establishment
  - grocery store
- 13. Infiltration beds for storm water disposal are considered shallow injection wells. An application and fee must be submitted to CDHD.

14. \_\_\_\_\_ Reviewed By: [Signature]  
Date: 10/31/18

Exhibit  
**C3**

# Communities in Motion 2040 Development Review

The Community Planning Association of Southwest Idaho (COMPASS) is the metropolitan planning organization (MPO) for Ada and Canyon Counties. COMPASS has developed this review as a tool for local governments to evaluate whether land developments are consistent with the goals of *Communities in Motion 2040 (CIM 2040)*, the regional long-range transportation plan for Ada and Canyon Counties. This checklist is not intended to be prescriptive, but rather a guidance document based on *CIM 2040 goals*.

**Development Name: Ledgestone (18-06-AN)**

**Agency: Kuna**

**CIM Vision Category: Future Neighborhoods**

**New households: 253**

**New jobs: 0**

**Exceeds CIM forecast: YES**

	<p>CIM Corridor: <b>N/A</b>          Pedestrian level of stress: <b>R—Hubbard Rd</b>          Bicycle level of stress: <b>R—Hubbard Rd</b></p>	<p>Level of Stress considers facility type, number of vehicle lanes, and speed. Roads with <b>G or PG</b> ratings better support bicyclists and pedestrians of all ages and comfort levels.</p>
	<p>Housing within 1 mile: <b>260</b>          Jobs within 1 mile: <b>30</b>          Jobs/Housing Ratio: <b>0.1</b></p>	<p>A good jobs/housing balance – a ratio between <b>1 and 1.5</b> – reduces traffic congestion. Higher numbers indicate the need for more housing and lower numbers indicate an employment need.</p>
	<p>Nearest police station: <b>&gt;4 miles</b>          Nearest fire station: <b>3.2 miles</b></p>	<p>Developments within <b>1.5 miles</b> of police and fire stations ensure that emergency services are more efficient and reduce the cost of these important public services.</p>
	<p>Farmland consumed: <b>Yes</b>          Farmland within 1 mile: <b>1,494 acres</b></p>	<p>Farmland contributes to the local economy, creates additional jobs, and provides food security to the region. Development in farm areas decreases the productivity and sustainability of farmland.</p>
	<p>Nearest bus stop: <b>&gt;4 miles</b>          Nearest public school: <b>2.6 miles</b>          Nearest public park: <b>2 miles</b>          Nearest grocery store: <b>1.9 miles</b></p>	<p>Residents who live or work less than <b>1/2 mile</b> from critical services have more transportation choices. Walking and biking reduces congestion by taking cars off the road, while supporting a healthy and active lifestyle.</p>

## Recommendations

This proposal exceeds growth forecasted for this area. Transportation infrastructure may not be able to support the new transportation demands. This location is still in a largely farmland area. Nearby services, such as schools, parks, emergency services, grocery, and other stores are likely accessed only by vehicle and there are no plans for public transportation to this location in the ValleyConnect 2.0 plan.

The FY2018-2022 Regional Transportation Improvement Program (TIP) identifies the signalization on SH-69 at the intersection of Hubbard Road to improve safety. Additional information about the TIP is available at: <http://www.compassidaho.org/documents/prodserv/trans/FY18/FY2018TIPrpt.pdf>

More information about COMPASS and *Communities in Motion 2040*:  
Web: [www.compassidaho.org](http://www.compassidaho.org)  
Email [info@compassidaho.org](mailto:info@compassidaho.org)  
More information about the development review process:  
<http://www.compassidaho.org/dashboard/devreview.htm>



Exhibit  
C4



ORGANIZED 1904

# Nampa & Meridian Irrigation District

1503 FIRST STREET SOUTH  
FAX #208-463-0092

NAMPA, IDAHO 83651-4395  
nmid.org

OFFICE: Nampa 208-466-7861  
SHOP: Nampa 208-466-0663

October 31, 2018

Jace Hellman, Planner II  
City of Kuna  
751 W. 4<sup>th</sup> St.  
Kuna, ID 83634

RECEIVED  
NOV 05 2018  
CITY OF KUNA

**RE: 18-06-AN & 18-08-S/ Ledgestone Subdivision; E. Hubbard Road**

Dear Jace:

Nampa & Meridian Irrigation District (NMID) has no comment on the above referenced application as it lies outside of our district boundaries. Please contact Thomas Ritthaler, Boise Project- Board of Control, at 208-344-1141 or 2465 Overland Road Room 202 Boise, ID 83705-3173.

All private laterals and waste ways must be protected. All municipal surface drainage must be retained on-site. If any surface drainage leaves the site NMID must review drainage plans. Developers must comply with Idaho Code 31-3805.

Sincerely,

Greg G. Curtis  
Water Superintendent  
Nampa & Meridian Irrigation District

GGC/ce

Cc: Office/ File  
T. Ritthaler, Board of Control



APPROXIMATE IRRIGABLE ACRES  
RIVER FLOW RIGHTS - 23,000  
BOISE PROJECT RIGHTS - 40,000



**RICHARD DURRANT**  
CHAIRMAN OF THE BOARD

**CLINTON PLINE**  
VICE CHAIRMAN OF THE BOARD

**ROBERT D. CARTER**  
PROJECT MANAGER

**THOMAS RITTHALER**  
ASSISTANT PROJECT MANAGER

**APRYL GARDNER**  
SECRETARY-TREASURER

**MARY SUE CHASE**  
ASSISTANT SECRETARY-TREASURER

# BOISE PROJECT BOARD OF CONTROL

(FORMERLY BOISE U.S. RECLAMATION PROJECT)

2465 OVERLAND ROAD  
BOISE, IDAHO 83705-3155

OPERATING AGENCY FOR 167,000  
ACRES FOR THE FOLLOWING  
IRRIGATION DISTRICTS

NAMPA-MERIDIAN DISTRICT  
BOISE-KUNA DISTRICT  
WILDER DISTRICT  
NEW YORK DISTRICT  
BIG BEND DISTRICT

TEL: (208) 344-1141  
FAX: (208) 344-1437

05 November 2018

City of Kuna  
P.O. Box 13  
Kuna, Idaho 83634

RE: Ledgestone Sub  
East Hubbard Rd.  
Boise-Kuna Irrigation District  
Hubbard Lateral 74+10  
Sec. 18, T2N, R1E, BM.

**RECEIVED**

**NOV 08 2018**

**CITY OF KUNA**

**18-06-AN, 18-08-S**

**BK-184A**

Jace Hellman, Planner I:

The United States' Mason Creek Feeder lies within the boundary of the above-mentioned location. The easement for this lateral is held in the name of the United States through the Bureau of Reclamation under the authority of the Act of August 30, 1890. (26 Stat. 391; 43 U.S.C. 945)

The Boise Project Board of Control is contracted to operate and maintain this lateral. We assert the federal easement 37 feet north and 35 feet south of the lateral's centerline. Whereas this area is for the operation and maintenance of our facility, no activity should hinder our ability to do so.

The Boise Project does not approve landscaping including trees and/or shrubs (other than grass or gravel), as this will certainly increase our cost of maintenance. Easement must remain a flat drivable surface.

Please note the appropriate easements on all future preliminary/final plats.

Boise Project Board of Control must also review and approve any final plats prior to recording.

Wording on the preliminary and final recorded plat needs to state that any proposed and/or future usage of the Boise Project Board of Control facilities are subject to Idaho Statues, Title 42-1209.



Project facilities and/or easements that parallel, and are within and/or intended to be within road right-of-ways due to any development of this property must be relocated outside of road right-of-ways. The easements of Boise Project facilities will remain the same unless agreed upon and/or approved with written permission from Boise Project Board of Control.

The construction of any roadway crossings must be conducted only during the non-irrigation season when the lateral is dewatered. In any case no work shall take place within the easement before the proper crossing agreements have been secured through the Bureau of Reclamation and the Boise Project Board of Control.

Utilities planning to cross any project facility must do so in accordance with the master policies now held between the Bureau of Reclamation and most of the utilities. In any case, no work shall take place within the easement before proper crossing agreements have been secured through both the Bureau of Reclamation and the Boise Project Board of Control.

Fencing (as may be required) must be constructed just off the canal easement, to insure public safety and prevent encroachments.

Parking lots, curbing, light poles, signs, etc. and the placing of asphalt and/or cement over Project facility easements must be approved by Boise Project Board of Control prior to construction.

The Boise Project does not approve landscaping (other than grass or gravel) within its easements, as this will certainly increase our cost of maintenance.

Boise Project Board of Control must approve any requests and/or relocation of delivery points prior to construction.

Storm Drainage and/or Street Runoff must be retained on site.

NO DISCHARGE into any live irrigation system is permitted.

Local irrigation/drainage ditches that cross this property, in order to serve neighboring properties, must remain unobstructed and protected by an appropriate easement.

Should there be any small (neighborhood) irrigation ditches on this site, the developers and/or landowners will be obligated to protect them and allow water to pass to downstream neighbors.

This development is subject to Idaho Code 31-3805, in accordance, this office is requesting a copy of the irrigation and drainage plans.

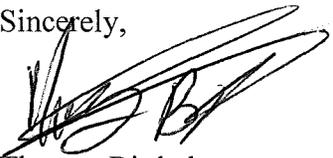
Whereas this property lies within the Bose-Kuna Irrigation District it is important that representatives of this development contact the BKID office as soon as possible to discuss the pressure system prior to any costly design work. If applicable, the irrigation system will have to be built to specific specifications as set by the District / Project.

Boise Project Board of Control must receive a written response from the Boise-Kuna Irrigation District as to who will own and operate the pressure irrigation system prior to review and approval of an irrigation plan by Boise Project Board of Control.

Whereas this development is in its preliminary stages, Boise Project Board of Control reserves the right to review plans and require changes when our easements and/or facilities are affected by unknown factors.

If you have any further questions or comments regarding this matter, please do not hesitate to contact me at (208) 344-1141.

Sincerely,



Thomas Ritthaler  
Assistant Project Manager

tbr/tr

cc: Clint McCormick      Watermaster, Div; 2 BPBC  
    Lauren Boehlke      Secretary – Treasurer, BKID  
    File



CITY OF KUNA

Paul A. Stevens, P.E.

P.O. BOX 13

Kuna City Engineer

KUNA, ID 83634

**PRELIMINARY PLAT REVIEW MEMORANDUM**

**Date:** 6 November 2018  
**From:** Paul A. Stevens, P.E.  
**To:** Wendy Howell, Planning and Zoning Director  
**RE:** Ledgestone Subdivision Application, File Numbers 18-03-S, 18-16-AN, 18-33-DR

---

The City of Kuna Engineering Department has reviewed the Preliminary Plat request of the above applicant dated 4 October 2018. The provided preliminary development plans include 253 single family lots and 45 common lots.

**1. Sanitary Sewer Needs**

- a) The City has sufficient sewer treatment capacity to serve this proposed development. Wastewater flows from this development would receive treatment at the North Wastewater Treatment facility.
- b) This property was not included in Local Improvement District 2006-1. Sewer connection fees were not pre-paid. Consequently, the Ledgestone Subdivision site has no connection fee credits and no reserved sewer treatment capacity. However, there are sufficient treatment connections available at standard rates to serve this site. Developer shall abide by any relevant sewer reimbursement policies, agreements, and any relevant connection fees when connecting to the sewer system.
- c) City Code (6-4-20) requires connection to the City sewer system for all sanitary sewer needs.
- d) A site-specific evaluation will be needed to verify the method of connecting to existing sewer infrastructure.
- e) For any connected load, it is recommended that this application be conditioned to conform to the sewer master plan.
- f) At all reasonable locations where sewer service could be extended to adjoining properties, sewer mains shall be stubbed to the property line or extended in right-of-way in or adjacent to the project. Sewer extensions shall be extended at useable depths such that gravity sewer can be extended to the maximum distance practicable.
- g) For assistance in locating existing facilities and understanding issues associated with connection, please contact the GIS Manager at 287-1726.

**2. Potable Water Needs**

Exhibit  
C7

- a) The City has sufficient potable water supply to serve this development. Water connection fees shall conform to the City's Fee Resolution and Standards Table.
- b) The nearest available water main is located in East Hubbard Road.
- c) City Code (6-4-2X) requires connection to the City water system for all potable water needs.
- d) For any connected load, it is recommended that this application be conditioned to conform to the water master plan.
- e) The minimum water main size shall be 8-inch diameter. Larger sizes may be needed based on site specific conditions and evaluations.
- f) 8-inch diameter (or larger) water mains shall be extended and connected by developer to water trunk lines and mains through all stub and entry-way streets.
- g) The development shall provide all water distribution system improvements necessary to assure adequate fire protection in accordance with applicable codes and agencies.
- h) For assistance in locating existing facilities, please contact the GIS Manager at 287-1726.

### 3. Pressure Irrigation

- a) The applicant's property is not connected to the City of Kuna pressure irrigation system. The nearest pressure main is located in E Hubbard Road.
- b) The property's irrigation needs are presently served by the Boise-Kuna Irrigation District.
- c) Relying on drinking (potable) water for irrigation purposes is contrary to City Code (6-4-2I) and the public interest and is not accounted for in the approved Water Master Plan. It is recommended that this project be conditioned to require connection and annexation to the City Pressure Irrigation system at the time of development.
- d) The development is subject to connection fees based on number of dwellings and lot size for the residential area and based on ultimate landscaped area for the common lots, as provided in City Resolutions.
- e) For any connected load, it is recommended that this application be conditioned to conform to the Pressure Irrigation Master Plan.
- f) It is further recommended that annexation into the municipal irrigation district and pooling of water rights is a requirement at the time of final platting.
- g) It is recommended that conformity with approved City Pressure Irrigation standards is required, including the providing of adequately sized internal and boundary loop lines.

### 4. Grading and Storm Drainage

This development will alter surface grading and features. The following requirements apply:

- a) Runoff from public right-of-way is regulated by ACHD or ITD, depending on the agency responsible for the right-of-way. Engineered design drawings shall conform to the appropriate agency standards.
- b) Any change in quality, quantity or discharge rate of stormwater, compared to historical conditions, must be detained, treated and released at discharge rates and quality equal to or better than historical values. The City of Kuna relies on the ACHD Storm water Policy Manual to establish the design and construction requirements for any private stormwater treatment and disposal system.
- c) The Developer shall provide a map that illustrates the surface and sub-surface water irrigation supply as well as drainage ways that exist on the applicant's property and in the right-of-way adjacent to the proposed development. The map must include 2-foot contours, a layout and essential features of existing irrigation ditches, drainage ditches and pipelines within and adjacent to the proposed development. Note open and piped facilities. Note applicable easements. Include any proposed irrigation system changes.

- d) All upstream drainage rights and downstream water delivery rights shall be preserved as a condition of development. To preserve irrigation rights, constructed facilities must be designed by a licensed professional engineer. Include irrigation plans in the project plan set for review by the City Engineer. Construction shall be high quality, showing good workmanship from materials acceptable to the City Engineer. Facilities provided must be accessible (easements or right-of-way) for continued maintenance, and if necessary, replacement.

## 5. General

- a) With the addition of this property into the corporate limits of the City of Kuna and its potential connection to sewer, water and irrigation services, this property will place demand on constructed facilities and on water rights provided by others. It is the reasonable expectation, in return, that this property transfer to the City of Kuna, at time of connection, any conveyable water rights by deed and "Change of Ownership" form from IDWR. The domestic water right associated solely with a residence and ½ acre or less is not conveyable. The water right held in trust by an irrigation district is also not conveyable.
- b) A plan approval letter will be required if this project affects any local irrigation districts or its facilities. Indian Creek is one of those facilities.
- c) The City reserves the right of prior approval to all agreements involving the applicant (or its successors) and the irrigation or drainage district related to the property of this application and any attempt to abandon surface water rights.
- d) Verify that existing and proposed elevations match at property boundaries such that a slope burden is not imposed on adjacent properties.
- e) State the vertical datum used for elevations on all drawings.
- f) All construction drawings shall be developed and sealed by an Idaho licensed Professional Engineer.
- g) The plat, legal description, easements and associated documents shall be sealed by an Idaho licensed Professional Land Surveyor.
- h) The submittals attached to the application include some alignments for City infrastructure. This information is helpful but has not received final approval. The applicant is advised that detailed plan review occurs upon submittal of the engineered, improvement plans. Approval of the engineered plans follows the review process.

## 6. Inspection Fees

An inspection fee will be required for City inspection of the construction of any public or community water, sewer and irrigation facility associated with this development. The developer will still need a qualified responsible engineer to provide sufficient inspection to justly certify to DEQ that the project was completed in accordance with approved plans and specifications and to provide accurate as-built drawings to the City. The developer's engineer and the City's inspector are permitted to coordinate inspections. The current City inspection fee is \$1.00 per lineal foot of sewer, water and irrigation related pipe and payment is due and payable prior to City's scheduling of a pre-construction conference.

## 7. Right-of-Way

The subject property fronts one section line street.

- a) Sufficient half right-of-way on the quarter line and section line for the classified streets should be provided pursuant to City and ACHD standards.

- b) Residential Easements – City Code (6-3-8) requires 10-foot front and back lot line easements and side-lot easements. More specifically:
  - a. 10-foot minimum subdivision boundary easement – which can be reduced to 5' on the phase lines if there are no irrigation or public utilities within those easements;
  - b. 10-foot minimum street frontage easement for public utilities and irrigation;
  - c. 10-foot back lot line easement inuring to the benefit of the City of Kuna for irrigation;
  - d. 5-foot side lot line irrigation easement;
  - e. Additional easements as needed for facilities not in right-of-way of width and alignment acceptable to the City Engineer.
- c) Approaches onto classified streets shall comply with ACHD approach policies.
- d) Sidewalk, curb and gutter, street widening and any related storm drainage facilities, consistent with city code and policies, shall be provided at the time of land-use change, development or re-development.

## 8. As-Built Drawings

As-built drawings are required at the conclusion of any public facility construction project and are the responsibility of the developer's engineer. The city may help track changes, but will not be responsible for the finished product. As-built drawings will be required before occupancy or final plat approval is granted.

## 9. Phasing of Development

- a) Any phasing plan, to be acceptable, must extend city services, extend transportation facilities and extend other utilities in a manner to maintain reliable service to the buildable lots in the subdivision and not disrupt service to neighboring properties.
- b) A phasing plan must not delay expenditures for infrastructure to burden with expenses in a disproportionate manner the later phases of a project.
- c) Irrespective of compliance with the above conditions, the City in general does not approve or reject phasing plans without the advice and consent of the Planning and Zoning Director.

## 10. Property Description

- a) Provide a metes and bounds legal description of the development property.



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**IDAHO TRANSPORTATION DEPARTMENT**  
P.O. Box 8028 • Boise, ID 83707-2028  
(208) 334-8300 • itd.idaho.gov

December 12, 2018

Jace Hellman  
City of Kuna, Planning and Zoning Department  
751 W. 4<sup>th</sup> St.  
Kuna, ID 83634

**VIA EMAIL**

<b>Development Application</b>	<b>18-06-AN, 18-08-S</b>
<b>Project Name</b>	<b>LEDGESTONE SUBDIVISION</b>
<b>Project Location</b>	South of East Hubbard Road approximately 0.57 miles east of the intersection of SH-69 and East Hubbard Road, east of SH-69 milepost 4.0
<b>Project Description</b>	<ul style="list-style-type: none"> <li>• Annex approximately 60.85 acre-parcel with an R-8 zoning designation and to subdivide the 60.58 acre lot into 298 total lots</li> <li>• Construct a subdivision consisting of 253 single family buildable lots and 45 common lots</li> </ul>
<b>Applicant</b>	Jane Suggs with WHPacific
<b>Representing</b>	Trilogy Development

The Idaho Transportation Department (ITD) reviewed the referenced annexation, zoning, and preliminary plat applications and has the following comments:

1. This project does not abut the State highway system.
2. ITD received and accepted a Traffic Impact Study for Ledgestone Subdivision. As stated in ITD's TIS Acceptance Letter dated December 3, 2018, a northbound right turn deceleration lane on SH-69 at Hubbard Lane is required to mitigate development impacts. A right turn lane warrant was completed for 2025 full buildout. It is anticipated to be needed as soon as the NB right turn movements reach 20 turns in the Peak Hour, potentially in 2020. If applicant chooses to construct his development, adding his development traffic to the intersection, he will be responsible for constructing the necessary improvements (northbound right turn lane) on SH-69 as identified by the TIS.
3. The City is reminded that the SH-69 corridor is already congested. This project will increase the number of vehicle trips in the corridor. As the City continues to add additional trips to the corridor

Exhibit  
08



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through development, the congestion will worsen until the roadway system is ultimately overloaded and fails. ITD has no current funding assigned to mitigate traffic congestion in the SH-69 corridor in this area.

- 4. Idaho Code 40-1910 does not allow advertising within the right-of-way of any State highway.
- 5. IDAPA 39.03.60 rules govern advertising along the State highway system. The applicant may contact Justin Pond, Right-of-Way Section Program Manager, at (208) 334-8832 for more information.
- 6. With the modifications described above, ITD does not object to the proposed preliminary plat application. There is a potential that other proposed developments in the area will also warrant the same right turn lane. The developer has indicated they would be amenable to coordinating with other developments in the area in arriving at an equitable share of construction costs to accommodate this improvement.

If you have any questions, you may contact Ken Couch at (208) 332-7190 or me at (208) 334-8338.

Sincerely,

Sarah Arjona  
Development Services  
[Sarah.Arjona@itd.idaho.gov](mailto:Sarah.Arjona@itd.idaho.gov)





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**IDAHO TRANSPORTATION DEPARTMENT**  
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December 10, 2018

Jace Hellman  
City of Kuna, Planning and Zoning Department  
751 W. 4<sup>th</sup> St.  
Kuna, ID 83634

**VIA EMAIL**

<b>Development Application</b>	<b>18-06-AN, 18-08-S</b>
<b>Project Name</b>	<b>LEDGESTONE SUBDIVISION</b>
<b>Project Location</b>	South of East Hubbard Road approximately 0.57 miles east of the intersection of SH-69 and East Hubbard Road, east of SH-69 milepost 4.0
<b>Project Description</b>	<ul style="list-style-type: none"> <li>• Annex approximately 60.85 acre-parcel with an R-8 zoning designation and to subdivide the 60.58 acre lot into 298 total lots</li> <li>• Construct a subdivision consisting of 253 single family buildable lots and 45 common lots</li> </ul>
<b>Applicant</b>	Jane Suggs with WHPacific
<b>Representing</b>	Trilogy Development

The Idaho Transportation Department (ITD) reviewed the referenced annexation, zoning, and preliminary plat applications and has the following comments:

1. This project does not abut the State highway system.
2. ITD received and accepted a Traffic Impact Study for Ledgestone Subdivision. As stated in ITD's TIS Acceptance Letter dated December 3, 2018, a northbound right turn deceleration lane on SH-69 is required to mitigate development impacts. If applicant chooses to construct his development, adding his development traffic to the intersection, he will be responsible for constructing the necessary improvements (northbound right turn lane) on SH-69 as identified by the TIS.
3. The City is reminded that the SH-69 corridor is already congested. This project will increase the number of vehicle trips in the corridor. As the City continues to add additional trips to the corridor through development, the congestion will worsen until the roadway system is ultimately overloaded





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and fails. ITD has no current funding assigned to mitigate traffic congestion in the SH-69 corridor in this area.

- 4. Idaho Code 40-1910 does not allow advertising within the right-of-way of any State highway.
- 5. IDAPA 39.03.60 rules govern advertising along the State highway system. The applicant may contact Justin Pond, Right-of-Way Section Program Manager, at (208) 334-8832 for more information.
- 6. ITD objects to the proposed application due to traffic concerns and needed mitigation as noted in item 2 above.
- 7. Once the proper ITD permit has been obtained and the required mitigation has been installed, ITD will withdraw any objection to the application. Until such time that these conditions have been met, ITD objects to the application.

If you have any questions, you may contact Ken Couch at (208) 332-7190 or me at (208) 334-8338.

Sincerely,

Sarah Arjona  
Development Services  
[Sarah.Arjona@itd.idaho.gov](mailto:Sarah.Arjona@itd.idaho.gov)





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December 3, 2018

Robert Beckham  
WHPacific, Inc.  
2141 W Airport Way, Ste 104  
Boise, ID 83705

Via Email

RE: Ledgeston Subdivision – ITD Traffic Impact Study Acceptance Letter

Dear Mr. Beckham,

The Idaho Transportation Department (ITD) has reviewed the traffic impact study (TIS) dated October 2018 for the Ledgestone Subdivision. The development is located east of SH-69 on Hubbard Road. The development is not requesting direct access off of the State highway system; however due to the number of trips generated by the development it does have impacts to SH-69. Please find ITD’s position below on required improvements to the transportation system to mitigate development impacts.

Intersection of SH-69 and Hubbard Road

- On SH-69, construct a northbound right turn deceleration lane designed to ITD standards.

If the applicant is in agreement with addressing the items as outlined above then this TIS is acceptable from a state highway access, safety and mobility standpoint. You will need to submit an encroachment application (ITD-2111 form) for work within the right of way. The application will need to include stamped engineered drawings to address the required corridor improvements, a traffic control plan, and \$50 application fee. These items can be submitted to Shona Tonkin at [ITDD3Permits@itd.idaho.gov](mailto:ITDD3Permits@itd.idaho.gov).

This letter only acknowledges the acceptance of the TIS, final approval of the improvements is determined once all documentation has been provided and the permit is signed.



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Maintaining safety and mobility for Idaho’s motorists is of the utmost importance to ITD. If you have any questions please do not hesitate to contact me at [erika.bowen@itd.idaho.gov](mailto:erika.bowen@itd.idaho.gov) or 208-265-4312 extension #7.

Sincerely,

Erika R. Bowen, P.E.  
ITD – District 3 Traffic Technical Engineer

Cc:  
Mindy Wallace – ACHD

751 W 4<sup>th</sup> St  
Kuna, ID 83634  
[jhellman@kunaid.gov](mailto:jhellman@kunaid.gov)



**From:** D3 Development Services <[D3Development.Services@itd.idaho.gov](mailto:D3Development.Services@itd.idaho.gov)>  
**Sent:** Monday, December 10, 2018 12:11 PM  
**To:** Jace Hellman <[jhellman@kunaID.gov](mailto:jhellman@kunaID.gov)>  
**Subject:** 18-06-AN (Annexation) & 18-8-S (Preliminary Plat) - Ledgestone Subdivision

I apologize for the tardiness of our response. I could not find a copy of the application to review. Ken Couch has become one of our Permits Coordinators so please send all development application information to [D3Development.Services@itd.idaho.gov](mailto:D3Development.Services@itd.idaho.gov) so that they may be reviewed within the given time frame. Attached is ITD's response to the application indicated as well as a copy of ITD's TIS Acceptance Letter.

Thank you,

*Sarah Arjona*  
*Development Services*  
*ITD District 3*  
*(208)334-8338*

**From:** Ken Couch <[Ken.Couch@itd.idaho.gov](mailto:Ken.Couch@itd.idaho.gov)>  
**Sent:** Friday, December 7, 2018 5:07 PM  
**To:** D3 Development Services <[D3Development.Services@itd.idaho.gov](mailto:D3Development.Services@itd.idaho.gov)>; Sarah Arjona <[Sarah.Arjona@itd.idaho.gov](mailto:Sarah.Arjona@itd.idaho.gov)>  
**Subject:** FW: [EXTERNAL] Kuna Planning and Zoning Request for Comment - Case No. 18-06-AN (Annexation) & 18-8-S (Preliminary Plat) - Ledgestone Subdivision

*Thanks!*  
*Ken Couch*

Permits Coordinator  
Idaho Transportation Department  
District 3  
208-332-7190 Office  
[Ken.Couch@itd.idaho.gov](mailto:Ken.Couch@itd.idaho.gov)

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---

**From:** Jace Hellman [<mailto:jhellman@kunaID.gov>]  
**Sent:** Monday, October 22, 2018 4:34 PM  
**To:** ACHD; Ada County Engineer; Adam Ingram; Attorney Icloud; Becky Rone - Kuna USPS Addressing; Bob Bachman;

Boise Project Board of Control ([Trithaler@boiseproject.org](mailto:Trithaler@boiseproject.org)); Cable One t.v.; Central District Health Dept. CDHD; COMPASS; DEQ ([Alicia.martin@deq.idaho.gov](mailto:Alicia.martin@deq.idaho.gov)); Eric Adolfson; Idaho Power; Idaho Power; Idaho Power Easements 1; Idaho Power Easements 2; Intermountain Gas; J&M Sanitation - Chad Gordon; [jmcdaniel@adaweb.net](mailto:jmcdaniel@adaweb.net); Julie Stanely - Regional Address Mgmt.; Ken Couch; Kuna Postmaster - Marc C. Boyer; Kuna School District; Kuna School District; Kuna School District; Megan Leatherman; Nampa Meridian Irrigation District; New York Irrigation; Paul Stevens; Perry Palmer; Planning Mgr: Ada County Development Services; Terry Gammel

**Subject:** [EXTERNAL] Kuna Planning and Zoning Request for Comment - Case No. 18-06-AN (Annexation) & 18-8-S (Preliminary Plat) - Ledgestone Subdivision

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October 22, 2018

Notice is hereby given by the City of Kuna that the following action(s) are under consideration:

<b>File Number &amp; Case Name:</b>	18-06-AN (Annexation) & 18-08-S (Preliminary Plat) - Ledgestone Subdivision.
<b>Project Description</b>	On behalf of Trilogy Development, Inc., Jane Suggs, with WHPacific, requests approval to annex an approximate 60.85-acre parcel into Kuna City Limits with an R-8 zoning designation and to subdivide the 60.85-acres into 298 total lots, consisting of 253 single family buildable lots, and 45 Common lots. The subject site is located at East Hubbard Road, Kuna, ID 83634, within Section 18, Township 2 North, Range 1 East; (APN#'s S1418121126 & S1418123400).
<b>Site Location</b>	East Hubbard Road, Kuna, ID 83634 (East Hubbard Road and North Locust Grove Road).
<b>Representative</b>	<b>WHPacific</b> Jane Suggs 2141 Airport Way, Suite 104 Boise, ID 83705 208-275-8729 <a href="mailto:jsuggs@whpacific.com">jsuggs@whpacific.com</a>
<b>Public Hearing Date</b>	Tuesday, <b>December 11, 2018</b> 6:00 pm Kuna City Hall is located at 751 W. 4 <sup>th</sup> Street, Kuna, ID 83634
<b>Staff Contact</b>	Jace Hellman, Planner II <a href="mailto:jhellman@kunaid.gov">jhellman@kunaid.gov</a> Phone: 208.922.5274 Fax: 208.922.5989

Enclosed is information to assist you with your consideration and response. All comments as to how this action may affect the service(s) your agency provides, is greatly appreciated. Please contact staff with any questions. **If your agency needs different or additional information to review and provide comments please notify our office and they will be sent to you.** If your agency needs additional time for review, please let our office know as soon as possible. *No response within 15 business days will indicate you have no objection or comments for this project.*

Thank you!

Jace Hellman  
Planner II  
751 W 4<sup>th</sup> St  
Kuna, ID 83634  
[jhellman@kuna.gov](mailto:jhellman@kuna.gov)



## Jace Hellman

---

**From:** Jane Suggs <JSuggs@whpacific.com>  
**Sent:** Monday, December 10, 2018 12:34 PM  
**To:** Jace Hellman  
**Cc:** 'Shawn Brownlee'; Robert Beckman  
**Subject:** RE: 18-06-AN (Annexation) & 18-8-S (Preliminary Plat) - Ledgestone Subdivision - ITD concerns

Hi Jace,

Bob Beckman in our office is working with Erica Bowen at ITD on the request for a Hwy 69 northbound right turn lane onto Hubbard Road.

Since there are other developments adding to the traffic at the Hwy 69/Hubbard intersection, we are proposing this language (below) in our Transportation Impact Study that is currently under review by ACHD.

***Comment #8 (Generated by ITD) - ITD reviewed the Ledgestone TIS and Addendum dated November 30, 2018. The TIS is missing an ITD right turn lane warrant analysis for Hubbard and SH-69 for the NB direction. Please refer to ITD's Traffic Manual, Figure 3B-1.***

*A warrant analysis, as requested, was completed for the northbound direction at the intersection of SH69 and Hubbard Road. Results indicate that the warrant is met in both the 2025 background and 2025 site plus background conditions. As the Ledgestone development is only adding 3 AM peak hour and 8 PM peak hour trips to this movement, the question was posed to ITD regarding who would be responsible for construction of this northbound turn lane. ITD's response is that the developer would be responsible; however, if other developments in this area also indicate a need for this right turn lane during their TIS review then these other developments would also be responsible if the turn lane is not already constructed. Review with Trilogy indicated that they would be amenable to coordinating with other developments in this area in arriving at an equitable share of construction costs to accommodate this improvement.*

**ITD has approved of this language** (I can send you that email if you need it). So there is not an actual objection, as noted in the December 3 letter from Erica, just a need to address the turn lane in a manner acceptable to ITD.

Call me if you have questions,  
Jane

Jane Suggs | WHPacific, Inc.

## Jace Hellman

---

**From:** Robert Beckman <RBeckman@whpacific.com>  
**Sent:** Monday, December 10, 2018 2:34 PM  
**To:** Jace Hellman  
**Cc:** Jane Suggs  
**Subject:** FW: [EXTERNAL] RE: Ledgestone Subdivision TIS

As requested. Message from Erika Bowen approving our proposed language on comment 8.

Robert Beckman | WHPacific, Inc.

**From:** Erika Bowen <Erika.Bowen@itd.idaho.gov>  
**Sent:** Friday, December 7, 2018 3:03 PM  
**To:** Robert Beckman <RBeckman@whpacific.com>; Mindy Wallace <Mwallace@achdidaho.org>  
**Cc:** Jane Suggs <JSuggs@whpacific.com>; Shawn Brownlee <shawn@trilogyidaho.com>; Aimee Loudenslager <Aloudenslager@achdidaho.org>  
**Subject:** RE: [EXTERNAL] RE: Ledgestone Subdivision TIS

Hi Bob-

That language is acceptable to the department.

Thanks,

*Erika R. Bowen, P.E.*

ITD District 3 Traffic Technical Engineer

**From:** Robert Beckman <RBeckman@whpacific.com>  
**Sent:** Friday, December 7, 2018 10:56 AM  
**To:** Erika Bowen <Erika.Bowen@itd.idaho.gov>; Mindy Wallace <Mwallace@achdidaho.org>  
**Cc:** Jane Suggs <JSuggs@whpacific.com>; Shawn Brownlee <shawn@trilogyidaho.com>; Aimee Loudenslager <Aloudenslager@achdidaho.org>  
**Subject:** RE: [EXTERNAL] RE: Ledgestone Subdivision TIS

Erika –

I was planning to add your comment to the addendum as follows:

***Comment #8 (Generated by ITD) - ITD reviewed the Ledgestone TIS and Addendum dated November 30, 2018. The TIS is missing an ITD right turn lane warrant analysis for Hubbard and SH-69 for the NB direction. Please refer to ITD's Traffic Manual, Figure 3B-1.***

A warrant analysis, as requested, was completed for the northbound direction at the intersection of SH69 and Hubbard Road. Results indicate that the warrant is met in both the 2025 background and 2025 site plus background conditions. As the Ledgestone development is only adding 3 AM peak hour and 8 PM peak hour trips to this movement, the question was posed to ITD regarding who would be responsible for construction of this northbound turn lane. ITD's response is that the developer would be responsible; however, if other developments in this area also indicate a need for this right turn lane during their TIS review then these other developments would also be responsible if the turn lane is not already constructed. Review with Trilogy indicated that they would

be amenable to coordinating with other developments in this area in arriving at an equitable share of construction costs to accommodate this improvement.

Let me know if this is acceptable and I will revise the Addendum to capture this additional review.

Robert Beckman | WHPacific, Inc.

**From:** Erika Bowen <Erika.Bowen@itd.idaho.gov>  
**Sent:** Tuesday, December 4, 2018 7:33 AM  
**To:** Robert Beckman <RBeckman@whpacific.com>; Mindy Wallace <Mwallace@achdidaho.org>  
**Cc:** Jane Suggs <JSuggs@whpacific.com>; Shawn Brownlee <shawn@trilogyidaho.com>; Aimee Loudenslager <Aloudenslager@achdidaho.org>  
**Subject:** RE: [EXTERNAL] RE: Ledgestone Subdivision TIS

Hi Bob-

If you straight line the NB right turn traffic volumes between 2018 to 2025 you come up with the turn lane being warranted in 2020. This assumes Ledgestone generates turning movements starting in 2019, which might not be the case. We'd need to determine the estimated year the turn lane is needed and require construction to coincide.

Patagonia is the other large subdivision in review right now that will probably trigger warrants. They just submitted revisions to their TIS so I'm not certain of when their analysis shows the turn lane is warranted.

Thanks,

*Erika R. Bowen, P.E.*

ITD District 3 Traffic Technical Engineer

**From:** Robert Beckman <RBeckman@whpacific.com>  
**Sent:** Monday, December 3, 2018 2:48 PM  
**To:** Erika Bowen <Erika.Bowen@itd.idaho.gov>; Mindy Wallace <Mwallace@achdidaho.org>  
**Cc:** Jane Suggs <JSuggs@whpacific.com>; Shawn Brownlee <shawn@trilogyidaho.com>; Aimee Loudenslager <Aloudenslager@achdidaho.org>  
**Subject:** RE: [EXTERNAL] RE: Ledgestone Subdivision TIS

Erika –

I have spoken to the developer about this. A followup question.....would this right turn lane be required when construction commences, or at full buildout (2025)? I think Trilogy would like to engage in a conversation with other developers in this area to see if an equitable solution could be agreed to. In this regard, could we offer the additional right turn analysis without a full commitment of payment at this point? Please confirm.

Thanks,

Bob Beckman | WHPacific, Inc.

**From:** Erika Bowen <Erika.Bowen@itd.idaho.gov>  
**Sent:** Monday, December 3, 2018 3:00 PM  
**To:** Robert Beckman <RBeckman@whpacific.com>; Mindy Wallace <Mwallace@achdidaho.org>  
**Cc:** Jane Suggs <JSuggs@whpacific.com>; Shawn Brownlee <shawn@trilogyidaho.com>; Aimee Loudenslager <Aloudenslager@achdidaho.org>  
**Subject:** RE: [EXTERNAL] RE: Ledgestone Subdivision TIS

Hi Bob-

The developer would be responsible regardless of whether it meets in background.

I have not issued any other TIS acceptance letters that require this improvement as part of their development. However, ITD is still in the process of reviewing some for this area. If any of those developments show they warrant a right turn lane, we would inform them that the turn lane needs to be built prior to occupation if it hasn't already been built by Ledgestone.

ITD cannot force developer's to jointly pay for overlapping improvements nor do we have a mechanism to collect impact fees from each to pay for a portion. It has happened in the past that several developments will all go through the TIS acceptance phase and then stall...each hoping the other will move forward in the permit process first and be responsible for fronting the entire cost of the improvement.

The most that ITD could do is bring developers together to facilitate your conversation.  
Let me know how I can help.

Thanks,

*Erika R. Bowen, P.E.*

ITD District 3 Traffic Technical Engineer

**From:** Robert Beckman <[RBeckman@whpacific.com](mailto:RBeckman@whpacific.com)>  
**Sent:** Monday, December 3, 2018 1:48 PM  
**To:** Erika Bowen <[Erika.Bowen@itd.idaho.gov](mailto:Erika.Bowen@itd.idaho.gov)>; Mindy Wallace <[Mwallace@achdidaho.org](mailto:Mwallace@achdidaho.org)>  
**Cc:** Jane Suggs <[JSuggs@whpacific.com](mailto:JSuggs@whpacific.com)>; Shawn Brownlee <[shawn@trilogyidaho.com](mailto:shawn@trilogyidaho.com)>; Aimee Loudenslager <[Aloudenslager@achdidaho.org](mailto:Aloudenslager@achdidaho.org)>  
**Subject:** RE: [EXTERNAL] RE: Ledgestone Subdivision TIS

Thanks Erika. I did look at the warrant as requested (see attached). It appears as though the warrant is met in both the 2025 background and 2025 site plus background. Under this scenario would the development (Ledgestone) still be responsible for the NB right turn lane? I know there are a few other proposed developments in this area including Patagonia, that ought to share in this cost if that is the case. Ledgestone is only adding 3 AM and 8 PM NB right turn trips. Please confirm and I'll add this discussion to the Addendum submitted to ACHD last Friday.

Thanks,

Bob Beckman | WHPacific, Inc.

**From:** Erika Bowen <[Erika.Bowen@itd.idaho.gov](mailto:Erika.Bowen@itd.idaho.gov)>  
**Sent:** Monday, December 3, 2018 12:04 PM  
**To:** Mindy Wallace <[Mwallace@achdidaho.org](mailto:Mwallace@achdidaho.org)>; Robert Beckman <[RBeckman@whpacific.com](mailto:RBeckman@whpacific.com)>  
**Cc:** Jane Suggs <[JSuggs@whpacific.com](mailto:JSuggs@whpacific.com)>; Shawn Brownlee <[shawn@trilogyidaho.com](mailto:shawn@trilogyidaho.com)>; Aimee Loudenslager <[Aloudenslager@achdidaho.org](mailto:Aloudenslager@achdidaho.org)>  
**Subject:** RE: [EXTERNAL] RE: Ledgestone Subdivision TIS

Hi Bob-

ITD reviewed the Ledgestone TIS and Addendum. The TIS is missing an ITD right turn lane warrant analysis for Hubbard and SH-69 for the NB direction. Please refer to ITD's traffic manual, figure 3B-1.

[http://apps.itd.idaho.gov/apps/manuals/Traffic\\_Manual.pdf](http://apps.itd.idaho.gov/apps/manuals/Traffic_Manual.pdf) . I took a look at the chart and am fairly certain the movement will meet turn lane warrants. Plus the v/c for 2025 build out in the NB direction is over a 1.0 in the AM peak. A dedicated right turn lane will help with some of that.

I need the turn lane warrant analysis to be included in the formal TIS documentation. I'll write up ITD's response letter and get it over to you today that the development will be responsible for constructing a NB right turn lane at the intersection of Hubbard and SH-69.

Thanks,

*Erika R. Bowen, P.E.*

ITD District 3 Traffic Technical Engineer

**From:** Mindy Wallace <[Mwallace@achdidaho.org](mailto:Mwallace@achdidaho.org)>

**Sent:** Monday, December 3, 2018 8:59 AM

**To:** [rbeckman@whpacific.com](mailto:rbeckman@whpacific.com)

**Cc:** Jane Suggs <[JSuggs@whpacific.com](mailto:JSuggs@whpacific.com)>; Shawn Brownlee <[shawn@trilogyidaho.com](mailto:shawn@trilogyidaho.com)>; Erika Bowen <[Erika.Bowen@itd.idaho.gov](mailto:Erika.Bowen@itd.idaho.gov)>; Aimee Loudenslager <[Aloudenslager@achdidaho.org](mailto:Aloudenslager@achdidaho.org)>

**Subject:** [EXTERNAL] RE: Ledgestone Subdivision TIS

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Hi Bob,

ACHD won't begin the review of the preliminary plat until we have accepted the traffic impact study. Aimee is out this week and your response will be formally reviewed next week.

Please let me know if you have any other questions.

Mindy

Mindy Wallace, AICP

Planner III

Ada County Highway District

208-387-6178

**From:** Robert Beckman <[RBeckman@whpacific.com](mailto:RBeckman@whpacific.com)>

**Sent:** Friday, November 30, 2018 3:55 PM

**To:** Mindy Wallace <[Mwallace@achdidaho.org](mailto:Mwallace@achdidaho.org)>

**Cc:** Jane Suggs <[JSuggs@whpacific.com](mailto:JSuggs@whpacific.com)>; Shawn Brownlee <[shawn@trilogyidaho.com](mailto:shawn@trilogyidaho.com)>; Erika Bowen <[Erika.Bowen@itd.idaho.gov](mailto:Erika.Bowen@itd.idaho.gov)>; Aimee Loudenslager <[Aloudenslager@achdidaho.org](mailto:Aloudenslager@achdidaho.org)>

**Subject:** RE: Ledgestone Subdivision TIS

Mindy –

The attached Addendum is provided in response to your TIS comments of Nov 16, 2018.

Due to timing of the Planning and Zoning hearing on Dec 11, it is desired that the ACHD staff report be finalized by next Wednesday, Dec 5. Hopefully, this will allow you sufficient time to meet this objective.

Thank you,

Bob Beckman | WHPacific, Inc.

**From:** Mindy Wallace <[Mwallace@achdidaho.org](mailto:Mwallace@achdidaho.org)>  
**Sent:** Friday, November 16, 2018 11:24 AM  
**To:** Robert Beckman <[RBeckman@whpacific.com](mailto:RBeckman@whpacific.com)>  
**Cc:** Jane Suggs <[JSuggs@whpacific.com](mailto:JSuggs@whpacific.com)>; Shawn Brownlee <[shawn@trilogyidaho.com](mailto:shawn@trilogyidaho.com)>  
**Subject:** LedgeStone Subdivision TIS

Bob,

Attached are ACHD's comments on the LedgeStone Subdivision TIS.

Please let me know if you have any questions.

Mindy

Mindy Wallace, AICP  
Planner III  
Ada County Highway District  
208-387-6178

Development Services Department



**Project/File:** Ledge Stone Subdivision/ KPP18-0011/ 18-08-S, 18-06-AN, 18-33-DR  
*This is an annexation, rezone, and preliminary plat application to allow for the development of a 298-lot subdivision consisting of 253 residential lots and 45 common lots on approximately 61 acres.*

**Lead Agency:** City of Kuna

**Site address:** 2425 N. Locust Grove Road

**Staff Approval:** January 14, 2019

**Applicant:** Trilogy Development  
Shawn Brownlee  
9839 Cable Car Street, STE 101  
Boise, ID 83709

**Representative:** Jane Suggs  
WH Pacific  
2141 W. Airport Way, STE 104  
Boise, ID 83705

**Staff Contact:** Mindy Wallace, AICP  
Phone: 387-6178  
E-mail: [mwallace@achdidaho.org](mailto:mwallace@achdidaho.org)

**A. Findings of Fact**

- 1. **Description of Application:** This is an annexation, rezone, and preliminary plat application to allow for the development of a 298-lot subdivision consisting of 253 residential lots and 45 common lots on approximately 61 acres.

The City of Kuna's Comprehensive Plan identifies this site as medium density residential.

- 2. **Description of Adjacent Surrounding Area:**

Direction	Land Use	Zoning
North	Single family/rural residential	R-6/RR
South	Rural residential	RR
East	Rural residential	RR
West	Rural residential	RR

- 3. **Site History:** ACHD has not previously reviewed this site for a development application.
- 4. **Adjacent Development:** The following developments are pending or underway in the vicinity of the site:



- Patagonia Subdivision, consisting of 470 buildable lots and 18 common lots, is located north of the site and was approved by ACHD in August of 2014.
5. **Transit:** Transit services are not available to serve this site.
  6. **Pathway Crossings:** United States Access Board R304.5.1.2 Shared Use Paths. In shared use paths, the width of curb ramps runs and blended transitions shall be equal to the width of the shared use path.

AASHTO's Guidelines for the Development of Bicycle Facilities 5.3.5 Other Intersection Treatments: The opening of a shared use path at the roadway should be at least the same width as the shared use path itself. If a curb ramp is provided, the ramp should be the full width of the path, not including any flared sides if utilized. . . . Detectable warnings should be placed across the full width of the ramp.

FHWA's "Designing Sidewalks and Trails for Access" (1999) reflected common ADA-related concepts: Chapter 6, Page 16-6: The width of the ramp should be at least as wide as the average width of the trail to improve safety for users who will be traveling at various speeds. In addition, the overall width of the trail should be increased, so the curb ramp can be slightly offset to the side. The increased width reduces conflict at the intersection by providing more space for users at the bottom of the ramp.

7. **New Center Lane Miles:** The proposed development includes 2.2 centerline miles of new public road.
8. **Impact Fees:** There will be an impact fee that is assessed and due prior to issuance of any building permits. The assessed impact fee will be based on the impact fee ordinance that is in effect at that time. The impact fee assessment will not be released until the civil plans are approved by ACHD.
9. **Capital Improvements Plan (CIP)/ Integrated Five Year Work Plan (IFYWP):**
  - The intersection of SH-69 and Hubbard Road is listed in the CIP to be widened to 6-lanes on the north leg, 5-lanes on the south, 4-lanes east, and 3-lanes on the west leg, and signalized between 2031 and 2035.

## **B. Traffic Findings for Consideration**

1. **Trip Generation:** This development is estimated to generate 2,398 vehicle trips per day; 253 vehicle trips per hour in the PM peak hour, based on the traffic impact study.

2. **Traffic Impact Study**

WHPacific, Inc. prepared a traffic impact study for the proposed Ledgestone Subdivision. Below is an executive summary of the findings as presented by WHPacific, Inc. ACHD has reviewed the submitted traffic impact study for consistency with ACHD policies and practices and may have additional requirements beyond what is noted in the summary. ACHD Staff comments on the submitted traffic impact study can be found below under staff comments.

The traffic impact study notes that under existing conditions (2018) all study roadway segments and intersections operate at an acceptable level of service.

Under 2025 background traffic conditions all study roadway segments and intersections operate at an acceptable level of service, with the exception of the Locust Grove/Columbia Road intersection.

- To mitigate the traffic impacts the installation of a signal or roundabout is recommended.

Under 2025 total traffic conditions all conditions all study roadway segments and intersections operate at an acceptable level of service, with the exception of the Locust Grove/Columbia Road intersection.

- To mitigate the traffic impacts the installation of a signal or roundabout is recommended at the Locust Grove/Columbia Road intersection.
- The Locust Grove/Hubbard Road intersection should be converted to an all-way stop intersection when 75% of the site is developed (224 lots).
- A dedicated right turn lane is warranted on Locust Grove Road at Rio Vallegas Street.

**Policy:**

**Level of Service Planning Thresholds:** District Policy 7205.3.1 states that, Level of Service Planning Thresholds have been established for principal arterials and minor arterials within ACHD’s Capital Improvement Plan and are also listed in section 7106. Unless otherwise required to provide a Traffic Impact Study under section 7106, a proposed development with site traffic less than 10% of the existing downstream roadway or intersection peak hour traffic shall not be required to provide mitigation for a roadway or intersection that currently exceeds the minimum acceptable level of service planning threshold or V/C ratio.

**Staff Comments/Recommendations:** The traffic impact study notes that the site traffic is approximately 6.8% of the total PM peak hour traffic entering the Locust Grove Road/Columbia Road intersection. Therefore, consistent with District policy 7205.3.1 Level of Service Planning Threshold, which states, a proposed development with site traffic less than 10% of the existing downstream roadway or intersection peak hour traffic shall not be required to provide mitigation; no improvements are required at this intersection.

Consistent with the finding and recommendations of the traffic impact study, the applicant should be required to convert the Locust Grove Road/Hubbard Road intersection to an all-way stop control intersection prior to plan approval and ACHD’s signature on the final plat that contains the 225 lot.

The applicant should be required to construct a dedicated right turn lane on Locust Grove Road at Rio Vallegas Street, when Rio Vallegas Street is constructed and intersects the roadway.

**3. Condition of Area Roadways**

Traffic Count is based on Vehicles per hour (VPH)

Roadway	Frontage	Functional Classification	PM Peak Hour Traffic Count	PM Peak Hour Level of Service
Hubbard Road	865-feet	Minor Arterial	47	Better than “E”
Locust Grove Road	235-feet	Minor Arterial	71	Better than “E”

\* Acceptable level of service for a two-lane minor arterial is “E” (575 VPH).

**4. Average Daily Traffic Count (VDT)**

*Average daily traffic counts are based on ACHD’s most current traffic counts.*

- The average daily traffic count for Hubbard Road west of Locust Grove Road was 628 on 8/8/18.
- The average daily traffic count for Locust Grove Road north of Hubbard Road was 1,009 on 8/8/18.

## C. Findings for Consideration

### 1. Hubbard Road

a. **Existing Conditions:** Hubbard Road is improved with 2-travel lanes and no curb, gutter or sidewalk abutting the site. There is 50-feet of right-of-way for Hubbard Road (25-feet from centerline).

b. **Policy:**

**Arterial Roadway Policy:** District Policy 7205.2.1 states that the developer is responsible for improving all street frontages adjacent to the site regardless of whether or not access is taken to all of the adjacent streets.

**Master Street Map and Typology Policy:** District Policy 7205.5 states that the design of improvements for arterials shall be in accordance with District standards, including the Master Street Map and Livable Streets Design Guide. The developer or engineer should contact the District before starting any design.

**Street Section and Right-of Way Width Policy:** District Policy 7205.2.1 & 7205.5.2 states that the standard 3-lane street section shall be 46-feet (back-of-curb to back-of-curb) within 70 feet of right-of-way. This width typically accommodates a single travel lane in each direction, a continuous center left-turn lane, and bike lanes.

**Right-of-Way Dedication:** District Policy 7205.2 states that The District will provide compensation for additional right-of-way dedicated beyond the existing right-of-way along arterials listed as impact fee eligible in the adopted Capital Improvements Plan using available impact fee revenue in the Impact Fee Service Area.

No compensation will be provided for right-of-way on an arterial that is not listed as impact fee eligible in the Capital Improvements Plan.

The District may acquire additional right-of-way beyond the site-related needs to preserve a corridor for future capacity improvements, as provided in Section 7300.

**Sidewalk Policy:** District Policy 7205.5.7 requires a concrete sidewalk at least 5-feet wide to be constructed on both sides of all arterial streets. A parkway strip at least 6-feet wide between the back-of-curb and street edge of the sidewalk is required to provide increased safety and protection of pedestrians. Consult the District's planter width policy if trees are to be placed within the parkway strip. Sidewalks constructed next to the back-of-curb shall be a minimum of 7-feet wide.

Detached sidewalks are encouraged and should be parallel to the adjacent roadway. Meandering sidewalks are discouraged.

A permanent right-of-way easement shall be provided if public sidewalks are placed outside of the dedicated right-of-way. The easement shall encompass the entire area between the right-of-way line and 2-feet behind the back edge of the sidewalk. Sidewalks shall either be located wholly within the public right-of-way or wholly within an easement.

**Frontage Improvements Policy:** District Policy 7205.2.1 states that the developer shall widen the pavement to a minimum of 17-feet from centerline plus a 3-foot wide gravel shoulder adjacent to the entire site. Curb, gutter and additional pavement widening may be required (See Section 7205.5.5).

**ACHD Master Street Map:** ACHD Policy Section 3111.1 requires the Master Street Map (MSM) guide the right-of-way acquisition, arterial street requirements, and specific roadway features required through development. This segment of Hubbard Road is designated in the MSM as a Residential Arterial with 3-lanes and on-street bike lanes, a 46-foot street section within 74-feet of right-of-way.

- c. **Applicant Proposal:** The applicant is proposing to construct an 8-foot wide sidewalk on Hubbard Road abutting the site.
- d. **Staff Comments/Recommendations:** The applicant should be required to dedicate additional right-of-way to total 37-feet from the centerline of Hubbard Road abutting the site. The applicant will not be compensated for this right-of-way dedication, as this segment of Hubbard Road is not listed as impact fee eligible in the CIP.

The applicant's proposal to construct an 8-foot wide sidewalk on Hubbard Road abutting the site exceeds District policy, which requires the construction of a 5-foot wide detached sidewalk and should be approved, as proposed. The sidewalk should be located a minimum of 31-feet from the centerline of Hubbard Road abutting the site. If detached sidewalks are located outside of the dedicated right-of-way, then a permanent right-of-way easement should be required.

Consistent with ACHD's Frontage Improvement policy, the applicant should be required to widen the pavement on Hubbard Road to a minimum width of 17-feet from centerline plus a 3-foot wide gravel shoulder adjacent to the entire site.

## 2. Locust Grove Road

- a. **Existing Conditions:** Locust Grove Road is improved with 2-travel lanes and no curb, gutter or sidewalk abutting the site. There is 58-feet of right-of-way for Locust Grove Road (27-feet from centerline). The centerline and section of Locust Grove Road are offset abutting the site.

- b. **Policy:**

**Arterial Roadway Policy:** District Policy 7205.2.1 states that the developer is responsible for improving all street frontages adjacent to the site regardless of whether or not access is taken to all of the adjacent streets.

**Master Street Map and Typology Policy:** District Policy 7205.5 states that the design of improvements for arterials shall be in accordance with District standards, including the Master Street Map and Livable Streets Design Guide. The developer or engineer should contact the District before starting any design.

**Street Section and Right-of Way Width Policy:** District Policy 7205.2.1 & 7205.5.2 states that the standard 3-lane street section shall be 46-feet (back-of-curb to back-of-curb) within 70 feet of right-of-way. This width typically accommodates a single travel lane in each direction, a continuous center left-turn lane, and bike lanes.

**Right-of-Way Dedication:** District Policy 7205.2 states that The District will provide compensation for additional right-of-way dedicated beyond the existing right-of-way along arterials listed as impact fee eligible in the adopted Capital Improvements Plan using available impact fee revenue in the Impact Fee Service Area.

No compensation will be provided for right-of-way on an arterial that is not listed as impact fee eligible in the Capital Improvements Plan.

The District may acquire additional right-of-way beyond the site-related needs to preserve a corridor for future capacity improvements, as provided in Section 7300.

**Sidewalk Policy:** District Policy 7205.5.7 requires a concrete sidewalk at least 5-feet wide to be constructed on both sides of all arterial streets. A parkway strip at least 6-feet wide between the back-of-curb and street edge of the sidewalk is required to provide increased safety and protection of pedestrians. Consult the District's planter width policy if trees are to be placed within the parkway strip. Sidewalks constructed next to the back-of-curb shall be a minimum of 7-feet wide.

Detached sidewalks are encouraged and should be parallel to the adjacent roadway. Meandering sidewalks are discouraged.

A permanent right-of-way easement shall be provided if public sidewalks are placed outside of the dedicated right-of-way. The easement shall encompass the entire area between the right-of-way line and 2-feet behind the back edge of the sidewalk. Sidewalks shall either be located wholly within the public right-of-way or wholly within an easement.

**Frontage Improvements Policy:** District Policy 7205.2.1 states that the developer shall widen the pavement to a minimum of 17-feet from centerline plus a 3-foot wide gravel shoulder adjacent to the entire site. Curb, gutter and additional pavement widening may be required (See Section 7205.5.5).

**ACHD Master Street Map:** ACHD Policy Section 3111.1 requires the Master Street Map (MSM) guide the right-of-way acquisition, arterial street requirements, and specific roadway features required through development. This segment of Locust Grove Road is designated in the MSM as a Residential Arterial with 3-lanes and on-street bike lanes, a 46-foot street section within 74-feet of right-of-way.

- c. The applicant is proposing to construct an 8-foot wide sidewalk on Hubbard Road abutting the site.
- d. **Staff Comments/Recommendations:** The applicant should be required to dedicate additional right-of-way to total 37-feet from the centerline of Locust Grove Road abutting the site. The applicant will not be compensated for this right-of-way dedication, as this segment of Locust Grove Road is not listed as impact fee eligible in the CIP.

The applicant's proposal to construct an 8-foot wide sidewalk on Locust Grove Road abutting the site exceeds District policy, which requires the construction of a 5-foot wide detached sidewalk and should be approved, as proposed. The sidewalk should be located a minimum of 31-feet from the centerline of Locust Grove Road abutting the site. If detached sidewalks are located outside of the dedicated right-of-way, then a permanent right-of-way easement should be required.

Consistent with ACHD's Frontage Improvement policy, the applicant should be required to widen the pavement on Locust Grove Road to a minimum width of 17-feet from centerline plus a 3-foot wide gravel shoulder adjacent to the entire site.

Consistent with the findings and recommendations of the traffic impact study, the applicant should be required to construct a dedicated right turn lane on Locust Grove Road at Rio Vallegas Street, when Rio Vallegas Street is constructed and intersects the roadway.

### 3. Stroebele Road

- a. **Existing Conditions:** Stroebele Road doesn't exist abutting the site.

- b. **Policy:**

**Collector Street Policy:** District policy 7206.2.1 states that the developer is responsible for improving all collector frontages adjacent to the site or internal to the development as required below, regardless of whether access is taken to all of the adjacent streets.

**Master Street Map and Typologies Policy:** District policy 7206.5 states that if the collector street is designated with a typology on the Master Street Map, that typology shall be considered for the required street improvements. If there is no typology listed in the Master Street Map, then standard street sections shall serve as the default.

**Street Section and Right-of-Way Policy:** District policy 7206.5.2 states that the standard right-of-way width for collector streets shall typically be 50 to 70-feet, depending on the location and width of the sidewalk and the location and use of the roadway. The right-of-way width may be reduced, with District approval, if the sidewalk is located within an easement; in which case the District will require a minimum right-of-way width that extends 2-feet behind the back-of-curb on each side.

The standard street section shall be 46-feet (back-of-curb to back-of-curb). This width typically accommodates a single travel lane in each direction, a continuous center left-turn lane, and bike lanes.

**Residential Collector Policy:** District policy 7206.5.2 states that the standard street section for a collector in a residential area shall be 36-feet (back-of-curb to back-of-curb). The District will consider a 33-foot or 29-foot street section with written fire department approval and taking into consideration the needs of the adjacent land use, the projected volumes, the need for bicycle lanes, and on-street parking.

**Sidewalk Policy:** District policy 7206.5.6 requires a concrete sidewalks at least 5-feet wide to be constructed on both sides of all collector streets. A parkway strip at least 6-feet wide between the back-of-curb and street edge of the sidewalk is required to provide increased safety and protection of pedestrians. Consult the District's planter width policy if trees are to be placed within the parkway strip. Sidewalks constructed next to the back-of-curb shall be a minimum of 7-feet wide.

Detached sidewalks are encouraged and should be parallel to the adjacent roadway. Meandering sidewalks are discouraged.

A permanent right-of-way easement shall be provided if public sidewalks are placed outside of the dedicated right-of-way. The easement shall encompass the entire area between the right-of-way line and 2-feet behind the back edge of the sidewalk. Sidewalks shall either be located wholly within the public right-of-way or wholly within an easement.

**Half Street Policy:** District Policy 7207.2.2 required improvements shall consist of pavement widening to one-half the required width, including curb, gutter and concrete sidewalk (minimum 5-feet), plus 12-feet of additional pavement widening beyond the centerline established for the street to provide an adequate roadway surface, with the pavement crowned at the ultimate centerline. A 3-foot wide gravel shoulder and a borrow ditch sized to accommodate the roadway storm runoff shall be constructed on the unimproved side.

**ACHD Master Street Map:** ACHD Policy Section 3111.1 requires the Master Street Map (MSM) guide the right-of-way acquisition, collector street requirements, and specific roadway features required through development. A new collector roadway was identified on the MSM with the street typology of Residential Collector. The new collector roadway should run north/south and be located at the half mile between Meridian Road/SH-69 and Locust Grove Road. The Residential Collector typology as depicted in the Livable Street Design Guide recommends a 2-lane roadway with bike lanes, a 36-foot street section within 54-feet of right-of-way.

- c. **Applicant Proposal:** The applicant is proposing to construct Stroebel Road with pavement widening, curb, and gutter within 42-feet of right-of-way with a 6-foot wide planter strip and 8-foot wide detached concrete sidewalk located outside of the right-of-way.
- d. **Staff Comments/Recommendations:** The applicant's proposal generally meets District policy and should be approved. Stroebel Road should be constructed as ½ of a 36-foot wide collector street section plus 12-additional feet of pavement (to total 30-feet) with vertical curb, gutter, and a minimum 5-foot wide detached (or 7-foot wide) attached concrete sidewalk. If detached sidewalks are constructed outside of the dedicated right-of-way, then a permanent right-of-way easement should be provided.

If street trees are desired, then the planter strip should be a minimum width of 8-feet.

#### 4. Internal Local Streets

a. **Existing Conditions:** There are no internal local streets within the site.

b. **Policy:**

**Local Roadway Policy:** District Policy 7207.2.1 states that the developer is responsible for improving all local street frontages adjacent to the site regardless of whether or not access is taken to all of the adjacent streets.

**Street Section and Right-of-Way Policy:** District Policy 7207.5 states that right-of-way widths for all local streets shall generally not be less than 47-feet wide and that the standard street section shall be 33-feet (back-of-curb to back-of-curb).

**Standard Urban Local Street—33-foot Street Section and Right-of-way Policy:** District Policy 7207.5.2 states that the standard street section shall be 33-feet (back-of-curb to back-of-curb) for developments with any buildable lot that is less than 1 acre in size. This street section shall include curb, gutter, and minimum 5-foot wide concrete sidewalks on both sides and shall typically be constructed within 47-feet of right-of-way.

For the City of Kuna and City of Star: Unless otherwise approved by Kuna or Star, the standard street section shall be 36-feet (back-of-curb to back-of-curb) for developments with any buildable lot that is less than 1 acre in size. This street section shall include curb, gutter, and minimum 5-foot wide concrete sidewalks on both sides and shall typically be constructed within 50-feet of right-of-way.

**Sidewalk Policy:** District Policy 7207.5.7 states that five-foot wide concrete sidewalk is required on both sides of all local street, except those in rural developments with net densities of one dwelling unit per 1.0 acre or less, or in hillside conditions where there is no direct lot frontage, in which case a sidewalk shall be constructed along one side of the street. Some local jurisdictions may require wider sidewalks.

The sidewalk may be placed next to the back-of-curb. Where feasible, a parkway strip at least 8-feet wide between the back-of-curb and the street edge of the sidewalk is recommended to provide increased safety and protection of pedestrians and to allow for the planting of trees in accordance with the District's Tree Planting Policy. If no trees are to be planted in the parkway strip, the applicant may submit a request to the District, with justification, to reduce the width of the parkway strip.

Detached sidewalks are encouraged and should be parallel to the adjacent roadway. Meandering sidewalks are discouraged.

A permanent right-of-way easement shall be provided if public sidewalks are placed outside of the dedicated right-of-way. The easement shall encompass the entire area between the right-of-way line and 2-feet behind the back edge of the sidewalk. Sidewalks shall either be located wholly within the public right-of-way or wholly within an easement.

**Landscape Medians Policy:** District policy 7207.5.16 states that landscape medians are permissible where adequate pavement width is provided on each side of the median to accommodate the travel lanes and where the following is provided:

- The median is platted as right-of-way owned by ACHD.
- The width of an island near an intersection is 12-feet maximum for a minimum distance of 150-feet. Beyond the 150-feet, the island may increase to a maximum width of 30-feet.
- At an intersection that is signalized or is to be signalized in the future, the median width shall be reduced to accommodate the necessary turn lane storage and tapers.
- The Developer or Homeowners Association shall apply for a license agreement if landscaping is to be placed within these medians.

- The license agreement shall contain the District's requirements of the developer including, but not limited to, a "hold harmless" clause; requirements for maintenance by the developer; liability insurance requirements; and restrictions.
  - Vertical curbs are required around the perimeter of any raised median. Gutters shall slope away from the curb to prevent ponding.
- c. **Applicant's Proposal:** The applicant has proposed to construct the entry portion of Boulder Basin Drive with two 20-foot wide travel lanes, an 8-foot wide center landscape island, vertical curb, gutter, and 5-foot wide attached sidewalks within 70-feet of right-of-way.

The applicant has proposed to construct all other internal local streets as 36-foot street sections with rolled curb, gutter, and 5-foot wide attached concrete sidewalks.

- d. **Staff Comments/Recommendations:** The applicant's proposal meets District policy and should be approved, as proposed. The right-of-way on the entry portion of Boulder Basin Drive may be reduced to extend 2-feet behind the back of curb.

The center landscape island on Boulder Basin Drive should be platted as right-of-way owned by ACHD. If landscaping is desired within the island, then the applicant or future home owner's association should enter into a license agreement with ACHD.

The right-of-way for Rio Vallegas Street should extend to the south property line so that the property to the south will have access to this street upon development.

## 5. Alleys

- a. **Existing Conditions:** There are no alleys within the site.

b. **Policy:**

**Alley Policy:** District Policy 7210.3.1 requires the minimum right-of-way width for all new residential alleys shall be a minimum of 16-feet or a maximum of 20-feet. If the residential alley is 16-feet in width building setbacks required by the land use agency having jurisdiction shall provide sufficient space for the safe backing of vehicles into the alley (see Section 7210.3.3). The minimum right-of-way width for all new commercial or mixed-use alleys shall be 20-feet. All alleys shall be improved by paving the full width and length of the right-of-way.

Dedication of clear title to the right-of-way and the improvement of the alley, and acceptance of the improvement by the District as meeting its construction standards, are required for all alleys contained in a proposed development.

**Alley Length Policy:** District Policy 7210.3.2 states that alleys shall be no longer than 700-feet in length. If the lead land use agency having jurisdiction requires a shorter block length, the alley shall be no longer than the agency's required block length.

**Alley Parking & Setbacks Policy:** District Policy 7210.3.3 states that parking within the alley right-of-way is prohibited. "No Parking" signs are required to be installed by the developer. The signs should be located at the alley/street intersections. Parking which is entered from the alley shall be designed so the minimum clear distance from the back of the parking stall to the opposite side of the alley is 20-feet for all perpendicular parking.

Setbacks for structures taking access from the alley should be closely coordinated with the lead land use agency. The setbacks shall either discourage parking within the alley (where it may partially block or occur within the right-of-way) or allow adequate area for one perpendicular parking pad. In order to discourage parking, building setbacks shall be minimal from the alley right-of-way line, while still achieving the required 20-feet of back-up space from a garage or other parking structure to the opposite side of the alley (i.e. 4-foot setback + 16-foot alley = 20-feet for back-up space).

**Alley Intersections and Offsets Policy:** District policy 7210.3.7 states that alleys should intersect public streets at each end. In specific circumstances as outlined in the policies below, the District may consider allowing an alley to intersect a public street at only one end. A 90-degree angle of intersection shall be designed where practical. In no case shall the intersecting angle be less than 75-degrees, as measured from centerline of intersecting street. An access to an alley shall be located a minimum of 50-feet from the nearest street (as measured centerline to centerline).

**Alley/Local Street Intersections Policy:** District Policy 7210.3.7.2 states that alleys may intersect all types of local streets including minor local streets. Alleys shall generally be designed with a curb cut type approach when intersecting a local street. Alleys shall generally intersect streets in the middle of the block equally offsetting the intersecting streets. Alleys shall either align with alley/street intersections or provide a minimum 100-foot offset (measured centerline to centerline) from other local street intersections. For alley intersections with local streets, the District may consider a reduced offset if the lead land use agency's required lot size allows for shorter buildable lots.

**Vacations of Alleys Policy:** District Policy 7210.3.6 states that vacations of alley right-of-way are discouraged and shall not result in dead-end alleys.

- c. **Applicant Proposal:** The applicant has proposed to construct two 20-foot wide east/west alleys within the site. One to run between Rocky Bottom Way and Ledge Meadow Way and one to run between Ledge Meadow Way and Rio Vallegas Street.

The applicant has proposed to construct one 20-foot wide north/south alley to run between New Spring Street and Whisper Wind Drive.

- d. **Staff Comments/Recommendations:** The applicant's proposal meets District policy and should be approved, as proposed.

All alleys with horizontal curves should be designed using the AASHTO equation 3-38.  $HSO = R(1 - \cos(28.65 \cdot S/R))$  using  $S = 80$ .

## 6. Roadway Offsets

- a. **Existing Conditions:** There are no roadways within the site.

- b. **Policy:**

**Local Street Intersection Spacing on Minor Arterials:** District policy 7205.4.3 states that new local streets should not typically intersect arterials. Local streets should typically intersect collectors. If it is necessary, as determined by ACHD, for a local street to intersect an arterial, the minimum allowable offset shall be 660-feet as measured from all other existing roadways as identified in Table 1a (7205.4.6).

**Collector Offset Policy:** District policy 7205.4.2 states that the optimum spacing for new signalized collector roadways intersecting minor arterials is one half-mile.

**Local Offset Policy:** District policy 7206.4.5, requires local roadways to align or offset a minimum of 330-feet from a collector roadway (measured centerline to centerline).

District policy 7209.4.2, requires industrial roadways intersecting other local streets (residential, industrial or commercial) to provide a minimum offset of 125-feet from any other roadway or intersection (measured centerline to centerline).

- c. **Applicant's Proposal:** The applicant has proposed to construct one local street, Boulder Basin Road, to intersect Stroebel Road, located approximately 475-feet south of Hubbard Road.

The applicant has proposed to construct one local street, Rio Vallegas Street, to intersect Locust Grove Road at the site's south property line.

The applicant has proposed to construct one temporary access onto Hubbard Road located approximately 750-feet east of Stroebel Road.

- d. **Staff Comments/Recommendations:** The applicant's proposal to construct Boulder Basin Road to intersect Stroebel Road and Rio Vallegas Street to intersect Locust Grove Road meets District policy and should be approved, as proposed.

Staff is supportive of the applicant's proposal to construct a temporary access onto Hubbard Road, as it is necessary to provide secondary emergency access to the site until Boulder Basin Road and Rio Vallegas Street are constructed or a stub street is extended providing secondary access to the site. A temporary right-of-way easement should be provided for the temporary access road. The easement would be released after both Boulder Basin Road and Rio Vallegas Street are constructed or a stub street is extended providing secondary access to the site.

## 7. Stub Streets

- a. **Existing Conditions:** There are no stub streets to or from the site.

- b. **Policy:**

**Stub Street Policy:** District policy 7207.2.4 and 7206.2.4 states that stub streets will be required to provide circulation or to provide access to adjoining properties. Stub streets will conform with the requirements described in Section 7207.2.5.4 and 7206.2.5.4 except a temporary cul-de-sac will not be required if the stub street has a length no greater than 150-feet. A sign shall be installed at the terminus of the stub street stating that, "THIS ROAD WILL BE EXTENDED IN THE FUTURE."

In addition, stub streets must meet the following conditions:

- A stub street shall be designed to slope towards the nearest street intersection within the proposed development and drain surface water towards that intersection; unless an alternative storm drain system is approved by the District.
- The District may require appropriate covenants guaranteeing that the stub street will remain free of obstructions.

**Temporary Dead End Streets Policy:** District policy 7207.2.4 requires that the design and construction for cul-de-sac streets shall apply to temporary dead-end streets. The temporary cul-de-sac shall be paved and shall be the dimensional requirements of a standard cul-de-sac. The developer shall grant a temporary turnaround easement to the District for those portions of the cul-de-sac which extend beyond the dedicated street right-of-way. In the instance where a temporary easement extends onto a buildable lot, the entire lot shall be encumbered by the easement and identified on the plat as a non-buildable lot until the street is extended.

- c. **Applicant Proposal:** The applicant has proposed to construct two stub streets to the south, Stroebel Road, located at the site's southwest property line and Moonshadow Avenue, located approximately 680-feet east of Stroebel Road and one stub street to the north, Rocky Bottom Way, located approximately 1,020-feet west of Locust Grove Road. Rocky Bottom Way is proposed to stub to Mason Creek.
- d. **Staff Comments/Recommendations:** The applicant's proposal to construct one stub street to the north and two to the south meets District policy and should be approved, as proposed. The applicant should be required to install signs at the terminus of Stroebel Road which states that, "THIS IS A DESIGNATED COLLECTOR ROADWAY. THIS STREET WILL BE EXTENDED AND WIDENED IN THE FUTURE."

The applicant should be required to construct a temporary turnaround at the terminus of Stroebel Road as it extends greater than 150-feet in length. The temporary cul-de-sac shall be paved and be constructed to the dimensional requirements of a standard cul-de-sac. The developer should grant a temporary turnaround easement to the District for those portions

of the cul-de-sac which extend beyond the dedicated street right-of-way. In the instance where a temporary easement extends onto a buildable lot, the entire lot shall be encumbered by the easement and identified on the plat as a non-buildable lot until the street is extended.

The applicant should be required to install a sign at the terminus of Moonshadow Avenue and Rocky Bottom Way which states that, "THIS ROAD WILL BE EXTENDED IN THE FUTURE."

Rocky Bottom Way is proposed to stub to Mason Creek. To accommodate the future extension of the stub street across the creek, the applicant should be required to design a crossing of the creek and provide a road trust deposit for ½ the crossing.

## 8. Tree Planters

**Tree Planter Policy:** Tree Planter Policy: The District's Tree Planter Policy prohibits all trees in planters less than 8-feet in width without the installation of root barriers. Class II trees may be allowed in planters with a minimum width of 8-feet, and Class I and Class III trees may be allowed in planters with a minimum width of 10-feet.

## 9. Landscaping

**Landscaping Policy:** A license agreement is required for all landscaping proposed within ACHD right-of-way or easement areas. Trees shall be located no closer than 10-feet from all public storm drain facilities. Landscaping should be designed to eliminate site obstructions in the vision triangle at intersections. District Policy 5104.3.1 requires a 40-foot vision triangle and a 3-foot height restriction on all landscaping located at an uncontrolled intersection and a 50-foot offset from stop signs. Landscape plans are required with the submittal of civil plans and must meet all District requirements prior to signature of the final plat and/or approval of the civil plans.

## 10. Other Access

Hubbard Road and Locust Grove Road are classified as minor arterial roadways. Other than the access specifically approved with this application, direct lot access is prohibited to these roadways and should be noted on the final plat.

## D. Site Specific Conditions of Approval

1. Convert the Locust Grove Road/Hubbard Road intersection to an all-way stop control intersection prior to plan approval and ACHD's signature on the final plat that contains the 225 lot.
2. Dedicate additional right-of-way to total 37-feet from the centerline of Hubbard Road abutting the site. There is no compensation for this right-of-way dedication.
3. Construct an 8-foot wide detached concrete sidewalk on Hubbard Road located a minimum of 31-feet from the centerline of Hubbard Road abutting the site. Provide a permanent right-of-way easement for detached sidewalks located outside of the dedicated right-of-way
4. Widen the pavement on Hubbard Road to a minimum width of 17-feet from centerline plus a 3-foot wide gravel shoulder adjacent to the entire site.
5. Dedicate additional right-of-way to total 37-feet from the centerline of Locust Grove Road abutting the site. There is no compensation for this right-of-way dedication.
6. Construct an 8-foot wide detached concrete sidewalk on Locust Grove Road located a minimum of 31-feet from the centerline of Locust Grove Road abutting the site. Provide a permanent right-of-way easement for detached sidewalks located outside of the dedicated right-of-way
7. Widen the pavement on Locust Grove Road to a minimum width of 17-feet from centerline plus a 3-foot wide gravel shoulder adjacent to the entire site.

8. Construct a dedicated right turn lane on Locust Grove Road at Rio Vallegas Street, when Rio Vallegas Street is constructed and intersects Locust Grove Road.
9. Construct Stroebel Road to intersect Hubbard Road at the half mile between Locust Grove Road and Meridian Road/SH-69, as proposed.
10. Construct Stroebel Road as  $\frac{1}{2}$  of a 36-foot wide collector street section plus 12-additional feet of pavement (to total 30-feet) with vertical curb, gutter, and a minimum 5-foot wide detached (or 7-foot wide) attached concrete sidewalk abutting the site. Right-of-way. Provide a permanent right-of-way easement for detached sidewalks located outside of the dedicated right-of-way.
11. Construct one 36-foot wide temporary access onto Hubbard Road located 750-feet east of Stroebel Road, as proposed. A temporary right-of-way easement shall be provided for the temporary access road. The easement will be released after both Boulder Basin Road and Rio Vallegas Street are constructed or a stub street is extended providing secondary access to the site.
12. Construct one local street, Boulder Basin Road, to intersect Stroebel Road, located 475-feet south of Hubbard Road, as proposed.
13. Construct the entry portion of Boulder Basin Drive with two 20-foot wide travel lanes, an 8-foot wide center landscape island, vertical curb, gutter, and 5-foot wide attached sidewalks within 70-feet of right-of-way, as proposed. Plat the center landscape island as right-of-way owned by ACHD. The applicant or future home owner's association may enter into a license agreement with ACHD if landscaping is desired within the island.
14. Construct one local street, Rio Vallegas Street, to intersect Locust Grove Road at the site's south property line, as proposed.
15. Construct all internal local streets as 36-foot wide street sections with rolled curb, gutter, and 5-foot wide attached concrete sidewalks, as proposed.
16. Extend the right-of-way for Rio Vallegas Street to the south property line.
17. Construct one 20-foot wide north/south alley to run between New Spring Street and Whisper Wind Drive, as proposed.
18. Construct one 20-foot wide east/west alley to run between Rocky Bottom Way and Ledge Meadow Way, as proposed.
19. Construct one 20-foot wide east/west alley to run between Ledge Meadow Way and Rio Vallegas Street, as proposed. The curve in this alley shall be designed using the AASHTO equation 3-38.  $HSO = R (1 - \cos(28.65 \cdot S/R))$  using  $S = 80$ .
20. Construct one stub street to the south, Stroebel Road, located at the site's southwest property line. Install a sign at the terminus of Stroebel Road which states that, "THIS IS A DESIGNATED COLLECTOR ROADWAY. THIS STREET WILL BE EXTENDED AND WIDENED IN THE FUTURE."
21. Construct a paved temporary turnaround with a minimum radius of 45-feet at the terminus of Stroebel Road. If the temporary turnaround extends beyond the dedicated right-of-way a temporary turn around easement shall be provided. In the instance where a temporary easement extends onto a buildable lot, the entire lot shall be encumbered by the easement and identified on the plat as a non-buildable lot until the street is extended.
22. Construct one stub street to the south, Moonshadow Avenue, located 680-feet east of Stroebel Road. Install a sign at the terminus of Moonshadow Avenue, which states that, ""THIS ROAD WILL BE EXTENDED IN THE FUTURE."

23. Construct one stub street to the north, Rocky Bottom Way, located approximately 1,020-feet west of Locust Grove Road. Rocky Bottom Way is proposed to stub to Mason Creek. Install a sign at the terminus of Rocky Bottom Way which states that, ""THIS ROAD WILL BE EXTENDED IN THE FUTURE.""
24. Rocky Bottom Way stubs to Mason Creek. To accommodate the future extension of the stub street across the creek, design a crossing of the creek and provide a road trust deposit for ½ the crossing.
25. Direct lot access to Hubbard Road and Locust Grove Road is prohibited and shall be noted on the final plat.
26. Submit civil plans to ACHD Development Services for review and approval. The impact fee assessment will not be released until the civil plans are approved by ACHD.
27. Payment of impact fees is due prior to issuance of a building permit.
28. Comply with all Standard Conditions of Approval.

## **E. Standard Conditions of Approval**

1. All proposed irrigation facilities shall be located outside of the ACHD right-of-way (including all easements). Any existing irrigation facilities shall be relocated outside of the ACHD right-of-way (including all easements).
2. Private Utilities including sewer or water systems are prohibited from being located within the ACHD right-of-way.
3. In accordance with District policy, 7203.3, the applicant may be required to update any existing non-compliant pedestrian improvements abutting the site to meet current Americans with Disabilities Act (ADA) requirements. The applicant's engineer should provide documentation of ADA compliance to District Development Review staff for review.
4. Replace any existing damaged curb, gutter and sidewalk and any that may be damaged during the construction of the proposed development. Contact Construction Services at 387-6280 (with file number) for details.
5. A license agreement and compliance with the District's Tree Planter policy is required for all landscaping proposed within ACHD right-of-way or easement areas.
6. All utility relocation costs associated with improving street frontages abutting the site shall be borne by the developer.
7. It is the responsibility of the applicant to verify all existing utilities within the right-of-way. The applicant at no cost to ACHD shall repair existing utilities damaged by the applicant. The applicant shall be required to call DIGLINE (1-811-342-1585) at least two full business days prior to breaking ground within ACHD right-of-way. The applicant shall contact ACHD Traffic Operations 387-6190 in the event any ACHD conduits (spare or filled) are compromised during any phase of construction.
8. Utility street cuts in pavement less than five years old are not allowed unless approved in writing by the District. Contact the District's Utility Coordinator at 387-6258 (with file numbers) for details.
9. All design and construction shall be in accordance with the ACHD Policy Manual, ISPWC Standards and approved supplements, Construction Services procedures and all applicable ACHD Standards unless specifically waived herein. An engineer registered in the State of Idaho shall prepare and certify all improvement plans.
10. Construction, use and property development shall be in conformance with all applicable requirements of ACHD prior to District approval for occupancy.
11. No change in the terms and conditions of this approval shall be valid unless they are in writing and signed by the applicant or the applicant's authorized representative and an authorized

representative of ACHD. The burden shall be upon the applicant to obtain written confirmation of any change from ACHD.

12. If the site plan or use should change in the future, ACHD Planning Review will review the site plan and may require additional improvements to the transportation system at that time. Any change in the planned use of the property which is the subject of this application, shall require the applicant to comply with ACHD Policy and Standard Conditions of Approval in place at that time unless a waiver/variance of the requirements or other legal relief is granted by the ACHD Commission.

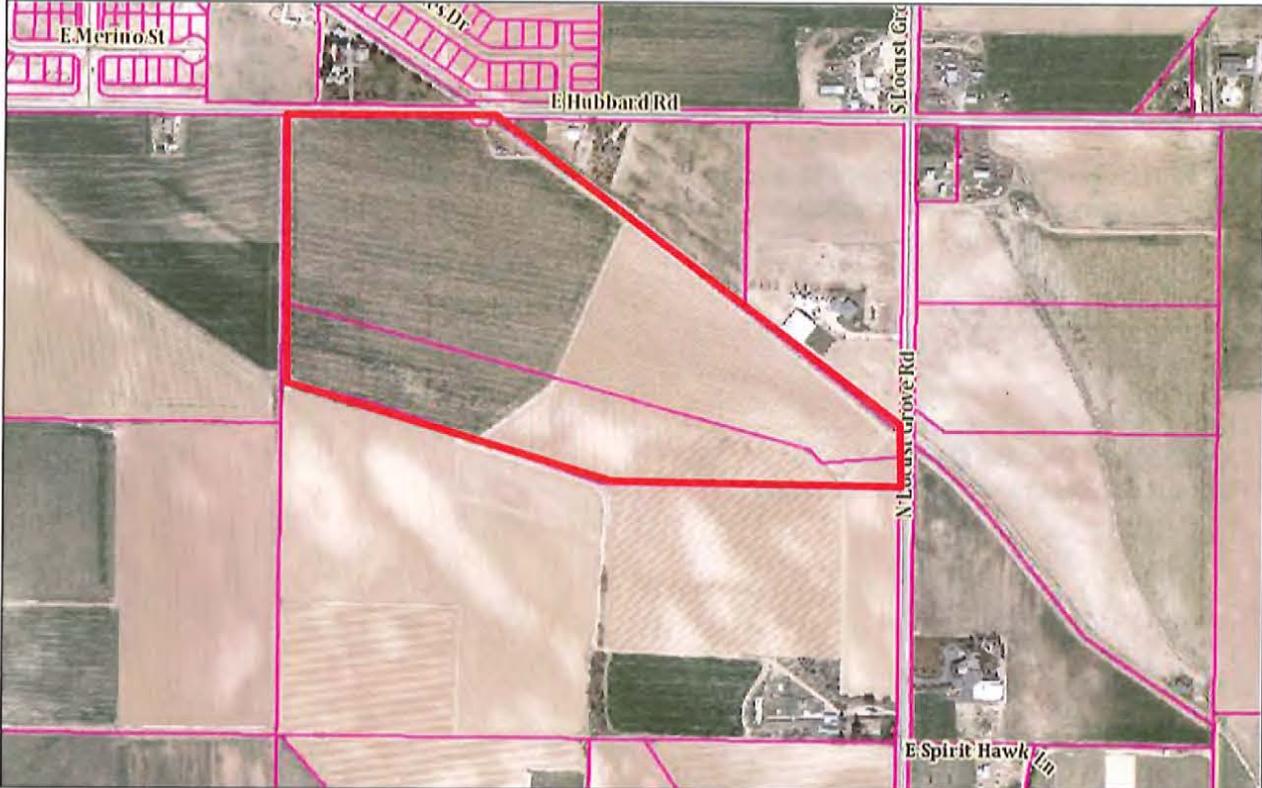
## **F. Conclusions of Law**

1. The proposed site plan is approved, if all of the Site Specific and Standard Conditions of Approval are satisfied.
2. ACHD requirements are intended to assure that the proposed use/development will not place an undue burden on the existing vehicular transportation system within the vicinity impacted by the proposed development.

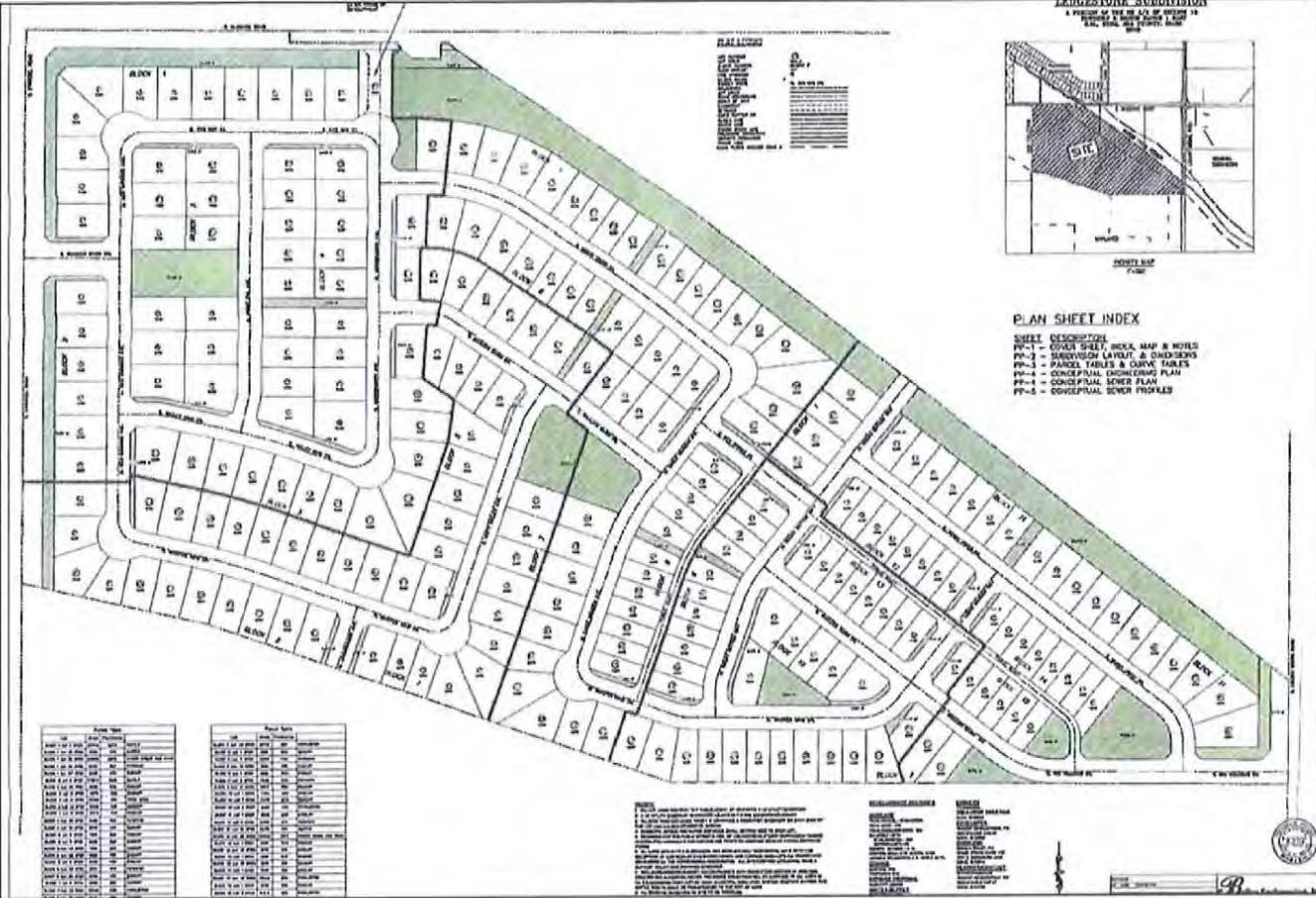
## **G. Attachments**

1. Vicinity Map
2. Site Plan
3. Utility Coordinating Council
4. Development Process Checklist
5. Request for Reconsideration Guidelines

VICINITY MAP



SITE PLAN



## Ada County Utility Coordinating Council

### Developer/Local Improvement District Right of Way Improvements Guideline Request

*Purpose: To develop the necessary avenue for proper notification to utilities of local highway and road improvements, to help the utilities in budgeting and to clarify the already existing process.*

- 1) **Notification:** Within five (5) working days upon notification of required right of way improvements by Highway entities, developers shall provide written notification to the affected utility owners and the Ada County Utility Coordinating Council (UCC). Notification shall include but not be limited to, project limits, scope of roadway improvements/project, anticipated construction dates, and any portions critical to the right of way improvements and coordination of utilities.
- 2) **Plan Review:** The developer shall provide the highway entities and all utility owners with preliminary project plans and schedule a plan review conference. Depending on the scale of utility improvements, a plan review conference may not be necessary, as determined by the utility owners. Conference notification shall also be sent to the UCC. During the review meeting the developer shall notify utilities of the status of right of way/easement acquisition necessary for their project. At the plan review conference each company shall have the right to appeal, adjust and/or negotiate with the developer on its own behalf. Each utility shall provide the developer with a letter of review indicating the costs and time required for relocation of its facilities. Said letter of review is to be provided within thirty calendar days after the date of the plan review conference.
- 3) **Revisions:** The developer is responsible to provide utilities with any revisions to preliminary plans. Utilities may request an updated plan review meeting if revisions are made in the preliminary plans which affect the utility relocation requirements. Utilities shall have thirty days after receiving the revisions to review and comment thereon.
- 4) **Final Notification:** The developer will provide highway entities, utility owners and the UCC with final notification of its intent to proceed with right of way improvements and include the anticipated date work will commence. This notification shall indicate that the work to be performed shall be pursuant to final approved plans by the highway entity. The developer shall schedule a preconstruction meeting prior to right of way improvements. Utility relocation activity shall be completed within the times established during the preconstruction meeting, unless otherwise agreed upon.

**Notification to the Ada County UCC can be sent to:** 50 S. Cole Rd. Boise 83707, or Visit [iducc.com](http://iducc.com) for e-mail notification information.

# Development Process Checklist

**Items Completed to Date:**

- Submit a development application to a City or to Ada County
- The City or the County will transmit the development application to ACHD
- The ACHD **Planning Review Section** will receive the development application to review
- The **Planning Review Section** will do one of the following:
  - Send a **"No Review"** letter to the applicant stating that there are no site specific conditions of approval at this time.
  - Write a **Staff Level** report analyzing the impacts of the development on the transportation system and evaluating the proposal for its conformance to District Policy.
  - Write a **Commission Level** report analyzing the impacts of the development on the transportation system and evaluating the proposal for its conformance to District Policy.

**Items to be completed by Applicant:**

- For **ALL** development applications, including those receiving a **"No Review"** letter:
  - The applicant should submit one set of engineered plans directly to ACHD for review by the **Development Review Section** for plan review and assessment of impact fees. (Note: if there are no site improvements required by ACHD, then architectural plans may be submitted for purposes of impact fee assessment.)
  - The applicant is required to get a permit from Construction Services (ACHD) for ANY work in the right-of-way, including, but not limited to, driveway approaches, street improvements and utility cuts.
- Pay Impact Fees prior to issuance of building permit. Impact fees cannot be paid prior to plan review approval.

*DID YOU REMEMBER:*

*Construction (Non-Subdivisions)*

- Driveway or Property Approach(s)**
  - Submit a "Driveway Approach Request" form to ACHD Construction (for approval by Development Services & Traffic Services). There is a one week turnaround for this approval.
- Working in the ACHD Right-of-Way**
  - Four business days prior to starting work have a bonded contractor submit a "Temporary Highway Use Permit Application" to ACHD Construction – Permits along with:
    - a) Traffic Control Plan
    - b) An Erosion & Sediment Control Narrative & Plat, done by a Certified Plan Designer, if trench is >50' or you are placing >600 sf of concrete or asphalt.

*Construction (Subdivisions)*

- Sediment & Erosion Submittal**
  - At least one week prior to setting up a Pre-Construction Meeting an Erosion & Sediment Control Narrative & Plan, done by a Certified Plan Designer, must be turned into ACHD Construction to be reviewed and approved by the ACHD Stormwater Section.
- Idaho Power Company**
  - Vic Steelman at Idaho Power must have his IPCO approved set of subdivision utility plans prior to Pre-Con being scheduled.
- Final Approval from Development Services is required** prior to scheduling a Pre-Con.

## Request for Appeal of Staff Decision

1. **Appeal of Staff Decision:** The Commission shall hear and decide appeals by an applicant of the final decision made by the Development Services Manager when it is alleged that the Development Services Manager did not properly apply this section 7101.6, did not consider all of the relevant facts presented, made an error of fact or law, abused discretion or acted arbitrarily and capriciously in the interpretation or enforcement of the ACHD Policy Manual.
  - a. **Filing Fee:** The Commission may, from time to time, set reasonable fees to be charged the applicant for the processing of appeals, to cover administrative costs.
  - b. **Initiation:** An appeal is initiated by the filing of a written notice of appeal with the Secretary and Clerk of the District, which must be filed within ten (10) working days from the date of the decision that is the subject of the appeal. The notice of appeal shall refer to the decision being appealed, identify the appellant by name, address and telephone number and state the grounds for the appeal. The grounds shall include a written summary of the provisions of the policy relevant to the appeal and/or the facts and law relied upon and shall include a written argument in support of the appeal. The Commission shall not consider a notice of appeal that does not comply with the provisions of this subsection.
  - c. **Time to Reply:** The Development Services Manager shall have ten (10) working days from the date of the filing of the notice of appeal to reply to the notice of the appeal, and may during such time meet with the appellant to discuss the matter, and may also consider and/or modify the decision that is being appealed. A copy of the reply and any modifications to the decision being appealed will be provided to the appellant prior to the Commission hearing on the appeal.
  - d. **Notice of Hearing:** Unless otherwise agreed to by the appellant, the hearing of the appeal will be noticed and scheduled on the Commission agenda at a regular meeting to be held within thirty (30) days following the delivery to the appellant of the Development Services Manager's reply to the notice of appeal. A copy of the decision being appealed, the notice of appeal and the reply shall be delivered to the Commission at least one (1) week prior to the hearing.
  - e. **Action by Commission:** Following the hearing, the Commission shall either affirm or reverse, in whole or part, or otherwise modify, amend or supplement the decision being appealed, as such action is adequately supported by the law and evidence presented at the hearing.



# CITY OF KUNA

PLANNING & ZONING DEPARTMENT

P.O. Box 13  
Kuna, ID 83634  
Phone: 208-922-5274  
Fax: 208-922-5989  
[www.kunacity.id.gov](http://www.kunacity.id.gov)

## CERTIFICATE OF MAILING

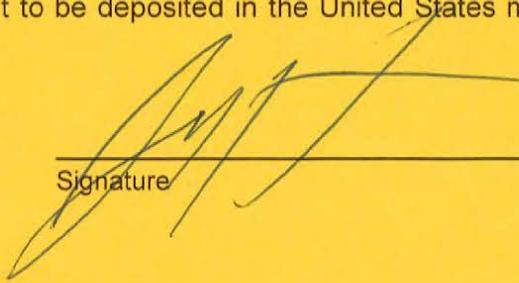
Date: February 13, 2019

To:  400' Property Owners     Other \_\_\_\_\_

Planner: Jace Hellman, Planner II

Case Name: Ledgestone Subdivision; 18-08-S and 18-06-AN

I HEREBY CERTIFY that on this 13<sup>th</sup> day of February, 2019, I caused a true and correct copy of the foregoing instrument to be deposited in the United States mail, with prepaid postage.

  
\_\_\_\_\_  
Signature

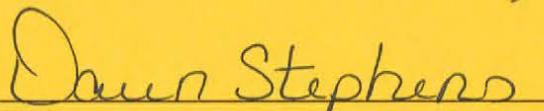
  
\_\_\_\_\_  
Attest

Exhibit  
D3



**CITY OF KUNA**  
**PLANNING & ZONING DEPARTMENT**  
 PO Box 13 • 751 W. 4th St • Kuna, Idaho • 83634  
 Phone (208) 922-5274 • Fax: (208) 922-5989  
 www.kunacity.id.gov

Dear Property Owner:

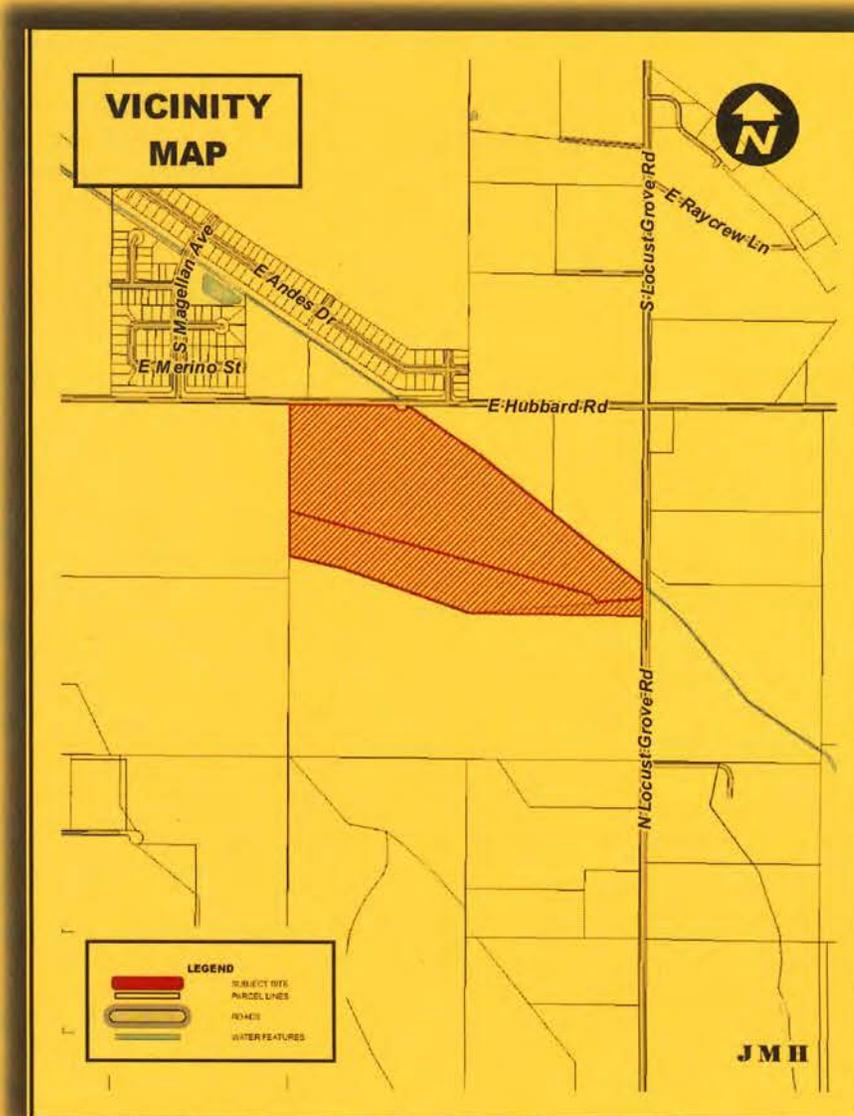
NOTICE IS HEREBY GIVEN that the City of Kuna **City Council** is scheduled to hold a public hearing on **March 5, 2019**, beginning at **6:00 pm** on the following case:

An **Annexation (AN)** and **Preliminary Plat (S)** request for Ledgestone Subdivision. WHPacific, on behalf of Trilogy Development Inc, requests to annex two contiguous parcels comprising of approximately 60.85 acres on East Hubbard Road and North Locust Grove Road into Kuna City Limits with an R-8 zoning designation; and to subdivide the 60.85-acres into 298 total lots, consisting of 253 single family buildable lots, and 45 Common lots. The subject sites are adjacent to the southwest corner of East Hubbard Road and North Locust Grove Road, Kuna, ID 83634, within Section 18, Township 2 North, Range 1 East; (APN's S1418121126 & S1418123400).

The hearing will be held at **6:00 PM in the Council Chambers at City Hall located at 751 W. 4th Street, Kuna, Idaho.**

You are invited to provide oral or written comments at the hearing. Written testimony received by the close of business on **February 27, 2019** will be included in the packets that is distributed to the governing body prior to the hearing. Late submissions (must submit six (6) copies) will be presented to the governing body at time of the hearing. Please note oral comments made during the public hearing will be restricted to three (3) minutes per person. Mail written comments to PO Box 13, Kuna, ID 83634 or drop them off at City Hall, 751 West 4<sup>th</sup> Street, Kuna, ID.

If you have questions or need special assistance, please contact the Planning and Zoning Division at (208) 922-5274.



In all correspondence concerning this case, please refer to the case name: **18-06-AN (Annexation) & 18-08-S (Preliminary Plat) – Ledgestone Subdivision**

MAILED 2/13/19

## Suggestions For Testifying at the Public Hearing:

### Be informed . . .

Review the proposal, the staff report, applicable provisions of the ordinance and comprehensive plan.

All items pertaining to the application can be found the Friday prior to the hearing at <http://kunacity.id.gov/index.aspx?nid=240>.

### Be on time . . .

Although the item you are interested in may not be first on the agenda, you never know when it will be heard. The governing body has authority to adjust the schedule according to its discretion. Thus, anticipate attending from the beginning.

### Speak to the point . . .

The governing body appreciates pertinent, well organized, factual and concise comments. Redundant testimony is prohibited. **The developer or their representative is given 20 minutes to present their project. Others wishing to testify is given three (3) minutes to comment. The developer or their representative is given additional time for rebuttal or discuss issues raised by opposition at the end of all testimony.** Long stories, abstract complaints, or generalities may not be the best use of time. Neighborhood groups are encouraged to organize testimony and have one (1) person speak on behalf of the group -- "opposition representative," will receive 10 minutes to make comments.

### If you don't wish to speak, write . . .

At most hearings, previously submitted written testimony may be reviewed by the governing body before the meeting. It is unreasonable to submit extensive written comments or information at the hearing and expect them to be reviewed prior to a decision. All written comments or documents should be submitted to the City of Kuna at least one (1) week **prior** to the hearing.

City of Kuna  
Planning and Zoning  
PO Box 13  
Kuna, ID 83634

**NOTICE**

Arbor Ridge LLC  
PO Box 344  
Meridian, ID 83680

John Browning  
880 E Hubbard Road  
Kuna, ID 83634

Greayer Clover  
Anne Yanagi  
998 E Andes Dr  
Kuna, ID 83634

Curt & Jann Cook  
1110 E Andes Dr  
Kuna, ID 83634

Benjamin & Kenda Deere  
2440 N Locust Grove Rd  
Kuna, ID 83634

Bruce Garrett  
Robyn Johnson-Garrett  
2933 N Locust Grove Rd  
Kuna, ID 83634

Charles Hiatt  
Lou Ann Hiatt Revocable Living Trust  
1301 E Hubbard Rd  
Kuna, ID 83634

Henry Huber Life Estate  
Sherry Huber Life Estate  
2601 E Hubbard Rd  
Kuna, ID 83634

Russell & Karen Hunemiller  
16130 N Elder St  
Nampa, ID 83687

Jerich 2014 Revocable Trust  
Brian Jerich Trustee  
1087 E Andes Dr  
Kuna, ID 83634

TJ & Elaine Johnson  
2425 N Locust Grove Rd  
Kuna, ID 83634

Scott & Shana Kerbs  
1062 E Andes Dr  
Kuna, ID 83634

JJ & Madge Koudelka  
PO Box 1564  
Nampa, ID 83653

KW Homes Inc  
3866 S Lamone Way  
Meridian, ID 83642

Cynthia Lewis  
919 E Andes Dr  
Kuna, ID 83634

Michael & Aleysha McCoy  
1061 E Andes Dr  
Kuna, ID 83634

Patricia Nations  
18549 Emerald Lake Ave  
Nampa, ID 83687

Olympus Homes LLC  
2060 Bodine Ct  
Boise, ID 83705

Patagonia Development LLC  
Patagonia HOA LLC  
PO Box 344  
Meridian, ID 83680

Patagonia Subdivision HOA Inc  
1710 S Wells St Ste 110  
Meridian, ID 83680

Robbie & Nancy Pond  
645 E Merino St  
Kuna, ID 83634

Riverwood Homes Inc  
PO Box 344  
Meridian, ID 83680

Daniel & Bonnie Robinson  
1046 E Andes Dr  
Kuna, ID 83634

Tony & Vicki Rose Declaration of Trust  
Tony Rose Trustee  
1099 E Andes Dr  
Kuna, ID 83634

Schroeder Enterprises Inc  
1825 E Spirit Hawk Ln  
Kuna, ID 83634

Shadow Mountain Construction Inc  
52 N Palmetto Ave Ste 102  
Eagle, ID 83616

Stacy Construction Inc  
1200 E Watertower St Ste 120  
Meridian, ID 83642

Thomas & Darcell Stewart  
1111 E Andes Dr  
Kuna, ID 83634

Varriale Construction Inc  
2018 S Pond St  
Boise, ID 83705

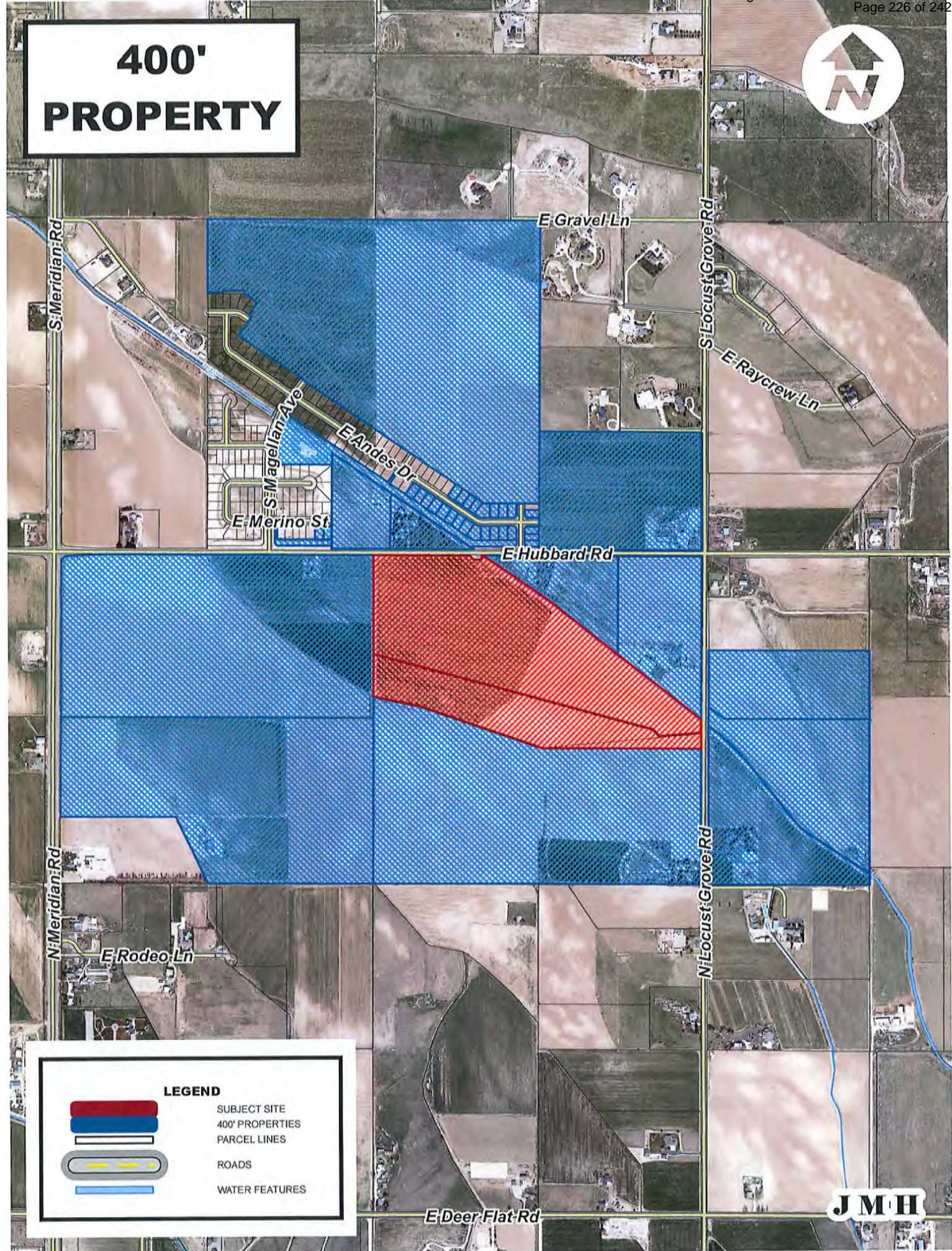
Lindsey & Connie Willman  
9520 S Fuego Ave  
Kuna, ID 83634



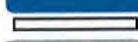
PRIMOWNER	SECOWNER	ADDCONCAT	STATCONCAT
ARBOR RIDGE LLC		P O BOX 344	MERIDIAN, ID 83680-0000
BROWNING JOHN W		880 E HUBBARD RD	KUNA, ID 83634-0000
CLOVER GREAYER D	YANAGI ANNE	998 E ANDES DR	KUNA, ID 83634-0000
COOK CURT	COOK JANN	1110 E ANDES DR	KUNA, ID 83634-0000
DEERE BENJAMIN F	DEERE KENDA J	2440 N LOCUST GROVE RD	KUNA, ID 83634-0000
GARRETT BRUCE	JOHNSON-GARRETT ROBYN	2933 N LOCUST GROVE RD	KUNA, ID 83634-0000
HIATT CHARLES E	HIATT LOU ANN TREVOCABLE LIVING TRUST	1301 E HUBBARD RD	KUNA, ID 83634-1329
HUBER HENRY A LIFE ESTATE	HUBER SHERRY R LIFE ESTATE	2601 E HUBBARD RD	KUNA, ID 83634-0000
HUNEMILLER RUSSELL D	HUNEMILLER KAREN	16130 N ELDER ST	NAMPA, ID 83687-0000
JERICH 2014 REVOCABLE TRUST THE	JERICH BRIAN A TRUSTEE	1087 E ANDES DR	KUNA, ID 83634-0000
JOHNSON T J	JOHNSON G ELAINE	2425 N LOCUST GROVE RD	KUNA, ID 83634-1313
KERBS SCOTT D	KERBS SHANA D	1062 E ANDES DR	KUNA, ID 83634-0000
KOUDELKA J J JR	KOUDELKA MADGE H	PO BOX 1564	NAMPA, ID 83653-1564
KW HOMES INC		3866 S LAMONE WAY	MERIDIAN, ID 83642-0000
LEWIS CYNTHIA M		919 E ANDES DR	KUNA, ID 83634-0000
MCCOY MICHAEL ALAN	MCCOY ALEYSHA DAWN	1061 E ANDES DR	KUNA, ID 83634-0000
NATIONS PATRICIA A		18549 EMERALD LAKE AVE	NAMPA, ID 83687-0000
OLYMPUS HOMES LLC		2060 BODINE CT	BOISE, ID 83705-0000
PATAGONIA DEVELOPMENT LLC		PO BOX 344	MERIDIAN, ID 83680-0000
PATAGONIA HOA LLC		PO BOX 344	MERIDIAN, ID 83680-0000
PATAGONIA SUBDIVISION HOA INC		1710 S WELLS ST STE 110	MERIDIAN, ID 83642-0000
POND ROBBIE J	POND NANCY J	645 E MERINO ST	KUNA, ID 83634-0000
RIVERWOOD HOMES INC		PO BOX 344	MERIDIAN, ID 83680-0000
ROBINSON DANIEL	ROBINSON BONNIE	1046 E ANDES DR	KUNA, ID 83634-0000
ROSE TONY & VICKI L DECLARATION OF TRUST	ROSE TONY TRUSTEE	1099 E ANDES DR	KUNA, ID 83634-0000
SCHROEDER ENTERPRISES INC		1825 E SPIRIT HAWK LN	KUNA, ID 83634-0000
SCHROEDER ENTERPRISES INC		1825 E SPIRIT HAWK LN	KUNA, ID 83634-5101
SHADOW MOUNTAIN CONSTRUCTION INC		52 N PALMETTO AVE STE 102	EAGLE, ID 83616-0000
STACY CONSTRUCTION INC		1200 E WATERTOWER ST STE 120	MERIDIAN, ID 83642-5028
STEWART THOMAS G	STEWART DARCELL	1111 E ANDES DR	KUNA, ID 83634-0000
VARRIALE CONSTRUCTION INC		2018 S POND ST	BOISE, ID 83705-0000
WILLMAN LINDSEY P	WILLMAN CONNIE L	9520 S FUEGO AVE	KUNA, ID 83634-0000
WOOD PROPERTIES LLC		PO BOX 344	MERIDIAN, ID 83680-0000
YOUNG ROBERT LYMAN	YOUNG JOLI ELISA STOKES	665 E MERINO ST	KUNA, ID 83634-0000



**400'  
PROPERTY**



**LEGEND**

-  SUBJECT SITE
-  400' PROPERTIES
-  PARCEL LINES
-  ROADS
-  WATER FEATURES

**JMH**

**Jace Hellman**

---

**From:** Jace Hellman  
**Sent:** Thursday, February 7, 2019 9:15 AM  
**To:** 'IDAHO PRESS TRIBUNE'  
**Subject:** City of Kuna Request for Legal Publication  
**Attachments:** Ledgestone Legal Publication KMN 2.13.19.docx

Greetings:

We would like to request that you publish the attached legal notification in the February 13, 2019 cycle of the Kuna Melba News on behalf of the City of Kuna, Planning and Zoning Department. This notification needs to only be published for one (1) cycle.

The Kuna P.O. for these requests is #8044 (if you need it)  
Thank you!

Jace Hellman  
Planner II  
751 W 4<sup>th</sup> St  
Kuna, ID 83634  
[jhellman@kunaid.gov](mailto:jhellman@kunaid.gov)



CITY OF KUNA  
PO Box 13 - Kuna, ID 83634  
Phone: 208.922.5274 - Fax: 208.922.5989

**File #'s 18-06-AN & 18-08-S, Ledgestone Subdivision**

NOTICE IS HEREBY GIVEN, that the Kuna City Council will hold a public hearing, **Tuesday, March 5, 2019 at 6:00 pm**, or as soon as can be heard at Kuna City Hall, 751 W. 4<sup>th</sup> St, Kuna, ID; in connection with an **Annexation (AN)** and **Preliminary Plat (S)** request for Ledgestone Subdivision. WHPacific, on behalf of Trilogy Development Inc, requests to annex two contiguous parcels comprising of approximately 60.85-acres on East Hubbard Road and North Locust Grove Road into Kuna City Limits with an R-8 zoning designation; and to subdivide the 60.85-acres, into 298 total lots, consisting of 253 single family buildable lots and 45 common lots. The subject sites are adjacent to the southwest corner of East Hubbard Road and North Locust Grove Road, Kuna, ID 83634, within Section 18, Township 2 North, Range 1 East (APN's S1418121126 & S1418123400).

The public is invited to present written or oral comments. Written testimony received by the close of business on **February 27, 2019** will be included in the packets distributed to the governing body. Late submissions (must include six (6) copies) will be presented to the governing body at the time of the hearing. Please mail written comments to PO Box 13, Kuna, ID 83634, or drop them off at City Hall: 751 West 4<sup>th</sup> Street, Kuna, ID.

Please do not contact anyone who would be involved in this decision making process, which would include the Planning & Zoning Commissioners, City Council Members, or the Mayor; as such private conversations would be considered ex parte (one sided) and could jeopardize the public hearing process.

If you have any questions or require special accommodations, please contact the Kuna Planning & Zoning Department prior to the meeting at (208) 922-5274.

Kuna Planning & Zoning Department

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*(No need to print this portion) Please publish one time on February 13, 2019.*

(Sent 2/7/2019)

Kuna P.O. #8044

IDAHO PRESS TRIBUNE  
EMMETT, MERIDIAN, KUNA, BOISE WEEKLY  
C/O ISJ PAYMENT PROCESSING CENTER  
PO BOX 1570  
POCATELLO ID 83204  
(208)467-9251  
Fax (208)475-2338

ORDER CONFIRMATION

Salesperson: LEGALS

Printed at 02/07/19 09:48 by sje14

Acct #: 345222

Ad #: 1866396

Status: New HOLD

1 KUNA, CITY OF  
P.O. BOX 13  
KUNA ID 83634

Start: 02/13/2019 Stop: 02/13/2019  
Times Ord: 1 Times Run: \*\*\*  
LEG 1.00 X 76.00 Words: 306  
Total LEG 76.00  
Class: 0006 GOVERNMENT NOTICES  
Rate: LG Cost: 61.24  
# Affidavits: 1

Contact: CHRIS ENGLER  
Phone: (208)387-7727  
Fax#:  
Email: awelker@kunaaid.gov; gsmith@k  
Agency:

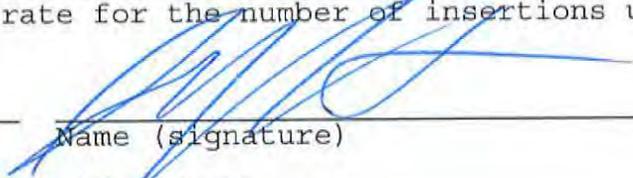
Ad Descrpt: 18-06-AN, 18-08-S  
Given by: JACE HELLMAN  
P.O. #:  
Created: sje14 02/07/19 09:45  
Last Changed: sje14 02/07/19 09:48

PUB ZONE EDT TP RUN DATES  
KMN A 96 S 02/13

AUTHORIZATION

Under this agreement rates are subject to change with 30 days notice. In the event of a cancellation before schedule completion, I understand that the rate charged will be based upon the rate for the number of insertions used.

Jace Hellman  
Name (print or type)



Name (signature)

(CONTINUED ON NEXT PAGE)

IDAHO PRESS TRIBUNE  
EMMETT, MERIDIAN, KUNA, BOISE WEEKLY  
C/O ISJ PAYMENT PROCESSING CENTER  
PO BOX 1570  
POCATELLO ID 83204  
(208)467-9251  
Fax (208)475-2338

ORDER CONFIRMATION (CONTINUED)

Salesperson: LEGALS

Printed at 02/07/19 09:48 by sje14

Acct #: 345222

Ad #: 1866396

Status: New CHOLD CHOI

LEGAL NOTICE

File #'s 18-06-AN & 18-08-S,  
Ledgestone Subdivision

*Looks Great*

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Kuna Planning & Zoning  
Department

February 13, 2019  
1866396











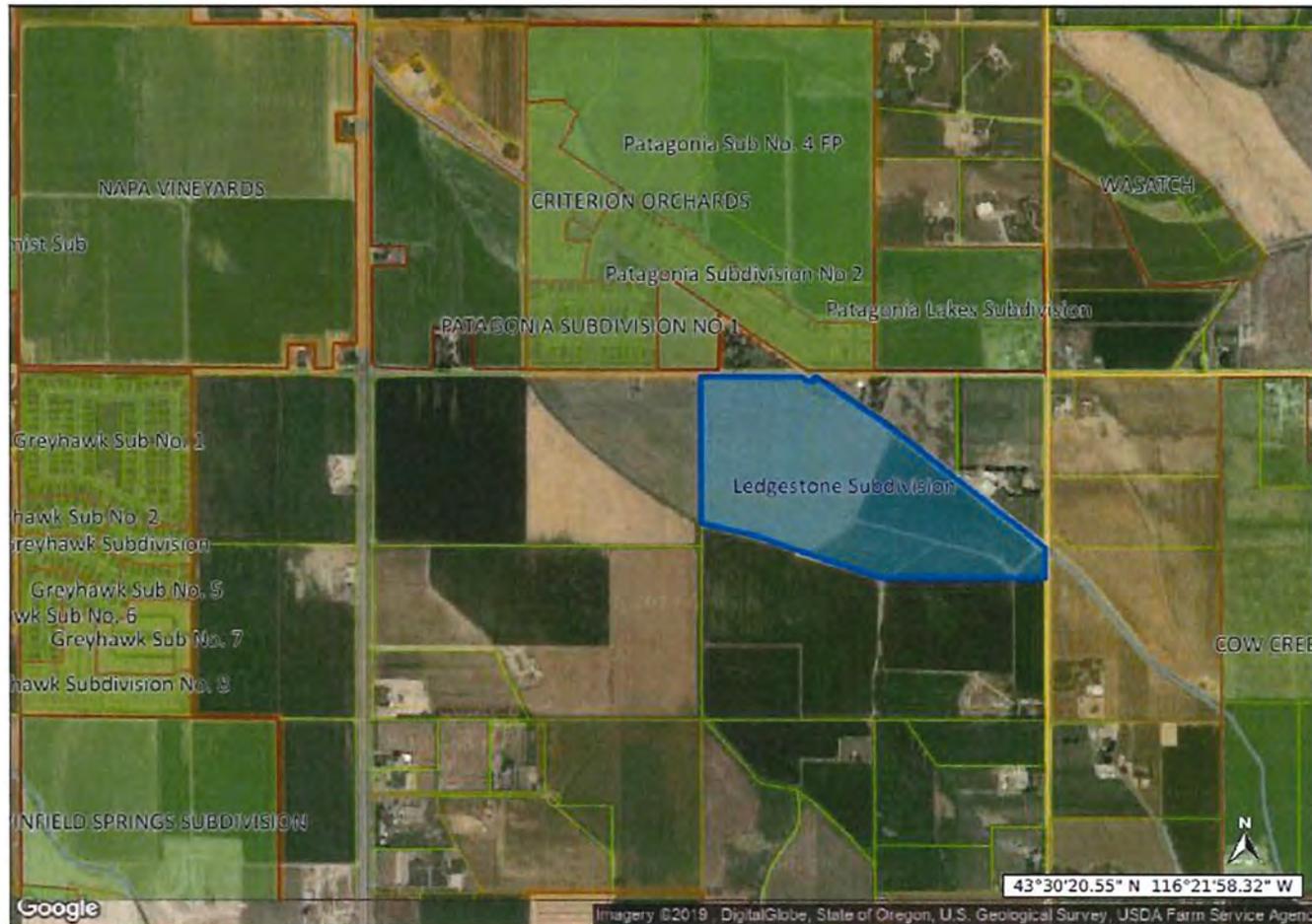








### Ledgestone Subdivision Vicinity Map



Feb 28, 2019 - landproDATA.com  
Scale: 1 inch approx 1000 feet

The materials available at this website are for informational purposes only and do not constitute a legal document.





# CITY OF KUNA FUTURE LAND USE MAP

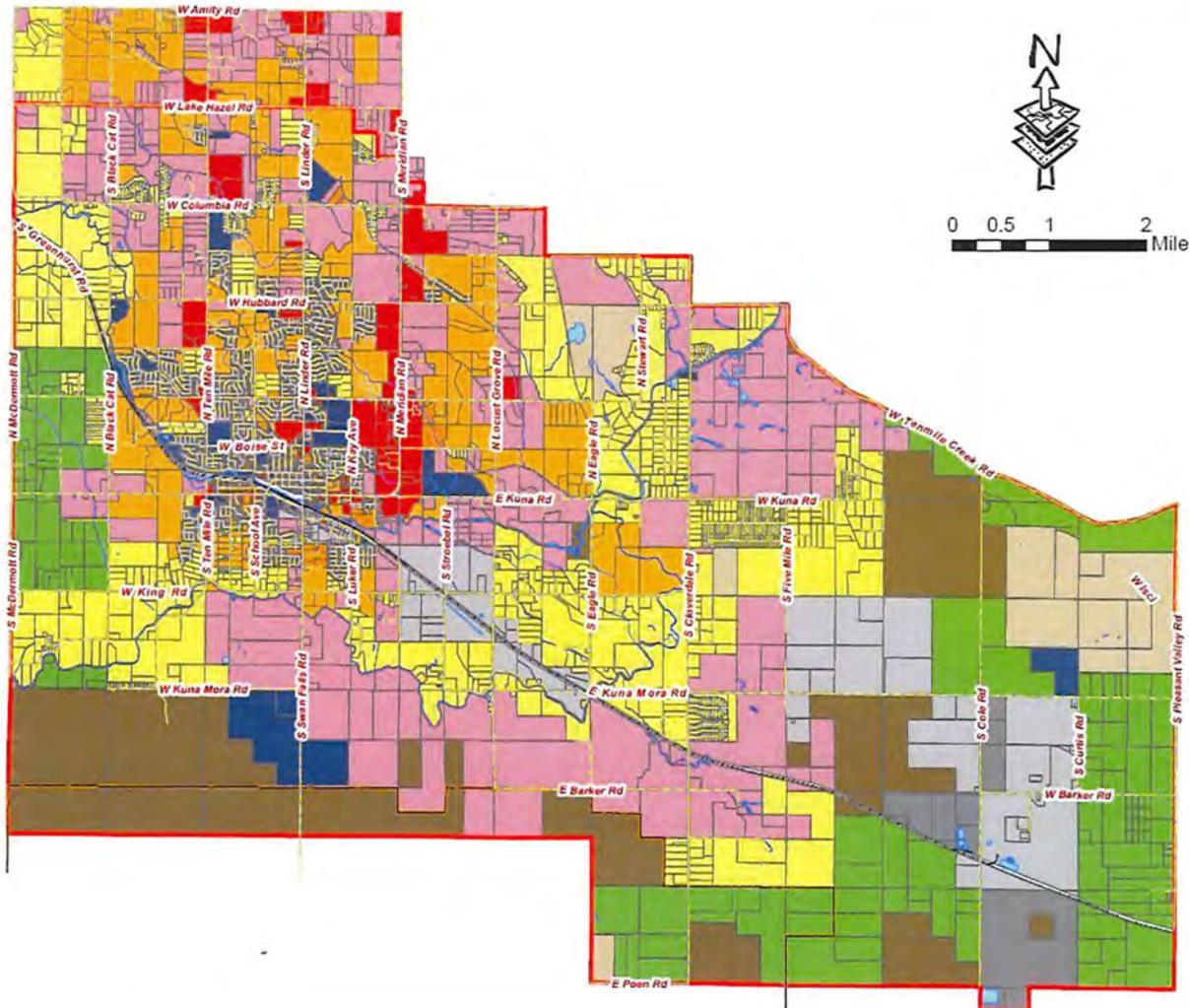
DECEMBER 19th, 2018

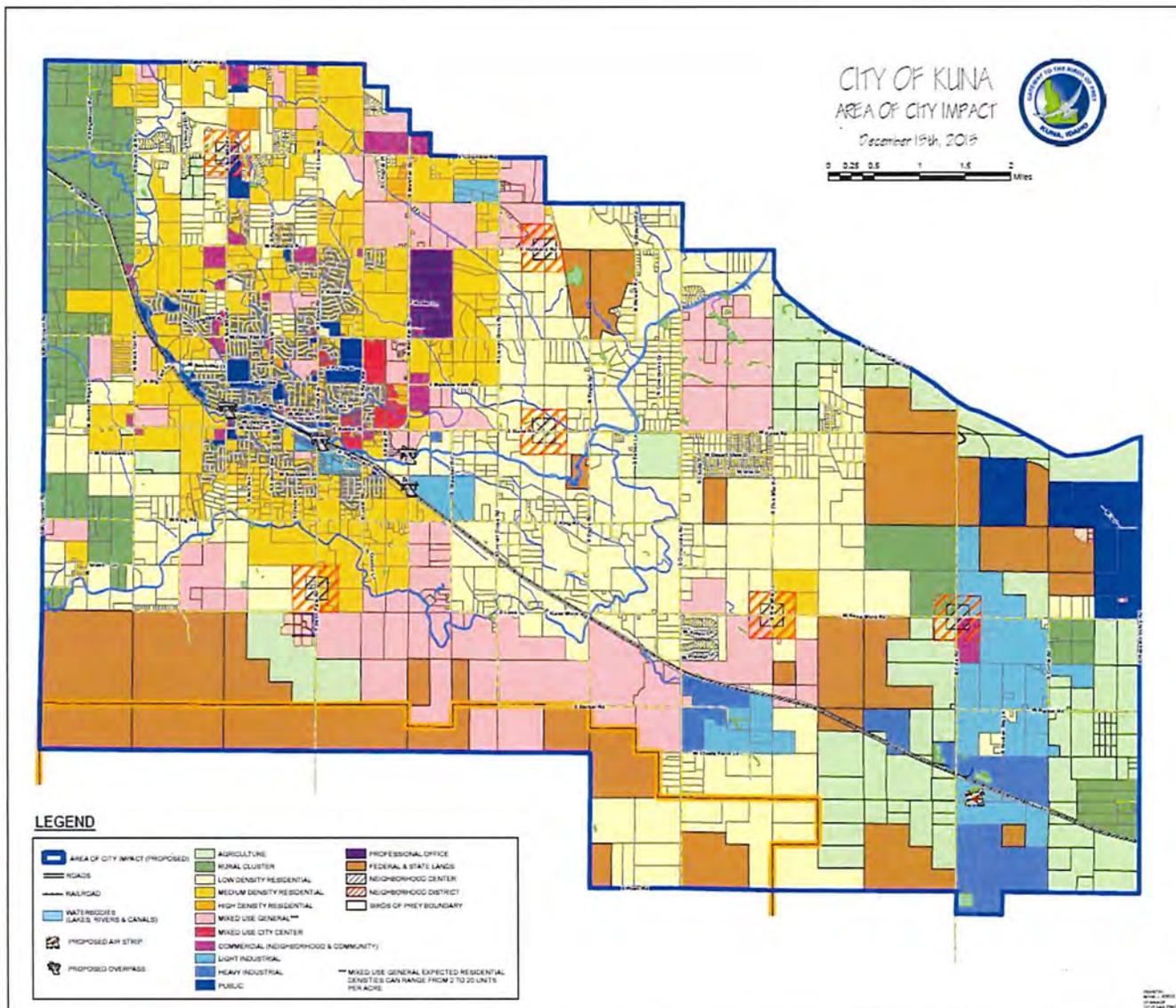
**Legend**

- EXISTING ROADS
- KUNA AREA OF IMPACT
- RAILROAD
- BIRDS OF PREY
- WATERWAY

**COMPREHENSIVE CLASSIFICATIONS**

- AGRICULTURE
- COMMERCIAL
- HEAVY INDUSTRIAL
- LIGHT INDUSTRIAL
- HIGH DENSITY RESIDENTIAL
- MEDIUM DENSITY RESIDENTIAL
- LOW DENSITY RESIDENTIAL
- MIXED-USE
- LOCAL PUBLIC LANDS
- STATE LAND
- FEDERAL LAND





**KUNA CITY ORDINANCE NO. 2019-07  
CITY OF KUNA**

**AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF KUNA:**

- **REPEALING SECTION 16 OF CHAPTER 3 OF TITLE 7 KUNA CITY CODE;  
AND**
- **AMENDING CHAPTER 3 OF TITLE 7 KUNA CITY CODE BY THE  
ADDITION OF NEW SECTION 16 MAKING IT UNLAWFUL TO SELL,  
DISTRIBUTE, POSSESS OR CONSUME ANY TYPE OF ALCOHOLIC  
BEVERAGE ON KUNA CITY PARK AND CITY PUBLIC PROPERTY  
EXCEPT FOR BEER AND/OR WINE UNDER A PARK BEER AND/OR WINE  
ALCOHOLIC BEVERAGES SPECIAL USE PERMIT ISSUED BY THE CITY;  
AND PROVIDING PERMIT TERMS AND CONDITIONS; AND**
- **DIRECTING THE CITY CLERK; AND**
- **PROVIDING AN EFFECTIVE DATE.**

**NOW, THEREFORE, BE IT ORDAINED** by the Mayor and City Council of the City of Kuna, Ada County, Idaho:

**Section 1:** That Section 16 of Chapter 3 of Title 7 of the Kuna City Code is hereby REPEALED.

**Section 2:** That Chapter 3 of Title 7 of the Kuna City Code be amended by the adoption of a new Section 16 to read as follows:

7-3-16: - BEER AND WINE PARK USE PERMITS: It shall be unlawful for any person, association, corporation entity or group, to sell, distribute, possess or consume any type of alcoholic beverage on the premises of any Kuna City Park and City Public Property, except as is specifically provided in this section.

- A. Beer and/or wine may be distributed, possessed and consumed under a *park beer and/or wine alcoholic beverages special use permit* (the "Permit") issued pursuant to this section within the following named city parks:

Bernie Fisher  
The Farm  
Arbor Ridge  
Nicholson  
Sadie Creek  
Winchester  
Greenbelt  
Butler

- B. The Application for a Permit (the “Application”) shall be completed by an applicant on a City Council approved form together with the payment of the Application fee, as set by the City Council and filed with the City Clerk, together with proof of all of the following:
1. The applicant’s name or the name of the applicant’s representative and the legal status of any entity applicant together with the applicant’s contact information as specified on the Application form; and
  2. Identification of the city park and the area within one of the city park which is the subject of the Application; and
  3. A description of the event, the time period and the number of persons anticipated to be the subject of the Permit; and
  4. Applicant or the applicant’s representative is twenty-one (21) years of age or older.
  5. Alcohol Beverage Catering Permit for and in the event there will be the sale of Beer and/or wine.
- C. The Application shall be signed by the applicant or the group representative of the applicant who will be responsible for the actions of the Permit participants who will be held liable for park damage, restoration, and/or cleanup costs.
- D. Completed applications filed together with the payment of the Application fee shall be filed with and processed by the city clerk.
- E. The City Clerk shall provide a copy of the Application to the Parks and Recreation Director for determination of park area availability, applicant eligibility and any needed additional Permit conditions.
- F. The city clerk or their designee, shall approve or deny the Application in accordance with the provisions of this section and state on the face of the Application, or upon an endorsement separately attached, under the seal of the city clerk either approval stating any special condition of approval or denial stating the basis of denial. The clerk shall retain a signed copy of the application, including attachments, if any.
- G. Permit conditions at a minimum shall include all of the following:
1. Park area availability; and
  2. Park hours; and
  3. Individual possession is limited to beer and/or wine
  4. Limitation of 7.5 gallons of beer and/or wine or less; and

5. Must be twenty-one (21) years of age to consume or possess beer and/or wine; and
6. All state, county, and city laws in regard to alcoholic beverages shall be obeyed; and
7. Beer and wine may only be sold or vended in any fashion, or be distributed as a basis to solicit donations, contributions nor under any other form of payment consideration, pursuant to the alcohol beverage catering permit provided by the Applicant and which alcohol beverage catering permit must be posted at the Permit site; and
8. Persons who are the subject of the Permit shall not disturb the peace of any other user of the park; and
9. Persons who are the subject of the Permit are also subject to park policies; and
10. Violators of Permit conditions are subject to ejection from the park by a law enforcement officer or City parks department official; and
11. In the event of a violation or violations of Permit the responding law enforcement officer may revoke the Permit, confiscate the remaining beer and/or wine and terminate the event which is the subject of the Permit; and
12. In the event of a Permit violation or violations, the applicant and any person who is the subject of a Permit violation shall be ineligible for a Permit for a one-year period subsequent to the date of the violation; and
13. Applicants are responsible to leave the subject area of the Permit clean and without litter; and
14. Applicants are responsible for the actions of any persons who are subject of the Permit and must remain present, be in possession of the Permit and shall produce the Permit upon request of law enforcement officer or City Park official at all times which are the subject of the Permit.
15. The City of Kuna assumes no liability for the negligence of any persons who are subject of the Permit; and
16. Permits are valid only during the date, time period and location listed on the Permit.

### **Section 3: Directing the City Clerk**

- 3.1 The City Clerk is directed to file, this Ordinance in the official records of the City and to provide the same to the City's codifier for inclusion and publication in the Kuna City Code.

**Section 4: Effective Date**

- 4.1 This Ordinance shall take effect and be in force from and after its passage, approval, and publication as required by law and at the discretion of the City Clerk and In lieu of publication of the entire ordinance, a summary thereof in compliance with Section 50-901A, Idaho Code, may be published.

DATED this 5th day of March, 2019.

CITY OF KUNA

---

Joe L. Stear, Mayor

ATTEST:

---

Chris Engels, City Clerk

**RESOLUTION NO. R14-2019  
CITY OF KUNA, IDAHO**

**A RESOLUTION OF THE CITY OF KUNA, IDAHO, ESTABLISHING FEES FOR BEER/WINE PERMIT ALLOWING FOR INDIVIDUAL USE AT CITY PARKS AND PROVIDING AN EFFECTIVE DATE.**

WHEREAS, the Kuna City Council has adopted Ordinance No. 2019-07, governing individual use of beer and wine at City parks; and

WHEREAS, a public hearing, properly noticed under the provisions of Idaho Code § 63-1311A, was held on March 5, 2019 establishing such fees;

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF KUNA, Ada County, Idaho, as follows:

Section 1: Until modified by further resolution of the Kuna City Council, fees to be charged and collected by the City of Kuna for issuance of licenses for the retail sale of beer, wine and liquor shall be as set forth in Exhibit "A" attached hereto and incorporated herein by this reference.

Section 2: This resolution shall take effect and be in force from and after its passage and approval.

DATED this 5<sup>th</sup> day of March, 2019.

\_\_\_\_\_  
Joe L. Stear, Mayor

ATTEST:

\_\_\_\_\_  
Chris Engels, City Clerk

**EXHIBIT TO RESOLUTION NO. R14-2019**

**CITY OF KUNA**

**FEEES FOR INDIVIUAL BEER/WINE PERMIT  
EFFECTIVE MARCH 5, 2019**

**Individual Beer/Wine Permit      \$20.00**

# CITY OF KUNA

## State of Idaho *Proclamation*

### FAIR HOUSING MONTH PROCLAMATION

**WHEREAS**, April 2019 marks the 51th anniversary of the passage of Title VIII of the Civil Rights Act of 1968, commonly known as the Federal Fair Housing Act; and

**WHEREAS**, the Idaho Human Rights Commission Act has prohibited discrimination in housing since 1969; and

**WHEREAS**, equal opportunity for all-regardless of race, color, religion, sex, disability, familial status or national origin-is a fundamental goal of our nation, state and city; and

**WHEREAS**, equal access to housing is an important component of this goal-as fundamental as the right to equal education and employment; and

**WHEREAS**, housing is a critical component of family and community health and stability and

**WHEREAS**, housing choice impacts our children’s access to education, our ability to seek and retain employment options, the cultural benefits we enjoy, the extent of our exposure to crime and drugs, and the quality of health care we receive in emergencies; and

**WHEREAS**, the laws of this nation and our state seek to ensure such equality of choice for all transactions involving housing; and

**WHEREAS**, ongoing education, outreach and monitoring are key to raising awareness of fair housing principles, practices, rights and responsibilities; and

**WHEREAS**, only through continued cooperation, commitment and support of all Idahoans can barriers to fair housing be removed;

**NOW, THEREFORE, BE IT RESOLVED** that I, Joe L. Stear, Mayor of the City of Kuna, Idaho, hereby proclaim April 2019 to be FAIR HOUSING MONTH in the City of Kuna/Ada County, State of Idaho.



**IN WITNESS WHEREOF,  
I set my hand on this the 5<sup>th</sup> day of March  
in the year of two thousand and nineteen.**

---

**Joe L. Stear  
Mayor of Kuna, Idaho**

**RESOLUTION NO. R21-2019  
CITY OF KUNA, IDAHO**

**A RESOLUTION OF THE CITY OF KUNA, IDAHO, APPROVING AND ADOPTING AN AMENDMENT TO THE CITY OF KUNA PERSONNEL POLICY MANUAL SECTION 12.3.5 REGARDING APPROVAL OF EMPLOYEE VACATION CAP EXTENSION AND PROVIDING AN EFFECTIVE DATE.**

**BE IT HEREBY RESOLVED** by the Mayor and Council of the City of Kuna, Idaho as follows:

Section 1. The amendment to section 12.3.5 of the City of Kuna, Idaho Personnel Policy shall read as follows:

The City recognizes that there may be unavoidable, however extremely limited circumstances that would cause an employee to exceed the Vacation Leave cap. If this is the case, employee may request a review of the circumstances of the case to the department Director, Human Resource Manager and Treasurer. Any approved extension must have final approval by the Mayor and it must be used no later than March 31 of the next calendar year

Section 2. The amendment to section 12.3.5 of the City of Kuna, Idaho Personnel Policy is hereby approved and adopted;

Section 2. The Human Resources Director shall distribute copies of this policy change to the holders of the personnel policy.

Section 3. This resolution shall be effective as of the date of adoption.

**PASSED BY THE COUNCIL** of Kuna, Idaho this 5<sup>th</sup> day of March, 2019.

**APPROVED BY THE MAYOR** of Kuna, Idaho this 5<sup>th</sup> day of March, 2019.

\_\_\_\_\_  
Joe L. Stear, Mayor

ATTEST:

\_\_\_\_\_  
Chris Engels, City Clerk

**ORDINANCE NO. 2019-05  
KUNA, IDAHO**

**AN ORDINANCE OF THE CITY COUNCIL OF KUNA, IDAHO AMENDING TITLE 1, CHAPTER 6, SECTION 8 ENTITLED PUBLIC HEARING RULES OF PROCEDURE OF THE KUNA CITY CODE PROVIDING FOR:**

- **CLARIFICATION ON PUBLIC HEARING RULES OF PROCEDURE;**
- **ESTABLISHING TESTIMONY TIME LIMITS;**
- **REGULATING THE PRESENTATION OF DOCUMENTS AND OTHER FORMS OF PHYSICAL EVIDENCE;**
- **AMENDING THE RECOGNIZED PUBLIC HEARING MOTIONS;**
- **CLARIFYING CONFLICT OF INTEREST PROCEEDURES; AND**
- **PROVIDING FOR AN EFFECTIVE DATE AND PUBLICATION.**

**NOW, THEREFORE, BE IT ORDAINED** by the Mayor and City Council of the City of Kuna, Ada County, Idaho:

**Section 1:** That Title 1, Chapter 6, Section 8 of the Kuna City Code be and the same is hereby amended to read as follows:

1-6-8: - PUBLIC HEARING RULES OF PROCEDURE:

The following rules are hereby established, shall be observed in the conduct of any public hearing before the city council, planning and zoning commission, design review committee, or any city standing or ad hoc committee (a "City Agency") authorized to conduct public hearings, and shall be known as the "public hearing rules of procedure".

- A. No person shall be permitted to testify or speak before a City Agency at a public hearing, unless such person has written his or her name and address thereafter on a sign-up sheet provided by the city or unless such person has been recognized and authorized by the presiding chairman to speak. This rule shall not apply to staff or technical witnesses directed by the presiding chairman to testify or provide evidence at the public hearing.
- B. No person shall be permitted to speak before a City Agency at a public hearing until such person has been recognized by the presiding chairman and has clearly stated for the record their name, address, and interest in the matter.
- C. All public hearing proceedings shall be recorded electronically and all persons speaking at such public hearings shall speak before a microphone in such a manner as will assure that the recorded testimony or remarks will be included in the record of the proceedings and the clerk of the proceedings shall notify speakers when they are not in compliance.

- D. The following is the general public hearing time limits, unless a special hearing time limit is otherwise established by majority vote of the members of the City Agency, for speakers [not including time for questions and response] is as follows:

Staff presentation: fifteen (15) minutes; and

Applicant representative: ten (10) minutes, and

Public testimony: three (3) minutes; and

Applicant rebuttal testimony: ten (10) minutes.

Persons called to testify by the City Agency members to respond to their questions are not subject to these time limits.

No testimony will be received after the approval of a motion to close evidence presentation and proceed to deliberation.

Any special time limit established by the City Agency shall consider but not be limited to, the number of speakers who sign up for each hearing, providing a fair opportunity for interested parties and persons to present relevant evidence or information, the lateness of the hour at the commencement of the hearing, and the number of hearings left on the meeting agenda, and shall apply only to the speaker's comments.

- E. Any person or applicant who wishes to speak and who also wishes to present any documents and other forms of physical evidence not already in the record of proceedings must provide the same to the Clerk of the Proceedings prior to the commencement of the receipt of testimony so the same may be marked as an exhibit by the Clerk which shall be identified by the speaker during their testimony; and any electronic displays or presentations must be provided prior to the meeting and prior to the distribution of the agenda and supporting documents to the governing body. No late electronic submissions will be allowed and no non-city produced external disc or external drive will be permitted to be used on city equipment. Any paper copies must be in a size that is clearly viewable by the governing body and attendees of the meeting from a minimum distance of ten (10) feet or individual copies may be provided and, in a quantity, to provide a copy for all members of the governing body, staff and any public in attendance.
- F. Speakers should not be interrupted by members of the City Agency so long as the speaker's comments are related to the subject matter of the hearing until an imposed time limit has been reached.
- G. At the conclusion of a speaker's comments, each member of the City Agency, when recognized by the presiding chairman, shall be allowed to question the speaker, and the speaker shall be limited to answers to the questions asked. The question and answer period shall not be included in the speaker's time limit, if one has been established.
- H. Any person not conforming to any of the above rules may be prohibited from speaking during the public hearing. Any person refusing to comply with such prohibition may be removed from the room by order of the presiding chairman.
- I. All decisions made or actions taken by a City Agency shall be initiated by motion of one (1) of its members.

- J. Every motion seconded by another member shall proceed to a vote unless such motion be withdrawn by the maker of the motion prior to the vote. Every motion failing to receive a second shall die without a vote.
- K. There shall be five (5) recognized public hearing motions which can be made in meetings subject to these rules, and listed in priority order in the event more than one (1) motion is pending, such motions are:
1. Motion to open the public hearing;
  2. Motion to suspend or amend these rules
  3. Motion for recess and continuance of the public hearing for matters as specified to a time and date certain;
  4. Motion to close evidence presentation and proceed to deliberation;
  5. Motion to take action as is relevant to the deliberation and when relevant a direction to prepare findings of fact, conclusions and law and order of recommendation or decision for approval at the next scheduled meeting.
- L. City Agency members shall disclose potential conflicts of interest they may have regarding the subject matter of the public hearing. They may ask the city attorney for an opinion as to whether or not they have a conflict of interest under law. A member with a conflict of interest; and additionally, in a quasi-judicial proceeding, who discloses that they will be unable to make a fair and impartial determination of the facts at issue based solely upon the evidence presented at the hearing, shall recuse themselves and request permission from the presiding chairman to absence themselves from the hearing. A member for any other reason, desiring not to participate in a matter before a public hearing shall advise the presiding chairman of the reason therefor. A vote of the other members present shall be called by the chairman which shall require a majority vote of the other members of the City Agency in order to be excused from the hearing. As all meetings and hearings, except for executive sessions, are open to public view and scrutiny, all deliberations by body members shall be clearly audible in order to be heard by all persons present in the room and capable of being later transcribed.
- M. For legislative matters, testimony and other information may be received by the body in any order chosen by the presiding chairman.
- N. For quasi-judicial matters such as, but not limited to, licensing applications and zoning applications, the presiding chairman shall receive testimony at the public hearing in the following order:
1. Report to the body by city staff;
  2. Presentation by the applicant;
  3. Testimony of those in support of the application;
  4. Testimony of those neutral toward the application;
  5. Testimony of those opposed to the application;
  6. Rebuttal presentation by the applicant.

- O. The body may suspend or amend any one or more of these rules by vote of one-half plus one (1) of the full body, and the presiding chairman shall have the authority to interpret and apply the foregoing rules. An appeal of such interpretation or application may be made to the city council, which will act on such appeal as expeditiously as it can. Appeals during a council meeting may be decided during the public hearing wherein the question arises, or may be decided at a later date, at the council's discretion.

**Section 2: Date of Effect and Publication:**

This ordinance shall be in full force and effect upon its passage, approval and publication, according to law.

ADOPTED this 19th day of February, 2019.

CITY OF KUNA

\_\_\_\_\_  
Joe L. Stear, Mayor

ATTEST:

\_\_\_\_\_  
Chris Engels, City Clerk

**ORDINANCE NO. 2019-X  
KUNA, IDAHO**

**AN ORDINANCE OF THE CITY COUNCIL OF KUNA, IDAHO AMENDING TITLE 1, CHAPTER 6, SECTION 8 ENTITLED PUBLIC HEARING RULES OF PROCEDURE OF THE KUNA CITY CODE PROVIDING FOR:**

- **CLARIFICATION ON PUBLIC HEARING RULES OF PROCEDURE;**
- **ESTABLISHING TESTIMONY TIME LIMITS;**
- **REGULATING THE PRESENTATION OF DOCUMENTS AND OTHER FORMS OF PHYSICAL EVIDENCE;**
- **AMENDING THE RECOGNIZED PUBLIC HEARING MOTIONS;**
- **CLARIFYING CONFLICT OF INTEREST PROCEEDURES; AND**
- **PROVIDING FOR AN EFFECTIVE DATE AND PUBLICATION.**

**NOW, THEREFORE, BE IT ORDAINED** by the Mayor and City Council of the City of Kuna, Ada County, Idaho:

**Section 1:** That Title 1, Chapter 6, Section 8 of the Kuna City Code be and the same is hereby amended to read as follows:

**1-6-8: - PUBLIC HEARING RULES OF PROCEDURE:**

The following rules are hereby established, shall be observed in the conduct of any public hearing before the city council, planning and zoning commission, design review committee, or any city standing or ad hoc committee other body ( a "City Agency") authorized to conduct public hearings, and shall be known as the "public hearing rules of procedure".

- A. A- No person shall be permitted to testify or speak before a City Agency the body at a public hearing, unless such person has written his or her name and address thereafter on sign up/sign-up sheets to be provided by the city or unless such person has been recognized and authorized by the presiding chairman and invited to speak. This rule shall not apply to staff or technical witnesses directed by the presiding chairman to testify or provide evidence at the public hearing. give evidence or information to the body.
- B. B- No person shall be permitted to speak before a City Agency the body at a public hearing until such person has been recognized by the presiding chairman and has clearly stated for the record such person's their name, and address and interest in the matter.
- C. C- All public hearing proceedings shall be recorded electronically and all persons speaking at such public hearings shall speak before a microphone in such a manner as will assure that the recorded testimony or remarks will be included in the record of the

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~~proceedings accurate and trustworthy and the clerk of the proceedings shall notify speakers when they are not in compliance.~~

~~D. D-~~ The following is the general public hearing time limits, unless a special hearing time limit is otherwise established by majority vote of the members of the City Agency, for speakers [not including time for questions and response] At the commencement of the public hearing is as follows:

Staff presentation: fifteen (15) minutes; and

~~Applicant representative, the presiding chairman may established a time limit is to be observed by all speakers. Applicant is limited to ten (10) minutes, and~~

~~Public testimony is limited to three (3) minutes; and~~

~~rebuttal of the Applicant rebuttal testimony is limited to ten (10) minutes.~~

~~Persons called to testify by the City Agency members to respond their questions are not subject to these time limits.~~

~~No testimony will be accepted by received after the approval of a motion to close evidence presentation and proceed to deliberation. applicant or public after the closure of the rebuttal time unless the governing body ask a question.~~

Any special time limit established by the City Agency shall consider other than listed may be imposed shall to reflect a reasonable balance of factors including, but not be limited to, the number of speakers who sign up for each hearing, providing a fair opportunity for interested parties and persons to present relevant evidence or information, the lateness of the hour at the commencement of the hearing, and the number of hearings left on the meeting agenda, and shall apply only to the speaker's comments.

~~E. E-~~ Any person or applicant who wishes to speak and who also wishes to present any documents and other forms of physical evidence not already in the record of proceedings must provide the same to the Clerk of the Proceedings prior to the commencement of the receipt of testimony so the same may be marked as an exhibit by the Clerk which shall be identified by the speaker during their testimony; and ~~a~~Any electronic displays or presentations must be provided prior to the meeting and prior to the distribution of the agenda and supporting documents to the governing body. No late electronic submissions will be allowed and no non-city produced external disc or external drive will be permitted to be used on city equipment. Any paper copies must be in a size that is clearly viewable by the governing body and attendees of the meeting from a minimum distance of ten (10) feet or individual copies may be provided and, in a quantity, to provide a copy for all members of the governing body, staff and any public in attendance.

~~F. F-~~ Speakers should not ~~No speaker shall~~ be interrupted by members of the City Agency body or the presiding chairman so long as the speaker's comments are related to the subject matter of the hearing until an imposed time limit has been reached.

~~G. G-~~ At the conclusion of a speaker's comments, each member of the City Agency body, when recognized by the presiding chairman, shall be allowed to question the speaker, and the speaker shall be limited to answers to the questions asked. The question and answer period shall not be included in the speaker's time limit, if one has been established.

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~~H. G.~~ Any person not conforming to any of the above rules may be prohibited from speaking during the public hearing. Any person refusing to comply with such prohibition may be removed from the room by order of the presiding chairman.

~~I. H.~~ All decisions made or actions taken by a City Agency body shall be initiated by motion of one (1) of its members.

~~J. I.~~ Every motion seconded by another member shall proceed to a vote unless such motion be withdrawn by the maker of the motion prior to the vote. Every motion failing to receive a second shall die without a vote.

~~K. J.~~ There shall be ~~seven-five (75)~~ recognized public hearing motions which can be made in meetings subject to these rules, and listed in priority order in the event more than one (1) motion is pending, such motions are:

~~1. I.~~ Motion to open the public hearing amend the body's meeting agenda;

~~2. 2.~~ Motion to suspend or amend these rules

~~3.~~ Motion for ~~adjournment or recess~~ and continuance of the public hearing for matters as specified to a time and date certain;

~~4. 3.~~ Motion to close evidence presentation and proceed to deliberation table or continue any matter;

~~4.~~ Motion to take action as is relevant to the deliberation and when relevant a direction to prepare findings of fact, conclusions and law and order of recommendation or decision for approval at the next scheduled meeting. suspend or amend these rules;

~~5.~~ Substitute motions or motions to amend a pending motion;

~~5. 6.~~ Motion to approve, with or without conditions, or motion to deny any pending application or other matter; and

~~7.~~ Motion to hold an executive session (for those bodies where an executive session may be allowed).

~~K.~~ All members present for a meeting shall vote on each motion unless excused by a majority vote of the other members present based on reasons of conflict of interest or other good cause or may be excused by the presiding chairman at the chairman's discretion without a vote. City Agency members Each member shall disclose ~~at any meeting~~ potential conflicts of interest they may have regarding the subject matter of the public hearing that may exist for any of the items on the agenda prior to that agenda item being discussed. They may ask the city attorney for an opinion as to whether or not they have a conflict of interest under law. A member with a conflict of interest; and additionally in a quasi-judicial proceeding, who discloses that they will be unable to make a fair and impartial determination of the facts at issue based solely upon the evidence presented at the hearing, shall recuse themselves and request permission from the presiding chairman to absense themselves from the hearing. A member for any other reason, desiring not to participate in a any matter before a public hearing shall advise the presiding chairman of the reason therefor. A vote of the other members present shall be called by the ~~presiding chairman~~ chairman or the presiding chairman may determine whether there exists a conflict or other

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~~good cause which shall require a majority vote of the other members of the City Agency in order to be excused excusing such member from the hearing participation and any voting. Good cause shall only be found in circumstances where allowing participation of the member would reasonably appear to undermine the fairness and/or impartiality of the body's deliberation of a matter.~~

~~L. L.~~ As all meetings and hearings, except for executive sessions, are open to public view and scrutiny, all deliberations by body members shall be clearly audible in order to be heard by all persons present in the room and capable of being later transcribed.

~~M. M.~~ For legislative matters, testimony and other information may be received by the body in any order chosen by the presiding chairman.

~~N. N.~~ For quasi-judicial matters such as, but not limited to, licensing applications and zoning applications, the presiding chairman shall receive testimony at the public hearing in the following order:

- ~~1. 1.~~ Report to the body by city staff;
- ~~2. 2.~~ Presentation by the applicant;
- ~~3. 3.~~ Testimony of those in support of the application;
- ~~4. 4.~~ Testimony of those neutral toward the application;
- ~~5. 5.~~ Testimony of those opposed to the application;
- ~~6. 6.~~ Rebuttal presentation by the applicant.

~~O.~~ The body may suspend or amend any one or more of these rules by vote of one-half plus one (1) of the full body, and the presiding chairman shall have the authority to interpret and apply the foregoing rules. An appeal of such interpretation or application may be made to the city council, which will act on such appeal as expeditiously as it can. Appeals during a council meeting may be decided during the public hearing wherein the question arises, or may be decided at a later date, at the council's discretion.

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ADOPTED this 19th day of February, 2019.

CITY OF KUNA

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Joe L. Stear, Mayor

ATTEST:

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Chris Engels, City Clerk

